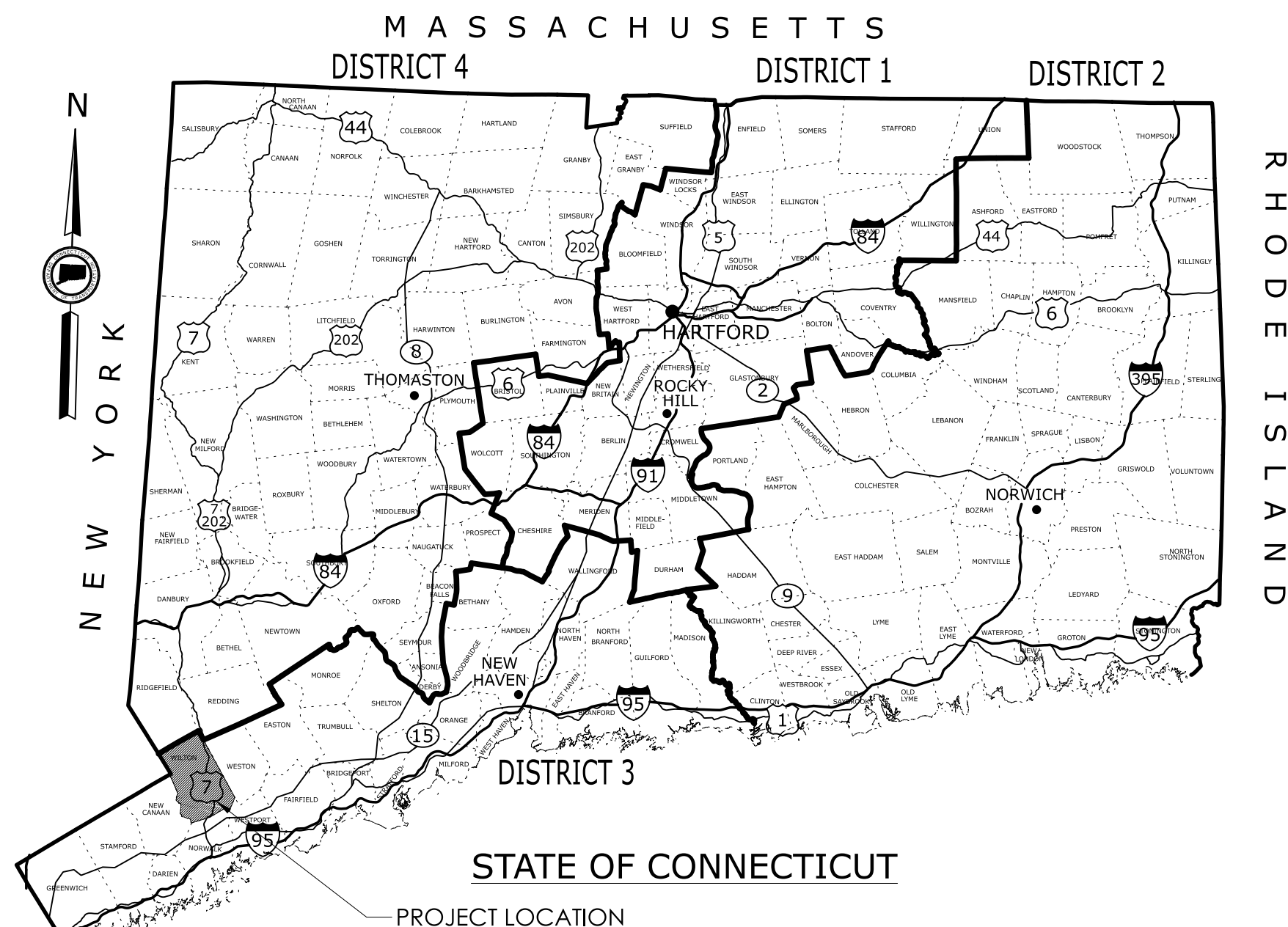
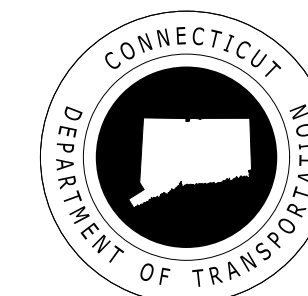


TOWN OF WILTON

Plans For

REPLACEMENT OF BRIDGE NO. 04975 LOVERS LANE OVER COMSTOCK BROOK



GENERAL NOTES:

- FEDERAL AID PROJECT NO. 6161(006)
- CONSTRUCTION SPECIFICATIONS:
Connecticut Department of Transportation, Standard Specifications for Roads, Bridges and Incidental Construction, Form 818, dated 2020; Supplemental Specifications, dated January 2022; and Special Provisions
- 400 FOOT GRID BASED ON CONNECTICUT COORDINATE SYSTEM N.A.D. 1983
- VERTICAL DATUM BASED ON NAVD 1988
- Surveyed By: CTDOT DISTRICT 3 SURVEYS
- DESIGN STANDARD:
AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS 9TH EDITION (2020) AND AS SUPPLEMENTED BY THE CONNECTICUT DEPARTMENT OF TRANSPORTATION BRIDGE DESIGN MANUAL (2003) WITH REVISIONS DATED 2019.
FUNCTIONAL CLASSIFICATION: LOCAL URBAN STREET
YEAR ADT: 508 (2019)

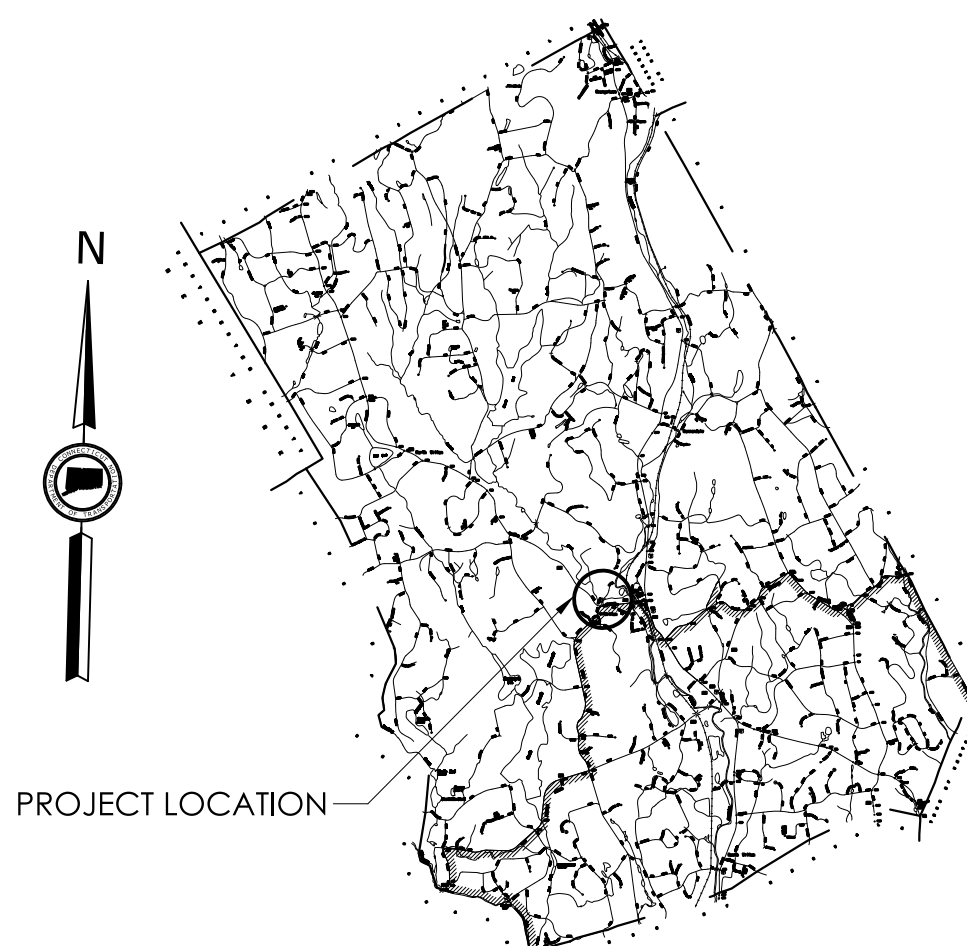
DISCLAIMER:

IT IS THE RESPONSIBILITY OF EACH BIDDER AND ALL OTHER INTERESTED PARTIES TO OBTAIN ALL BIDDING RELATED INFORMATION AND DOCUMENTS FROM OFFICIAL SOURCES WITHIN THE TOWN OF WILTON.

PERSONS AND/OR ENTITIES WHICH REPRODUCE AND/OR MAKE SUCH INFORMATION AVAILABLE BY ANY MEANS ARE NOT AUTHORIZED BY THE TOWN OF WILTON TO DO SO AND MAY BE LIABLE FOR CLAIMS RESULTING FROM THE DISSEMINATION OF UNOFFICIAL, INCOMPLETE AND/OR INACCURATE INFORMATION.

| ROAD | MAINTENANCE RESPONSIBILITY | LENGTH |
|-------------|----------------------------|------------|
| LOVERS LANE | TOWN | 449.4 FEET |

| F.A.P. # | MAINTENANCE RESPONSIBILITY | PROJECT # |
|-----------|----------------------------|-----------|
| 6161(006) | TOWN | 0161-0142 |



TOWN OF WILTON
N.T.S.



LOCATION PLAN
SCALE: 1" = 1000'

LIST OF SUBSETS

| SUBSET NO. | SUBSET TITLE | *SUBSET SHEET COUNT |
|------------|-------------------------------|---------------------|
| #01 | GENERAL | 3 |
| #02 | REVISIONS | 1 |
| #03 | HIGHWAY | 17 |
| #04 | STRUCTURE | 24 |
| #05 | TRAFFIC | 4 |
| #06 | F.I.O. - UTILITY RELOC. PLANS | 1 |

*THE INITIAL SUBSET SHEET COUNT DOES NOT INCLUDE ADDENDUM AND CHANGE ORDERS

LIST OF DRAWINGS SUBSET 01 - GENERAL

| DRAWING TITLE | DRAWING NO. |
|-----------------------------|-------------|
| TITLE SHEET | GEN-01 |
| DETAILED ESTIMATE SHEET - 1 | GEN-02 |
| DETAILED ESTIMATE SHEET - 2 | GEN-03 |

STANDARD CONVENTIONS

| | | | | |
|--------------------------|-----------------------|------------------|--------------|----------------|
| North Arrow W/No. Coord. | Grid Arrow | Chain Link Fence | Water Edge | Riprap |
| Edge Of Road | Limit Of Marsh | Rustic Fence | Stream | Hedge Row |
| Concrete Pavement | Stone Wall | Pipe Fence | Ditch | Tree Line |
| Dirt Road | Ledge Outcrop | Board Fence | TOWN LINE | Shrub |
| B.C.L.C. | Inland Wetland Limits | Property Line | Highway Line | Evergreen Tree |
| Granite Curb | STATE LINE | Lot Line | Street Line | Deciduous Tree |
| Guide Rail | Power Line | Easement Line | | Retaining Wall |
| Concrete Median Barrier | Swamp | | | |
| Bit. Walk | Building | | | |
| Cont. Sidewalk | Transmission Tower | | | |
| Railroad Tracks | | | | |

THE DESIGN APPEARS TO CONFORM TO APPLICABLE CRITERIA. APPROVAL IS NOT TO BE CONSTRUED TO MEAN THAT ALL ASPECTS OF THE DESIGN HAVE BEEN PERSONALLY CHECKED BY THE UNDERSIGNED.

SUBMITTED BY: DESIGNER (FUSS AND O'NEILL)

APPROVED BY: TOWN

Plans For
REPLACEMENT OF BRIDGE
NO. 04975 LOVERS LANE
OVER COMSTOCK BROOK

Town
WILTON

STATE PROJECT NO.

0161-0142

DRAWING NO.
GEN-01
SHEET NO.
01.01

| | | HIGHWAY | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|-------------|--|-----------------------|---------------|------------------|-----------------|----------------------------------|-----------------------|---------------|------------------------------|--|--|--------------------------|---------------------|----------|------------|------------------------|--|--|--|---|---|-------------------------------|--|--|--------------------------------------|-----------------|------------------------------------|--|---------------------------------|--|--|---|------------------------|------------------------------|---------------------------|------------------------|--------------------------------|
| ITEM NUMBER | | 0201001A | 0201501A | 0202000 | 0202100 | 0202529 | 0209001 | 0213100 | 0219001 | 0219011A | 0286001.10 | 0304002 | 0305001 | 0406171 | 0406172 | 0406236 | 0586002.10 | 0586041.10 | 0586045.10 | 0586300.15A | 0586790.10 | 0686000.15 | 0686230.15 | 0686715.15 | 0686950.10 | 0703012 | 0728001 | 0755014 | 0815001 | 0910090A | 0910091A | 0910092A | 0912503 | 0922501 | 0939001 | 0943001 | 0944000 |
| ITEM | | CLEARING AND GRUBBING | RESET MAILBOX | EARTH EXCAVATION | ROCK EXCAVATION | CUT BITUMINOUS CONCRETE PAVEMENT | FORMATION OF SUBGRADE | GRANULAR FILL | SEDIMENTATION CONTROL SYSTEM | SEDIMENT CONTROL SYSTEM AT CATCH BASIN | ROCK IN DRAINAGE TRENCH EXCAVATION 0'-10' DEEP | PROCESSED AGGREGATE BASE | PROCESSED AGGREGATE | HMA S0.5 | HMA S0.375 | MATERIAL FOR TACK COAT | TYPE 'C' CATCH BASIN (4' SUMP) 0' - 10' DEEP | TYPE 'C-L' CATCH BASIN (4' SUMP) - 0' - 10' DEEP | TYPE 'C-L' CATCH BASIN DOUBLE GRATE TYPE 2 (4' SUMP) - 0' - 10' DEEP | OFFSET TYPE 'C-M' CATCH BASIN DOUBLE GRATE TYPE 2 (4' SUMP) - 0' - 10' DEEP | REMOVE DRAINAGE STRUCTURE - 0' - 10' DEEP | 15" R.C. PIPE - 0' - 10' DEEP | 15" HIGH DENSITY POLYETHYLENE PIPE - 0' - 10' DEEP | 15" HIGH DENSITY POLYETHYLENE PIPE END | REMOVE EXISTING PIPE - 0' - 10' DEEP | MODIFIED RIPRAP | CRUSHED STONE FOR SLOPE PROTECTION | GEOTEXTILE (SEPARATION - HIGH SURVIVABILITY) | BITUMINOUS CONCRETE LIP CURBING | STEEL-BACKED TIMBER GUIDERAIL - TYPE A | STEEL-BACKED TIMBER GUIDERAIL - TERMINAL SECTION | STEEL-BACKED TIMBER GUIDERAIL - BRIDGE ATTACHMENT | REMOVE METAL BEAM RAIL | BITUMINOUS CONCRETE DRIVEWAY | SWEEPING FOR DUST CONTROL | WATER FOR DUST CONTROL | FURNISHING AND PLACING TOPSOIL |
| UNIT | | L.S. | EA. | C.Y. | C.Y. | L.F. | S.Y. | C.Y. | L.F. | EA. | C.Y. | C.Y. | TON | TON | GAL. | EA. | EA. | EA. | EA. | EA. | L.F. | L.F. | EA. | L.F. | C.Y. | TON | S.Y. | L.F. | L.F. | EA. | EA. | L.F. | S.Y. | HR. | M.GAL. | S.Y. | |
| | | 2 | 300 | 14 | 70 | 630 | 10 | 430 | 7 | 5 | 160 | 55 | 90 | 80 | 72 | 4 | 1 | 1 | 1 | 1 | 160 | 58 | 2 | 20 | 12 | 12 | 30 | 260 | 105 | 2 | 2 | 215 | 250 | 8 | 55 | 490 | |
| SUBTOTAL | | 2 | 300 | 14 | 70 | 630 | 10 | 430 | 7 | 5 | 160 | 55 | 90 | 80 | 72 | 4 | 1 | 1 | 1 | 1 | 160 | 58 | 2 | 20 | 12 | 12 | 30 | 260 | 105 | 2 | 2 | 215 | 250 | 8 | 55 | 490 | |
| UNASSIGNED | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| TOTAL | | 2 | 300 | 14 | 70 | 630 | 10 | 430 | 7 | 5 | 160 | 55 | 90 | 80 | 72 | 4 | 1 | 1 | 1 | 1 | 160 | 58 | 2 | 20 | 12 | 12 | 30 | 260 | 105 | 2 | 2 | 215 | 250 | 8 | 55 | 490 | |

| | | HIGHWAY | | | | | | | | | | | | | | TRAFFIC | | | | | | | | | | | | | | | | | | |
|-------------|--|--|---|--|---|--------------------------------|---------------------------------|-----------------------------|---------------------------------|--|----------------------------------|-----------------------------------|------------------------|------|---------------------------|-------------------------------------|-----------------------------------|---------------------------------------|---|--------------|---------------------------------|-----------------------------|--------------------------------------|---|--------------------------------------|-----------------------|--|--|---|------------------------------|--------------------|--------------------------------|---------------------------------|---------------------------------|
| ITEM NUMBER | | 0949087 | 0949099 | 0949226 | 0949831 | 0950039 | 0950040A | 0950043A | 0952001 | 0952051A | 0959060A | 0975004 | 0980020 | | 0822100.01A | 0822101.01A | 0970007 | 0971001A | 0976002 | 0978002 | 0979003 | 1020030A | 1106001 | 1107007 | 1118101A | 1205217 | 1206023A | 1209124 | 1209131 | 1211001 | 1220027 | 1802210.07 | 1802210.14 | 1802210.21 |
| ITEM | | CLETHRA ALNIFOLIA SUMMERSWEET 3'-4' HT. B.B. | CORNUS AMOMUM SILKY DOGWOOD 18"-24" HT. CONTAINER | ILEX VERTICILLATA COMMON WINTERBERRY 18"-24" HT. BB. | ACER RUBRUM RED MAPLE 1 3/4"-2" CAL. B.B. | EROSTON CONTROL MATTING TYPE D | CONSERVATION SEEDING FOR SLOPES | WETLAND GRASS ESTABLISHMENT | SELECTIVE CLEARING AND THINNING | CONTROL AND REMOVAL OF INVASIVE VEGETATION | CONSTRUCTION FIELD OFFICE, SMALL | MOBILIZATION AND PROJECT CLOSEOUT | CONSTRUCTION SURVEYING | | TEMPORARY TRAFFIC BARRIER | RELOCATED TEMPORARY TRAFFIC BARRIER | TRAFFICPERSON (UNIFORMED FLAGGER) | MAINTENANCE AND PROTECTION OF TRAFFIC | BARRICADE WARNING LIGHTS - HIGH INTENSITY | TRAFFIC DRUM | CONSTRUCTION BARRICADE TYPE III | TEMPORARY ILLUMINATION UNIT | 1 WAY PEDESTRIAN SIGNAL POLE MOUNTED | PEDESTRIAN PUSH BUTTON AND SIGN (PIEZO) | TEMPORARY SIGNALIZATION (SITE NO. 1) | TYPE DE-7D DELINEATOR | REMOVAL AND RELOCATION OF EXISTING SIGNS | HOT-APPLIED PAINTED PAVEMENT MARKINGS 4" WHITE | HOT-APPLIED PAINTED LEGEND, ARROWS AND MARKINGS | REMOVAL OF PAVEMENT MARKINGS | CONSTRUCTION SIGNS | TEMPORARY SAND BARREL (700 LB) | TEMPORARY SAND BARREL (1400 LB) | TEMPORARY SAND BARREL (2100 LB) |
| UNIT | | EA. | EA. | EA. | EA. | S.Y. | S.Y. | S.F. | L.S. | S.Y. | MO. | L.S. | L.S. | L.F. | L.F. | HR. | L.S. | DAY | EA. | EA. | EA. | EA. | EA. | L.S. | EA. | L.S. | L.F. | S.F. | S.F. | S.F. | EA. | EA. | EA. | |
| | | 10 | 10 | 8 | 8 | 180 | 430 | 175 | L.S. | 470 | 8 | L.S. | L.S. | | 180 | 140 | 240 | L.S. | 3650 | 17 | 2 | 6 | 2 | 2 | L.S. | 8 | L.S. | 355 | 20 | 120 | 280 | 2 | 8 | 4 |
| SUBTOTAL | | 10 | 10 | 8 | 8 | 180 | 430 | 175 | L.S. | 470 | 8 | L.S. | L.S. | | 180 | 140 | 240 | L.S. | 3650 | 17 | 2 | 6 | 2 | 2 | L.S. | 8 | L.S. | 355 | 20 | 120 | 280 | 2 | 8 | 4 |
| UNASSIGNED | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| TOTAL | | 10 | 10 | 8 | 8 | 180 | 430 | 175 | L.S. | 470 | 8 | L.S. | L.S. | | 180 | 140 | 240 | L.S. | 3650 | 17 | 2 | 6 | 2 | 2 | L.S. | 8 | L.S. | 355 | 20 | 120 | 280 | 2 | 8 | 4 |

P = FEDERAL AID PARTICIPATING
 NP = FEDERAL AID NON-PARTICIPATING
 * = FOR INTERNAL USE ONLY,
 NOT A LEGAL DEFINITION

| REV. | DATE | DESCRIPTION |
|------|------|-------------|
| | | |
| | | |
| | | |

| | | STRUCTURES | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|-------------|--|--|---|--|------------------------|----------------|-----------------------------|---------------|----------|-----------|---------------------------|------------------|---------------------------|--|--|---------------------------------------|--------------------------|------------------|----------------------------|----------------------|------------------|------------------------|----------------------------------|------------------------------|------------------------------|----------------|------------------------------------|------------|----------------------------------|----------------------|-----------------------------|
| ITEM NUMBER | | 0220903A | 0203202 | 0203304 | 0204001 | 0204151A | 0216000 | 0216011A | 0406171 | 0406173 | 0406303 | 0502195A | 0503001 | 0514202 | 0514222 | 0520036 | 0521001A | 0601062 | 0601064 | 0601118 | 0601121 | 0601123 | 0602030 | 0603233A | 0603474A | 0605003A | 061001 | 0702026 | 0702027 | 0702028 | 0702029 |
| ITEM | | LEAD COMPLIANCE FOR MISCELLANEOUS EXTERIOR TASKS | STRUCTURE EXCAVATION - EARTH (EXCL. COFFERDAM & DEWATERING) | STRUCTURE EXCAVATION - ROCK (EXCL. COFFERDAM & DEWATERING) | COFFERDAM & DEWATERING | HANDLING WATER | PERVIOUS STRUCTURE BACKFILL | FLOWABLE FILL | HMA S0.5 | HMA S0.25 | SAWING AND SEALING JOINTS | TEMPORARY BRIDGE | REMOVAL OF SUPERSTRUCTURE | PRESTRESSED DECK UNITS (3'-0" X 1'-3") | PRESTRESSED DECK UNITS (4'-0" X 1'-3") | ASPHALTIC PLUG EXPANSION JOINT SYSTEM | ELASTOMERIC BEARING PADS | FOOTING CONCRETE | ABUTMENT AND WALL CONCRETE | BRIDGE DECK CONCRETE | PARAPET CONCRETE | APPROACH SLAB CONCRETE | DEFORMED STEEL BARS - GALVANIZED | GALVANIZING STRUCTURAL STEEL | METALLIZING STRUCTURAL STEEL | MASONRY FACING | DRILLING HOLES AND BONDING ANCHORS | MICROPILES | VERIFICATION TEST FOR MICROPILES | MICROPILE PROOF TEST | MICROPILE LENGTH ADJUSTMENT |
| UNIT | | L.S. | C.Y. | C.Y. | L.F. | L.S. | C.Y. | C.Y. | TON | TON | L.F. | L.S. | L.S. | L.F. | L.F. | C.F. | C.I. | C.Y. | C.Y. | C.Y. | L.F. | C.Y. | LBS | LS | L.S. | S.F. | EA | EA | EA | EA | L.F. |
| | | L.S. | 496 | 95 | 507 | L.S. | 567 | 109 | 12 | 7 | 48 | L.S. | L.S. | 134 | 179 | 30 | 8463 | 141 | 155 | 33 | 47 | 37 | 79639 | 0 | L.S. | 1900 | 185 | 12 | 2 | 2 | 20 |
| SUBTOTAL | | L.S. | 496 | 95 | 507 | L.S. | 567 | 109 | 12 | 7 | 48 | L.S. | L.S. | 134 | 179 | 30 | 8463 | 141 | 155 | 33 | 47 | 37 | 79639 | 0 | L.S. | 1900 | 185 | 12 | 2 | 2 | 20 |
| UNASSIGNED | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| TOTAL | | L.S. | 496 | 95 | 507 | L.S. | 567 | 109 | 12 | 7 | 48 | L.S. | L.S. | 134 | 179 | 30 | 8463 | 141 | 155 | 33 | 47 | 37 | 79639 | 0 | L.S. | 1900 | 185 | 12 | 2 | 2 | 20 |

| ITEM NUMBER | | 0707009A | 0708001 | 0714999A | 0819002A | 0904051A | 0974001 | | | | | | | | | | | | | | | | | | | | | | | | | |
|-------------|--|--|-------------|----------------------|--|---------------------------------|-----------------------------|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|
| ITEM | | MEMBRANE WATERPROOFING (COLD LIQUID ELASTOMERIC) | DAMPPOOFING | MONITORING STRUCTURE | PENETRATING SEALER PROTECTIVE COMPOUND | 3 TUBE CURB MOUNTED BRIDGE RAIL | REMOVAL OF EXISTING MASNORY | | | | | | | | | | | | | | | | | | | | | | | | | |
| UNIT | | S.Y. | S.Y. | L.S. | S.Y. | L.F. | C.Y. | | | | | | | | | | | | | | | | | | | | | | | | | |
| | | 187 | 427 | L.S. | 110 | 69 | 111 | | | | | | | | | | | | | | | | | | | | | | | | | |
| SUBTOTAL | | 187 | 427 | L.S. | 110 | 69 | 111 | | | | | | | | | | | | | | | | | | | | | | | | | |
| UNASSIGNED | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| TOTAL | | 187 | 427 | L.S. | 110 | 69 | 111 | | | | | | | | | | | | | | | | | | | | | | | | | |

P = FEDERAL AID PARTICIPATING
 NP = FEDERAL AID NON-PARTICIPATING
 * =FOR INTERNAL USE ONLY,
 NOT A LEGAL DEFINITION

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |

**03 - HIGHWAY
INDEX OF DRAWINGS**

| DRAWING NUMBER | DRAWING TITLE | DRAWING NUMBER | DRAWING TITLE |
|----------------|---|----------------|---------------|
| HWY-01 | INDEX OF DRAWINGS | | |
| HWY-02 | EXISTING CONDITIONS SURVEY | | |
| HWY-03 | TYPICAL SECTIONS | | |
| HWY-04 | ROADWAY PLAN | | |
| HWY-05 | ROADWAY PROFILE | | |
| HWY-06 | DRAINAGE, GRADING, AND SEDIMENTATION & EROSION CONTROL PLAN | | |
| HWY-07 | TEMPORARY BYPASS PLAN | | |
| HWY-08 | TEMPORARY BYPASS PROFILE | | |
| HWY-09 | LANDSCAPING PLAN | | |
| HWY-10 TO 13 | MISCELLANEOUS DETAILS 1-4 | | |
| HWY-14 TO 20 | ROADWAY CROSS SECTIONS 1-7 | | |
| | | | |
| | | | |
| | | | |

DESIGNED BY:
FUSS & O'NEILL INC.
146 HARTFORD ROAD
MANCHESTER, CT 06040

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |

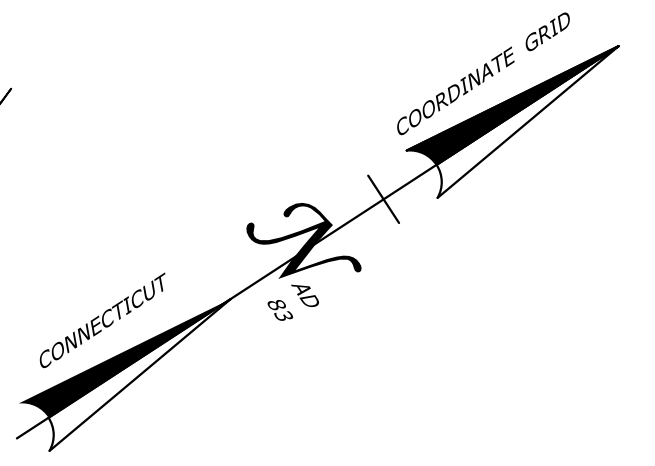
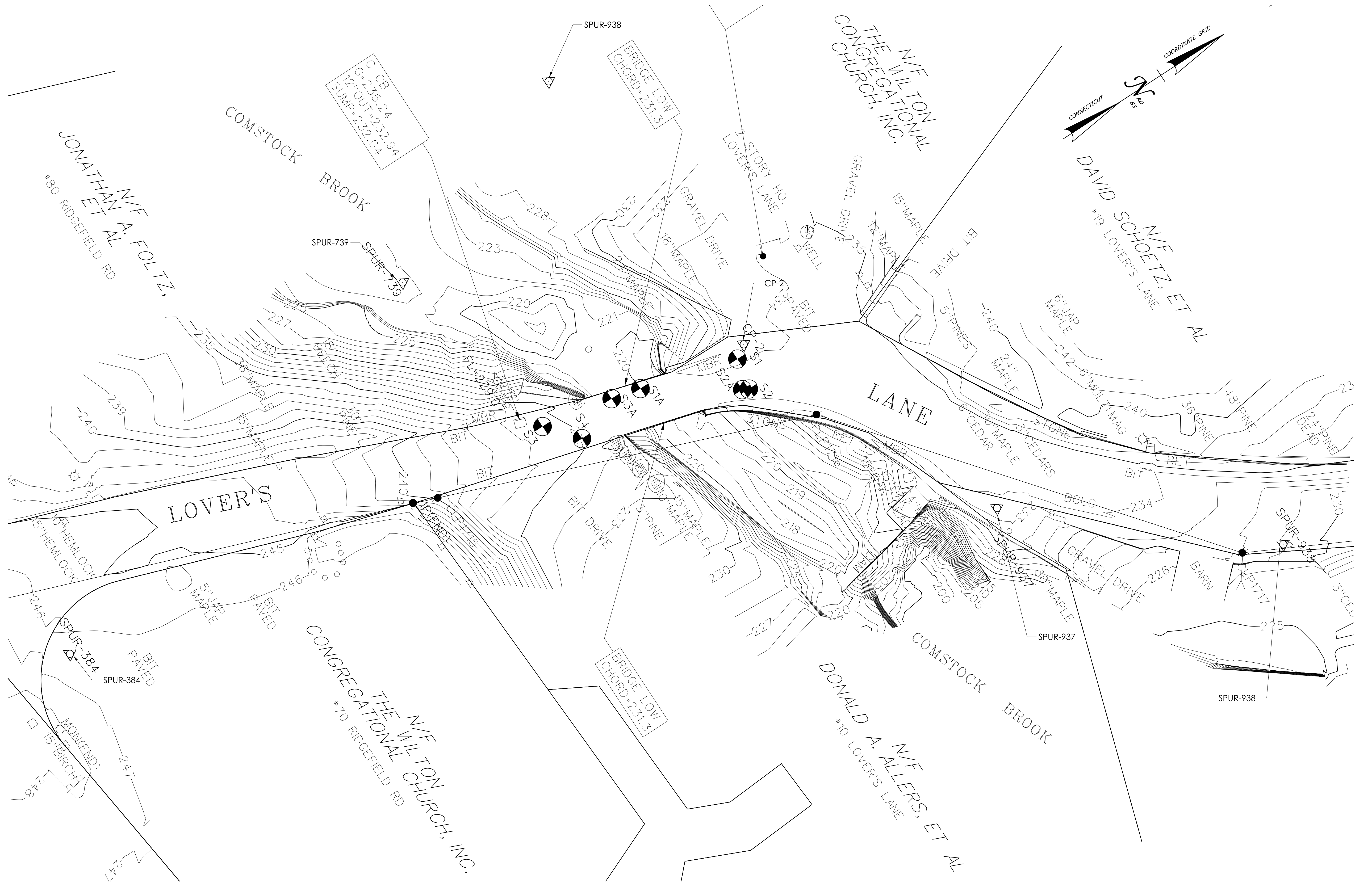
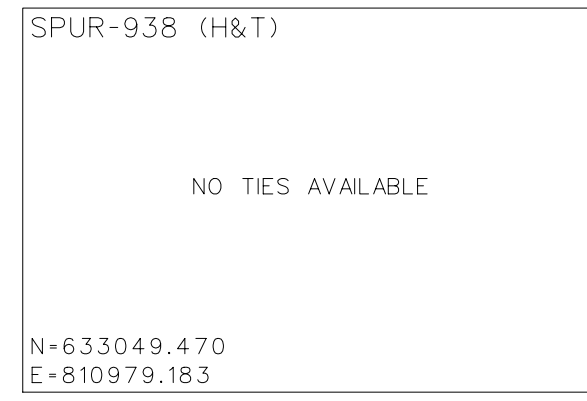
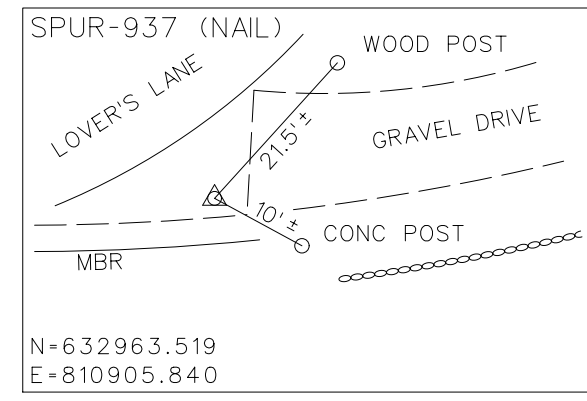
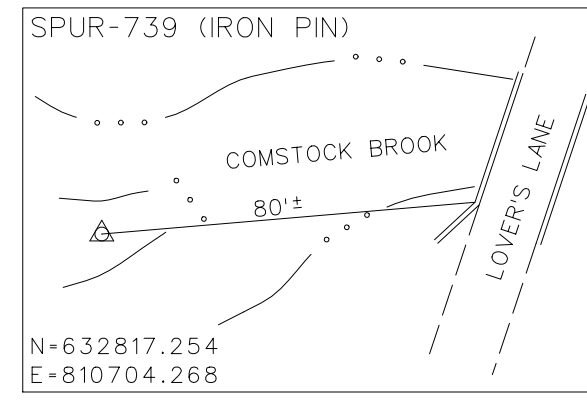
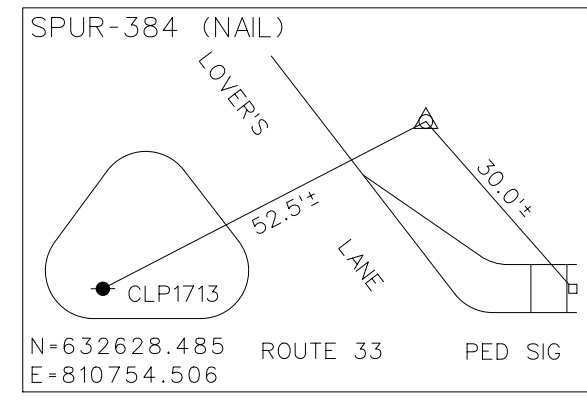
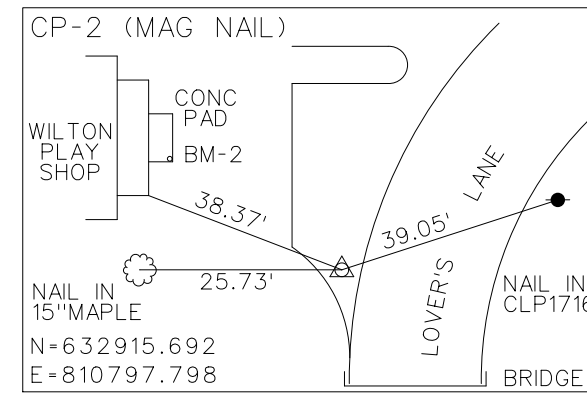
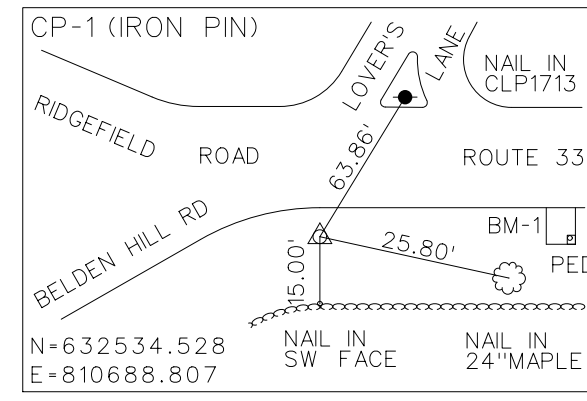
DESIGNER/DRAFTER: J. ALDRICH CHECKED BY: S. BEARCE

SIGNATURE/
BLOCK:

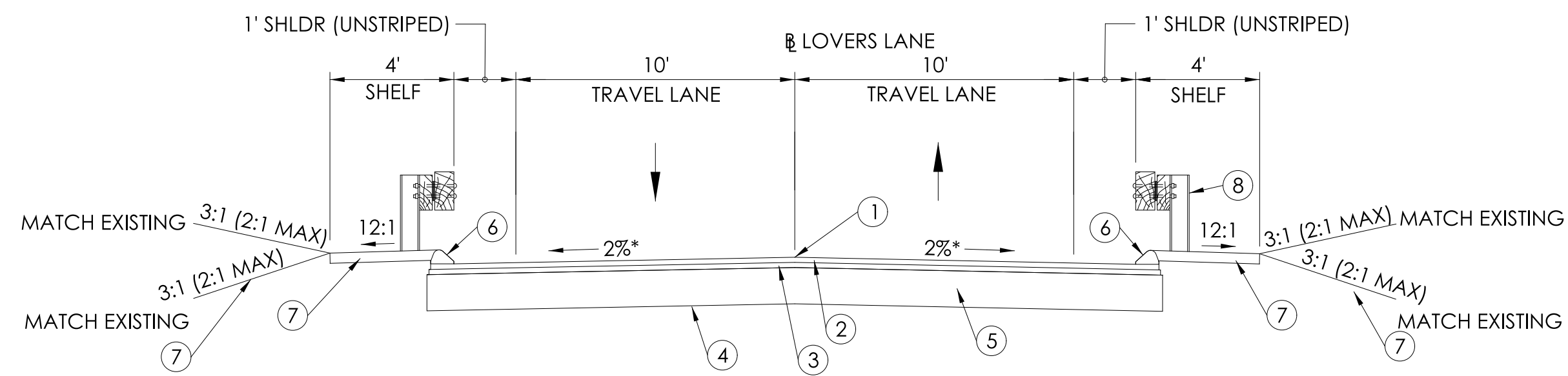


PROJECT NUMBER: 0161-0142
PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVERS LANE OVER COMSTOCK BROOK
TOWN(S): WILTON
DRAWING TITLE: INDEX OF DRAWINGS

DRAWING NO.
HWY-01
SHEET NO.



SURVEY PLAN
SCALE: 1" = 20'-0"



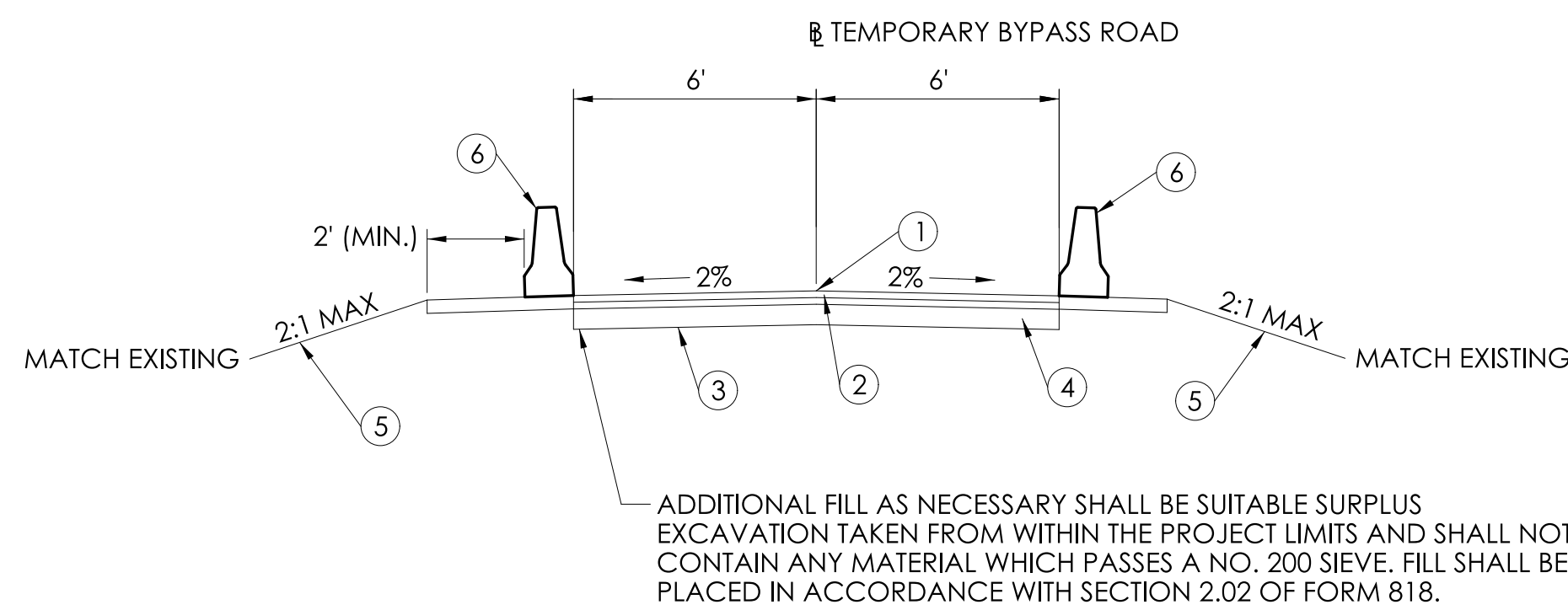
LOVERS LANE - FULL DEPTH PAVEMENT RECONSTRUCTION
 FROM STA. 100+75.00 TO STA. 101+70.71
 FROM STA. 102+47.48 TO STA. 103+70.00
 NOT TO SCALE

LEGEND

- ① POINT OF APPLICATION OF GRADE
- ② 2" HMA S0.375 (TRAFFIC LEVEL 2)
- ③ 2" HMA S0.5 (TRAFFIC LEVEL 2)
- ④ FORMATION OF SUBGRADE
- ⑤ 9" PROCESSED AGGREGATE BASE
- ⑥ BITUMINOUS CONCRETE LIP CURBING
- ⑦ 4" TOPSOIL (SEE LANDSCAPING PLAN FOR FINAL RESTORATION TREATMENT) (PROCESSED AGGREGATE IN GUIDERAIL LOCATIONS)
- ⑧ STEEL-BACKED TIMBER GUIDERAIL

*TRANSITION CROSS SLOPE TO STA. 102+50 (LOW PT) AS FOLLOWS:
 - FROM -2% (STA. 102+38) TO -3% (STA. 102+50)
 - FROM -3% (STA. 102+50) TO -2% (STA. 102+75)

PAVEMENT CROSS SLOPE TRANSITION SHALL OCCUR WITHIN LIMITS OF BRIDGE APPROACH SLAB (SEE STRUCTURES SUBSET).



**TEMPORARY BYPASS ROAD
 ALTERNATING ONE-WAY TRAFFIC**
 FROM STA. 50+47.19 TO STA. 51+04.27
 FROM STA. 51+76.98 TO STA. 51+82.78
 NOT TO SCALE

LEGEND

- ① POINT OF APPLICATION OF GRADE
- ② 4" HMA S0.5 (PLACED IN TWO EQUAL LIFTS)
- ③ FORMATION OF SUBGRADE
- ④ 6" (MIN.) PROCESSED AGGREGATE BASE
- ⑤ 4" TOPSOIL AND TURF ESTABLISHMENT
- ⑥ TEMPORARY TRAFFIC BARRIER

NOTES:

1. MATERIAL FOR TACK COAT SHALL BE APPLIED PER SECTION 4.06 - BITUMINOUS CONCRETE.
2. EROSION CONTROL MATTING, TYPE D SHALL BE INSTALLED ON ALL PERMANENT SLOPES STEEPER THAN 3:1
3. TEMPORARY BYPASS ROAD SHALL BE REMOVED AFTER COMPLETION OF BRIDGE CONSTRUCTION AND SITE SHALL BE RESTORED AS SHOWN ON THE CONTRACT PLANS. REMOVAL OF BYPASS ROAD SHALL BE PAID FOR UNDER THE APPLICABLE CONTRACT ITEMS.

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |
| | | |

DESIGNER/DRAFTER: A. VOMACKA CHECKED BY: S. BEARCE

NOT TO SCALE

SIGNATURE/
BLOCK:

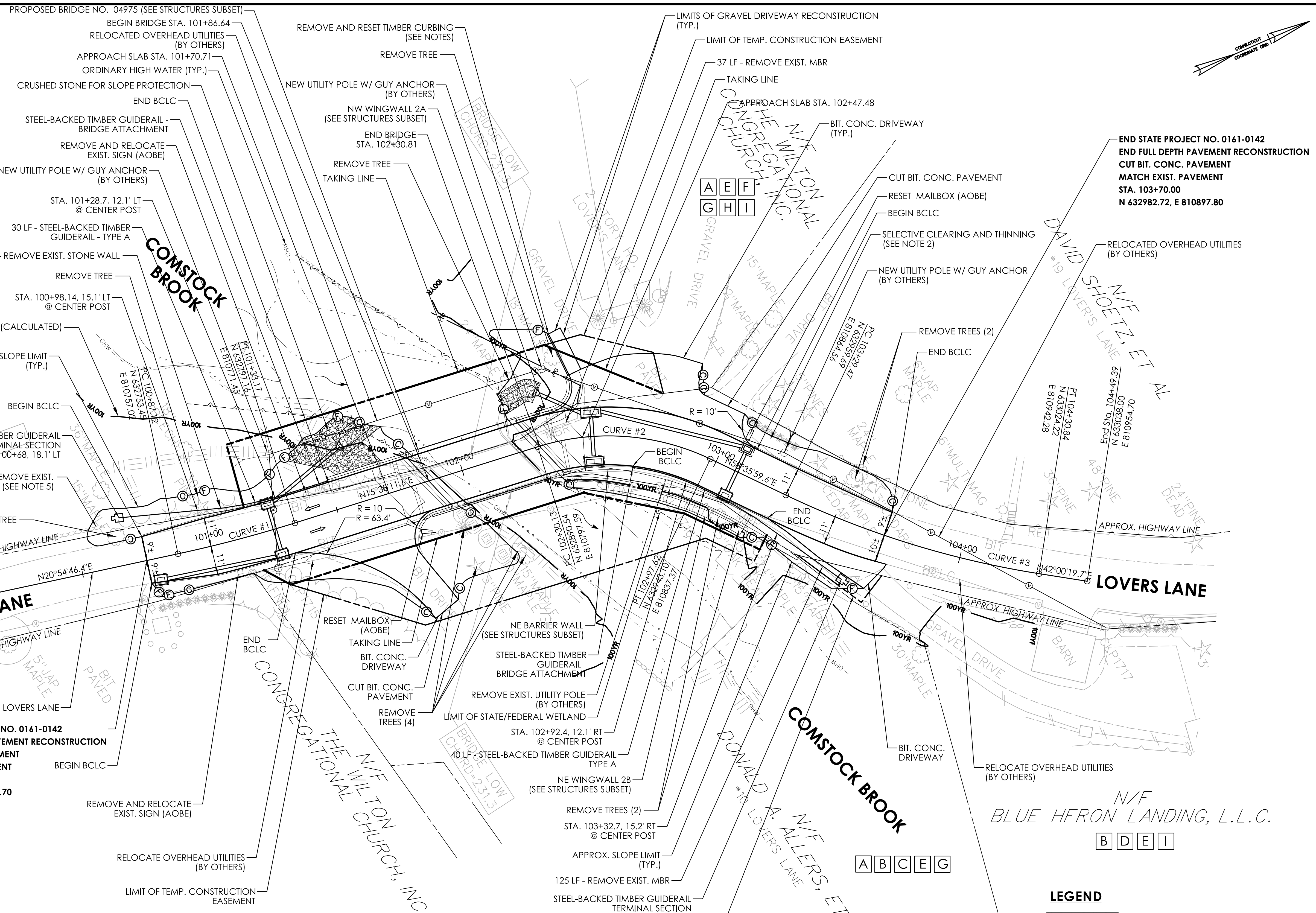


PROJECT NUMBER: 0161-0142
 PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVERS LANE OVER COMSTOCK BROOK
 TOWN(S): WILTON
 DRAWING TITLE: TYPICAL SECTIONS

DRAWING NO.
HWY-03
SHEET NO.

SCHEDULE OF RIGHTS AND EASEMENTS

- A** PARTIAL TAKE REQUIRED.
- B** EASEMENT TO SLOPE FOR THE SUPPORT OF THE HIGHWAY REQUIRED.
- C** EASEMENT TO SLOPE FOR THE SAFETY OF THE HIGHWAY AND REMOVE, USE OR RETAIN EXCAVATED MATERIAL REQUIRED.
- D** EASEMENT TO INSTALL, CONSTRUCT AND MAINTAIN GUIDERAIL AND END ANCHOR REQUIRED.
- E** TEMPORARY CONSTRUCTION EASEMENT REQUIRED.
- F** RIGHT TO GRADE, CONSTRUCT DRIVEWAY AND CURB REQUIRED.
- G** RIGHT TO INSTALL SEDIMENTATION CONTROL SYSTEM REQUIRED.
- H** RIGHT TO INSTALL SIGN REQUIRED.
- I** RIGHT TO INSTALL TEMPORARY BARRICADES REQUIRED.



A C D G

**A E F
G H I**

END STATE PROJECT NO. 0161-0142
 END FULL DEPTH PAVEMENT RECONSTRUCTION
 CUT BIT. CONC. PAVEMENT
 MATCH EXIST. PAVEMENT
 STA. 103+70.00
 N 632982.72, E 810897.80

BEGIN STATE PROJECT NO. 0161-0142
 BEGIN FULL DEPTH PAVEMENT RECONSTRUCTION
 CUT BIT. CONC. PAVEMENT
 MATCH EXIST. PAVEMENT
 STA. 100+75.00
 N 632742.13, E 810752.70

**CURVE DATA
 LOVERS LANE**

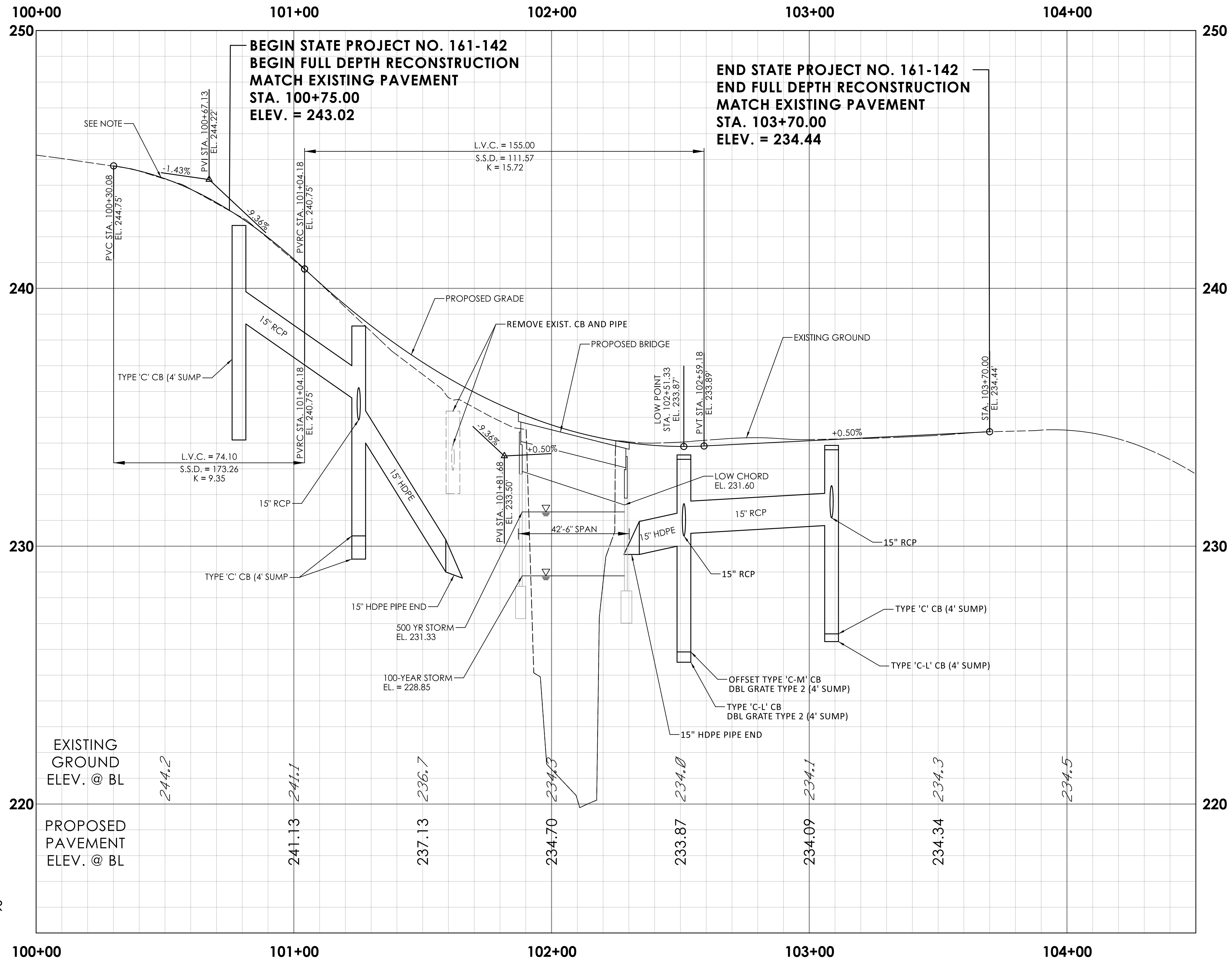
| | | |
|---|---|--|
| <p>CURVE #1 DELTA = 05°16'34.70" LEFT T = 23.039' L = 46.045' R = 500.000' PI N 632774.9759 PI E 810765.2477 PI STA 101+10.16</p> | <p>CURVE #2 DELTA = 42°57'48.00" RIGHT T = 35.419' L = 67.487' R = 90.000' PI N 632924.6451 PI E 810807.1390 PI STA 102+65.55</p> | <p>CURVE #3 DELTA = 16°35'39.90" LEFT T = 51.042' L = 101.370' R = 350.000' PI N 632986.2865 PI E 810908.1235 PI STA 103+80.51</p> |
|---|---|--|

NOTES:

- ALL TREES WITHIN THE APPROX. LIMIT OF SLOPES, AND AS IDENTIFIED ON THIS PLAN, SHALL BE REMOVED UNLESS OTHERWISE DIRECTED BY THE ENGINEER. THIS WORK SHALL BE PAID FOR UNDER ITEM "CLEARING AND GRUBBING".
- ACTUAL LIMITS OF SELECTIVE CLEARING AND THINNING TO BE DETERMINED IN THE FIELD BY THE ENGINEER TO SATISFY OVERHEAD UTILITY CLEARANCE REQUIREMENTS AND FOR INSTALLATION OF TEMPORARY BRIDGE. FROM THE DRIVEWAY AT #19 LOVERS LANE, THIS SHALL BE PERFORMED TO MAXIMIZE SIGHT DISTANCE LOOKING LEFT
- CLEARING AND GRUBBING LIMITS SHALL INCLUDE THE LIMITS OF INVASIVE VEGETATION REMOVAL. SEE LANDSCAPING PLAN.
- FOR PROPOSED DRAINAGE LAYOUT, SEE DRAINAGE, GRADING, AND SEDIMENTATION & EROSION CONTROL PLAN.
- REMOVAL OF EXIST. STONE WALL SHALL BE PAID FOR UNDER ITEM "REMOVAL OF EXISTING MASONRY".
- EXIST. TIMBER CURBING SHALL BE REMOVED AND TEMPORARILY STORED WITHIN PROJECT LIMITS PRIOR TO DISTURBING THE AREA, AND SHALL BE RESET DURING FINAL RESTORATION. THERE WILL BE NO MEASUREMENT OR DIRECT PAYMENT FOR REMOVAL AND RESETTING EXIST. TIMBER CURBING. THE COST OF THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE GENERAL COST OF THE WORK.

LEGEND

- TYPE C RIPRAP APRON
- CRUSHED STONE FOR SLOPE PROTECTION
- AS ORDERED BY ENGINEER



NOTE: PROJECT WORK BEGINS AT 100+75. VERTICAL CURVE SHOWN FOR INFORMATIONAL AND LAYOUT PURPOSES.

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |

DESIGNER/DRAFTER: A. VOMACKA CHECKED BY: S. BEARCE

0 40 80
HORIZONTAL SCALE IN FEET

0 4 8
VERTICAL SCALE IN FEET

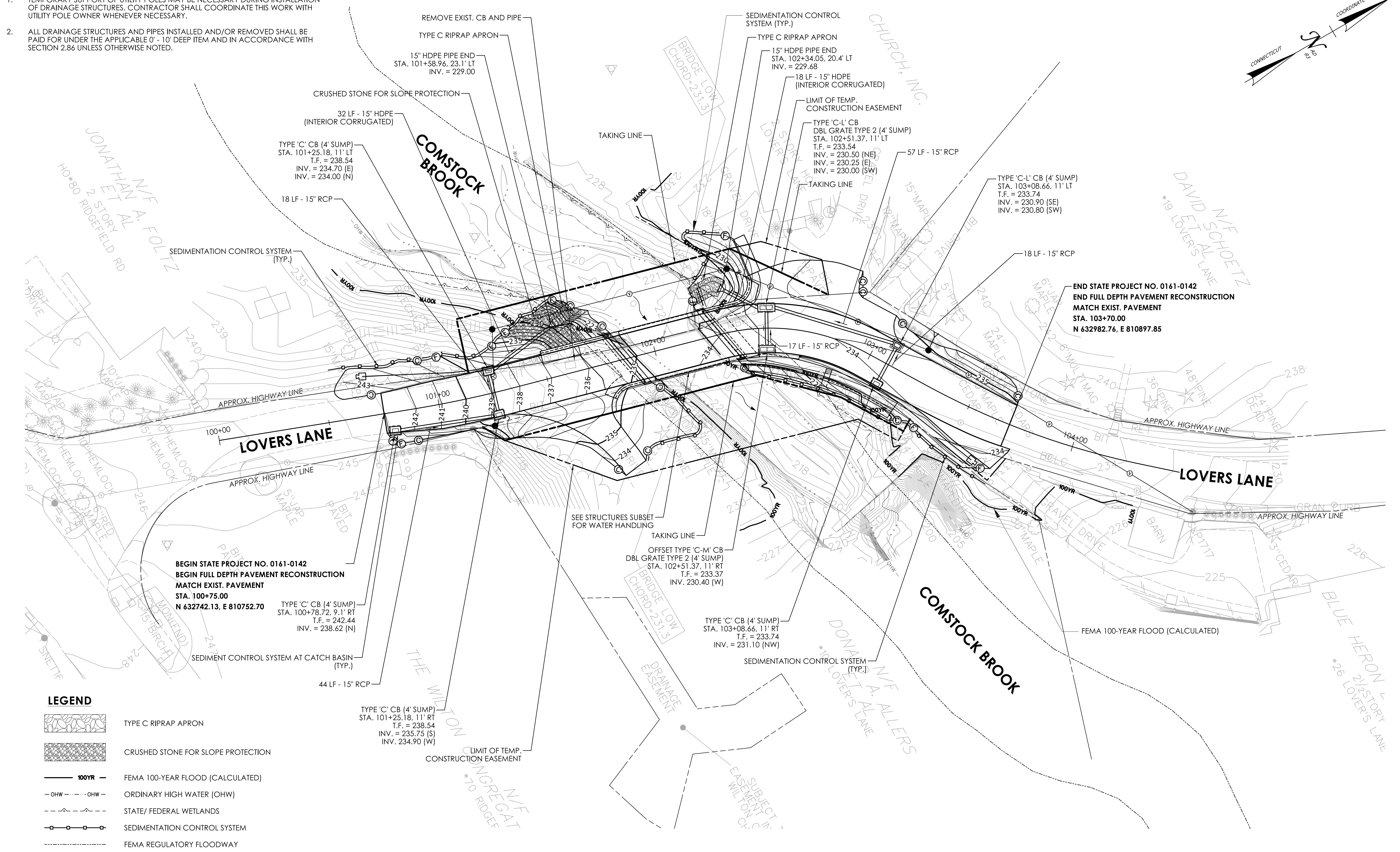
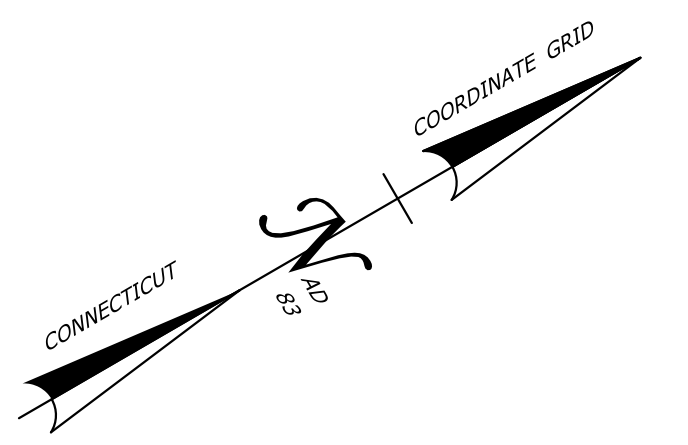
SIGNATURE/BLOCK:



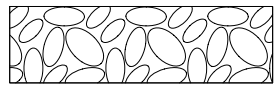
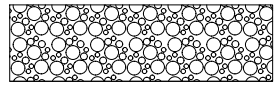
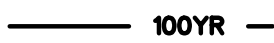
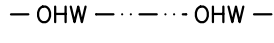



PROJECT NUMBER: 0161-0142
 PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVER'S LANE OVER COMSTOCK BROOK
 TOWN(S): WILTON
 DRAWING TITLE: ROADWAY PROFILE

DRAWING NO. HWY-05
 SHEET NO.

- NOTES:
- TEMPORARY SUPPORT OF UTILITY POLES MAY BE NECESSARY DURING INSTALLATION OF DRAINAGE STRUCTURES. CONTRACTOR SHALL COORDINATE THIS WORK WITH UTILITY POLE OWNER WHENEVER NECESSARY.
 - ALL DRAINAGE STRUCTURES AND PIPES INSTALLED AND/OR REMOVED SHALL BE PAID FOR UNDER THE APPLICABLE 0' - 10' DEEP ITEM AND IN ACCORDANCE WITH SECTION 2.86 UNLESS OTHERWISE NOTED.

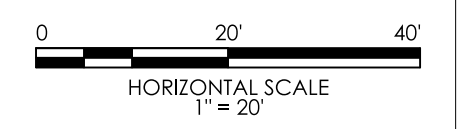


LEGEND

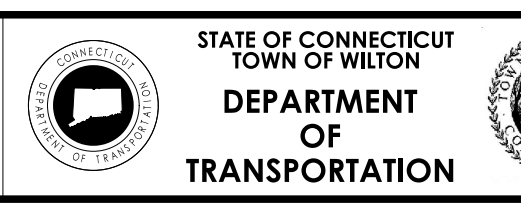
-  TYPE C RIPRAP APRON
-  CRUSHED STONE FOR SLOPE PROTECTION
-  FEMA 100-YEAR FLOOD (CALCULATED)
-  ORDINARY HIGH WATER (OHW)
-  STATE/ FEDERAL WETLANDS
-  SEDIMENTATION CONTROL SYSTEM
-  FEMA REGULATORY FLOODWAY

BEGIN STATE PROJECT NO. 0161-0142
BEGIN FULL DEPTH PAVEMENT RECONSTRUCTION
MATCH EXIST. PAVEMENT
STA. 100+75.00
N 632742.13, E 810752.70

END STATE PROJECT NO. 0161-0142
END FULL DEPTH PAVEMENT RECONSTRUCTION
MATCH EXIST. PAVEMENT
STA. 103+70.00
N 632982.76, E 810897.85

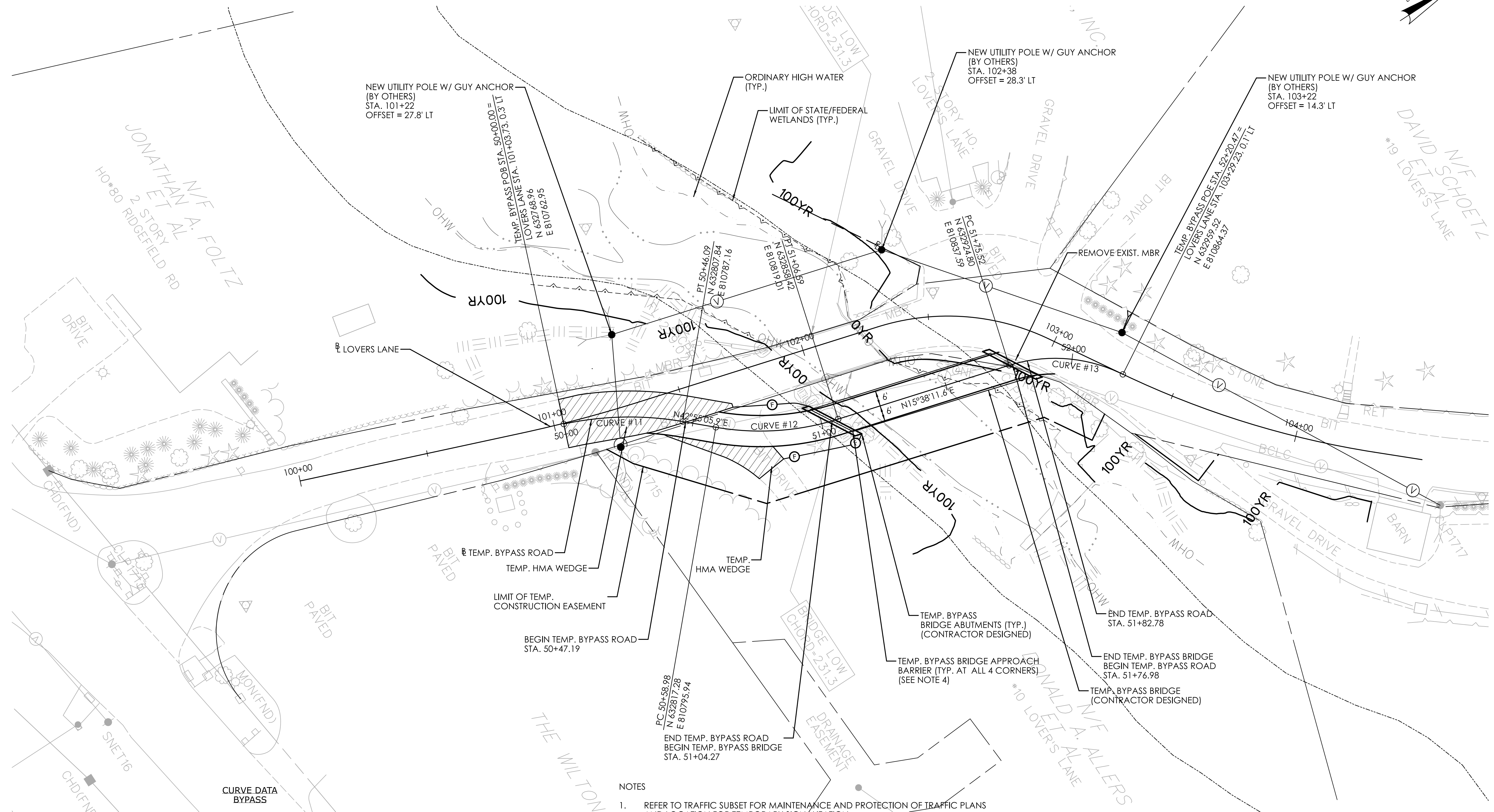
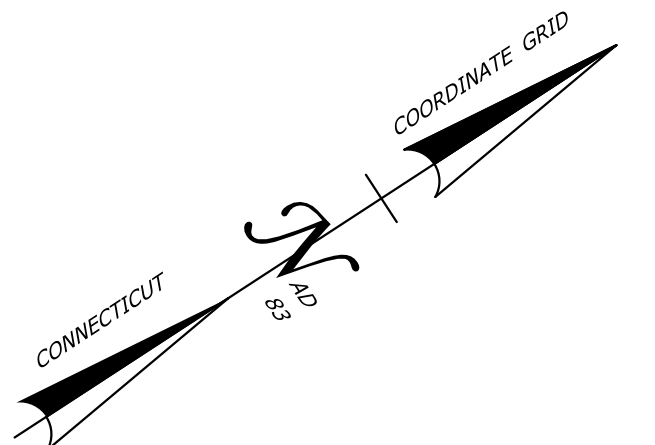


SIGNATURE/
BLOCK:



PROJECT NUMBER: 0161-0142
 PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVERS LANE OVER COMSTOCK BROOK
 TOWN(S): WILTON
 DRAWING TITLE: DRAINAGE, GRADING, AND SEDIMENTATION & EROSION CONTROL PLAN

DRAWING NO.
HWY-06
SHEET NO.



| CURVE DATA BYPASS | | |
|---|--|--|
| CURVE #11 DELTA = 22°00'18.00" RIGHT T = 23.332' L = 46.088' R = 120.000' PI N 632790.7560 PI E 810771.2776 PI STA 50+23.33 | CURVE #12 DELTA = 27°16'55.20" LEFT T = 24.268' L = 47.616' R = 100.000' PI N 632835.0521 PI E 810812.4664 PI STA 50+83.24 | CURVE #13 DELTA = 44°01'30.00" RIGHT T = 23.650' L = 44.950' R = 58.500' PI N 632947.5771 PI E 810843.9614 PI STA 51+99.17 |

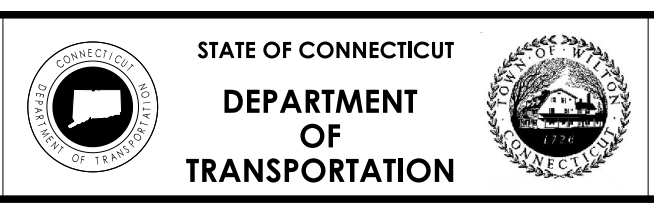
- NOTES
- REFER TO TRAFFIC SUBSET FOR MAINTENANCE AND PROTECTION OF TRAFFIC PLANS AND LOCATION FOR TEMPORARY SIGNALIZATION.
 - SEE TYPICAL SECTIONS FOR TEMPORARY BYPASS ROAD.
 - UPON COMPLETION OF BRIDGE WORK, TEMPORARY BYPASS ROAD AND BRIDGE SHALL BE REMOVED. REMOVAL OF TEMPORARY BYPASS ROAD MATERIALS SHALL BE PAID FOR UNDER APPLICABLE APPLICABLE CONTRACT ITEMS.
 - THE APPROACHES TO THE TEMP. BYPASS BRIDGE WILL REQUIRE BARRIER PROTECTION. THE DESIGN, CONSTRUCTION AND REMOVAL OF THIS APPROACH BARRIER IS INCLUDED IN THE COST FOR THE TEMP. BYPASS BRIDGE. SEE SPECIAL PROVISIONS.

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |

DESIGNER/DRAFTER: A. VOMACKA CHECKED BY: S. BEARCE

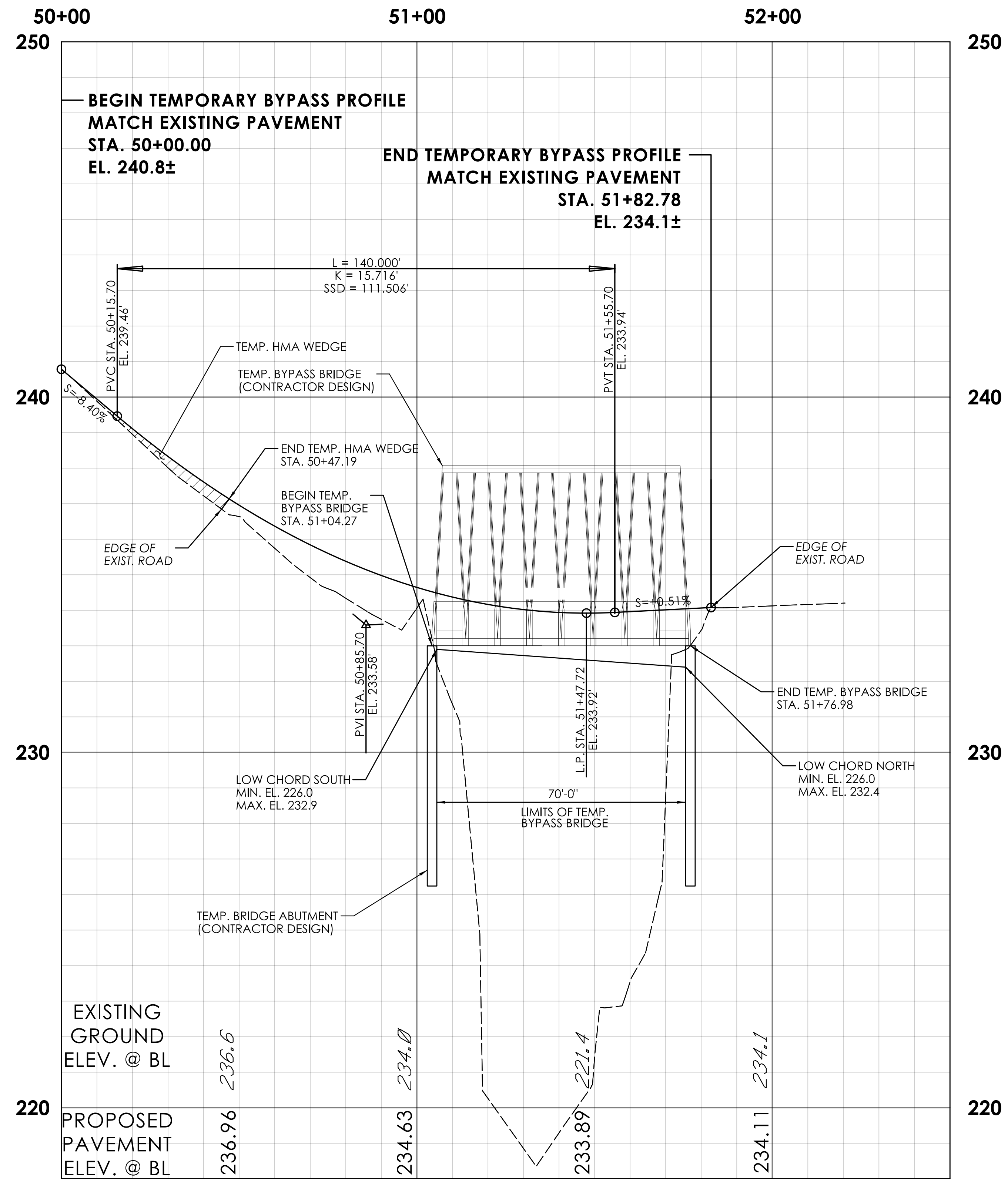
HORIZONTAL SCALE IN FEET: 0 20 40

SIGNATURE/BLOCK: _____



PROJECT NUMBER: 0161-0142
 PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVER'S LANE OVER COMSTOCK BROOK
 TOWN(S): WILTON
 DRAWING TITLE: TEMPORARY BYPASS PLAN

DRAWING NO. HWY-07
 SHEET NO.



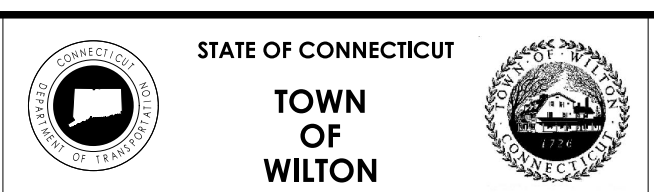
| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |

DESIGNER/DRAFTER: A. VOMACKA CHECKED BY: S. BEARCE

HORIZONTAL SCALE IN FEET: 0, 20, 40

VERTICAL SCALE IN FEET: 0, 2, 4

SIGNATURE/BLOCK:



PROJECT NUMBER: 0161-0142

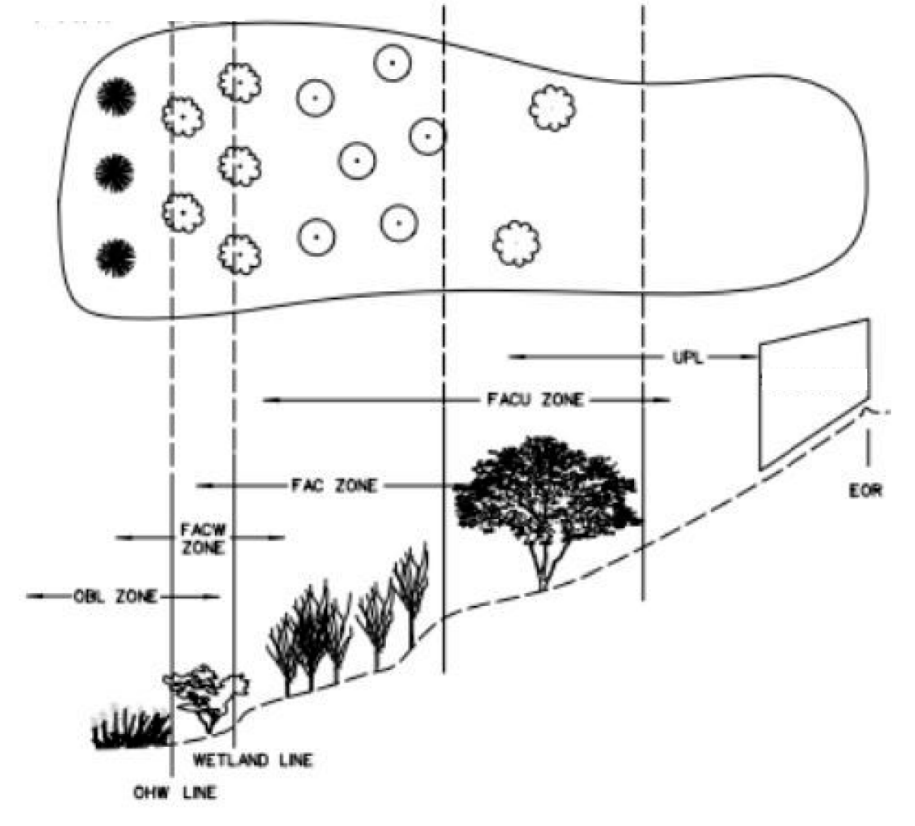
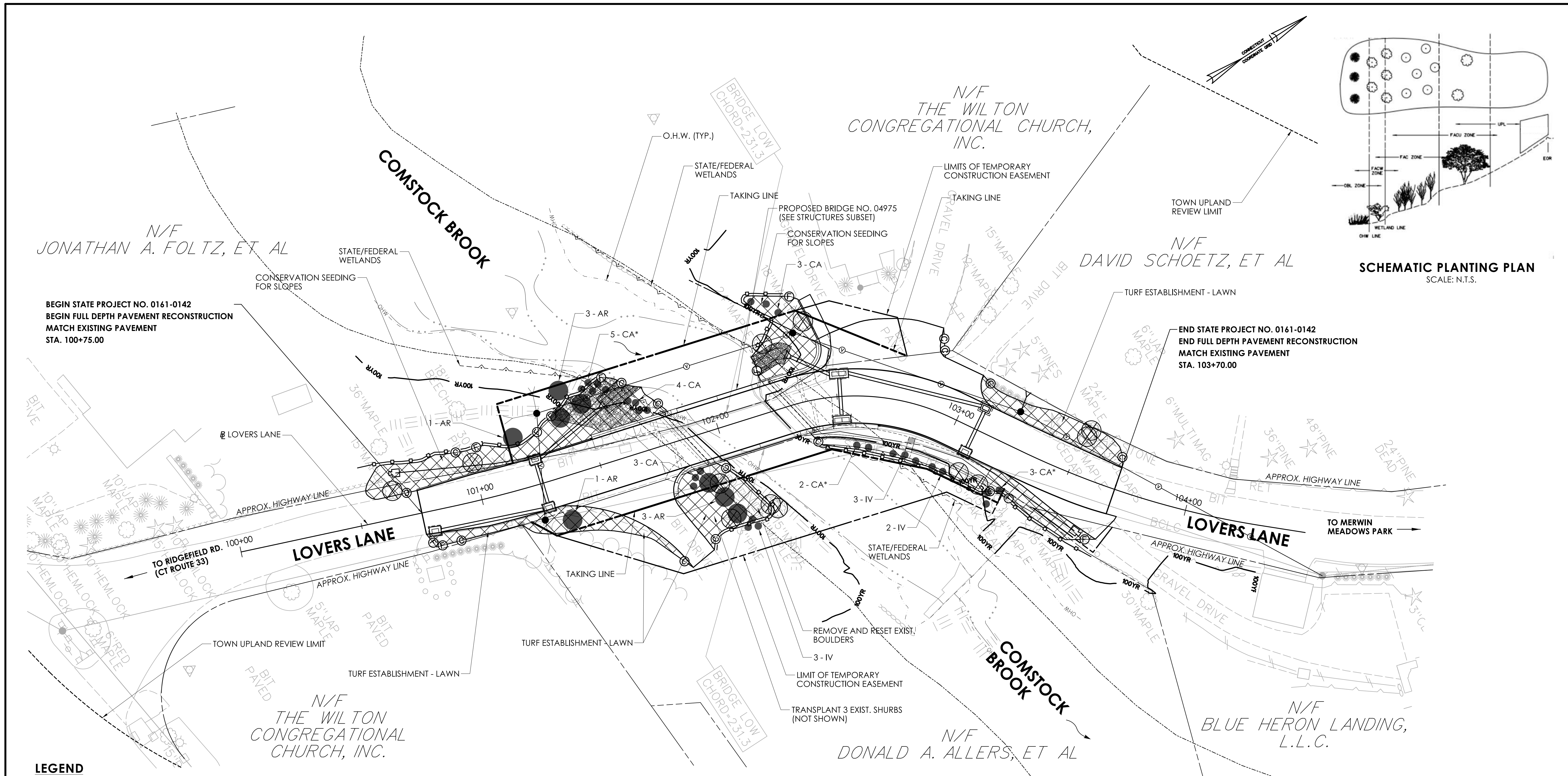
PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVER'S LANE OVER COMSTOCK BROOK

TOWN(S): WILTON

DRAWING TITLE: TEMPORARY BYPASS PROFILE

DRAWING NO. HWY-08

SHEET NO.



SCHEMATIC PLANTING PLAN
SCALE: N.T.S.

BEGIN STATE PROJECT NO. 0161-0142
BEGIN FULL DEPTH PAVEMENT RECONSTRUCTION
MATCH EXISTING PAVEMENT
STA. 100+75.00

END STATE PROJECT NO. 0161-0142
END FULL DEPTH PAVEMENT RECONSTRUCTION
MATCH EXISTING PAVEMENT
STA. 103+70.00

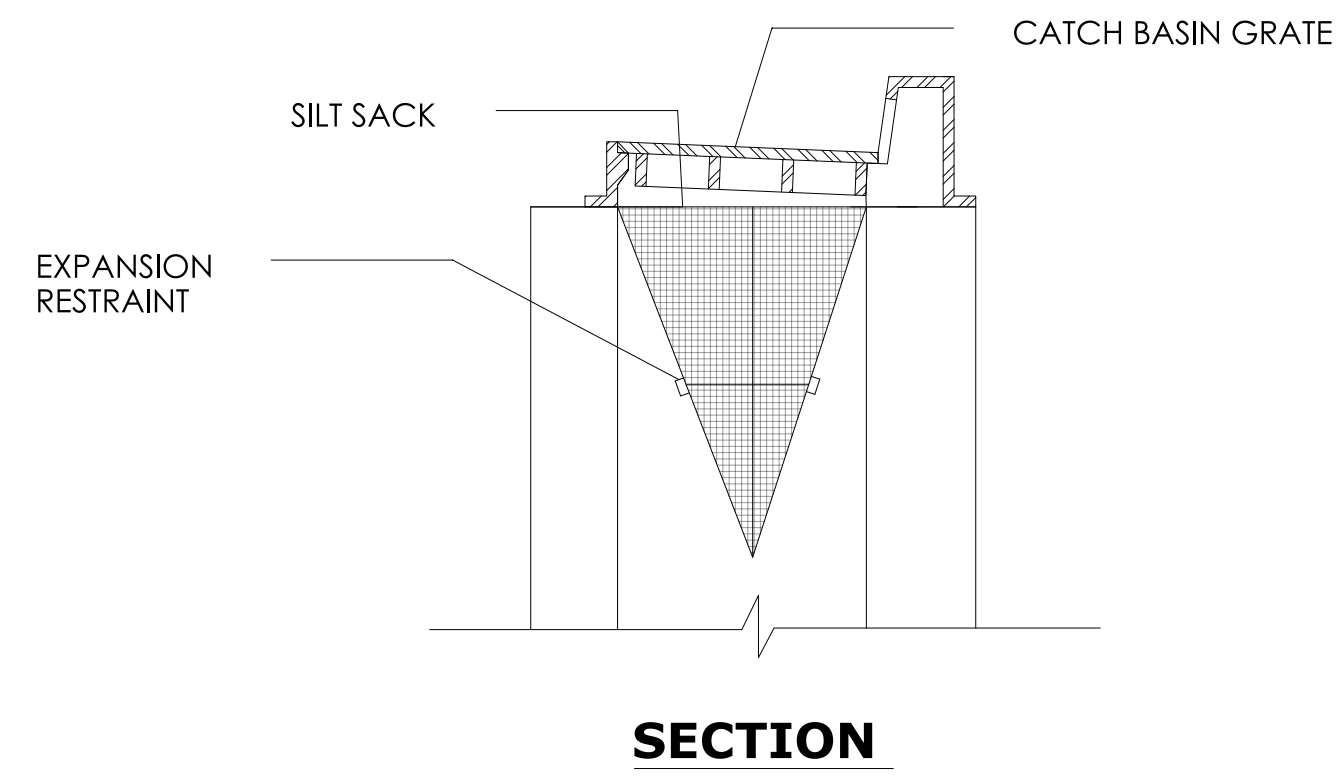
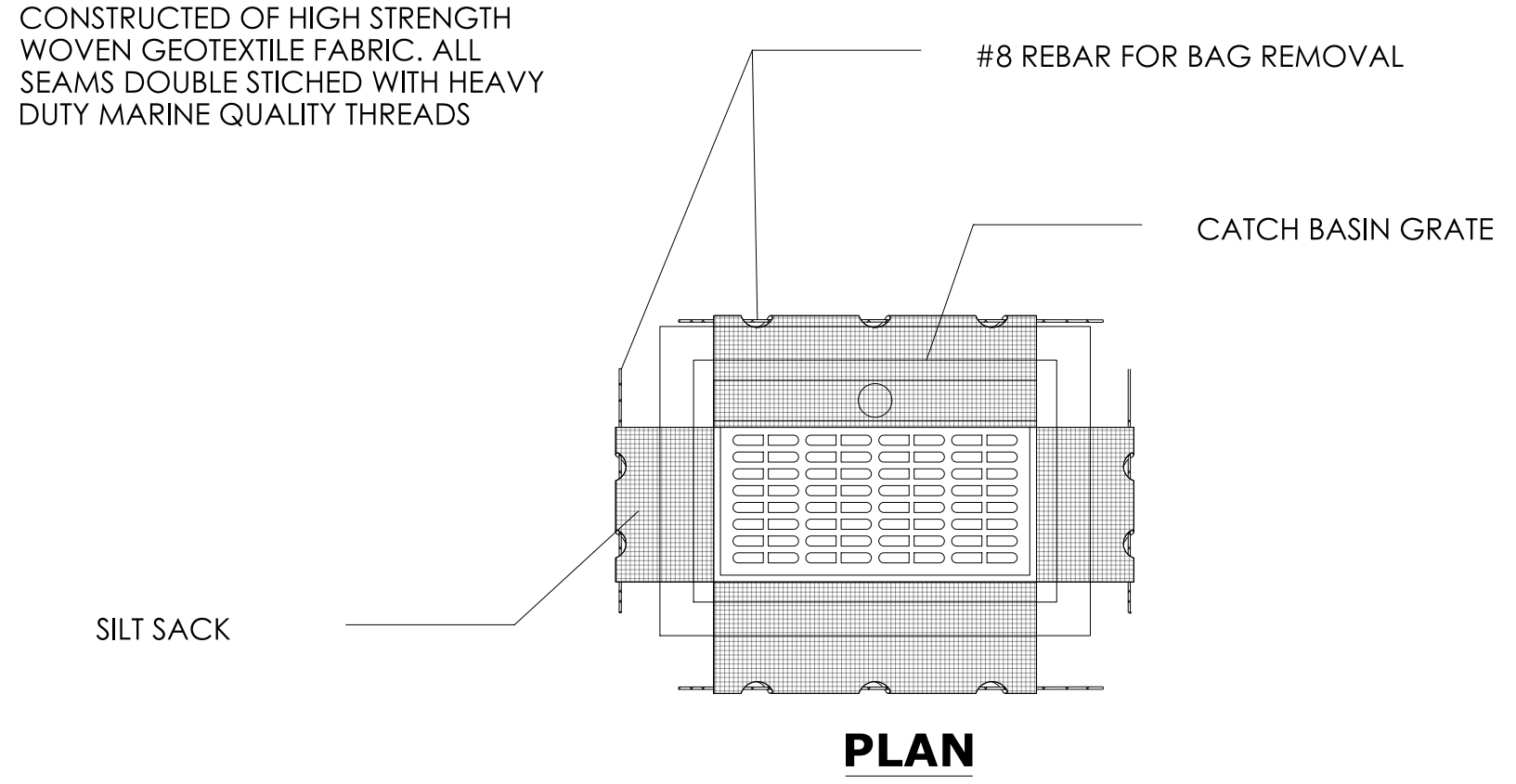
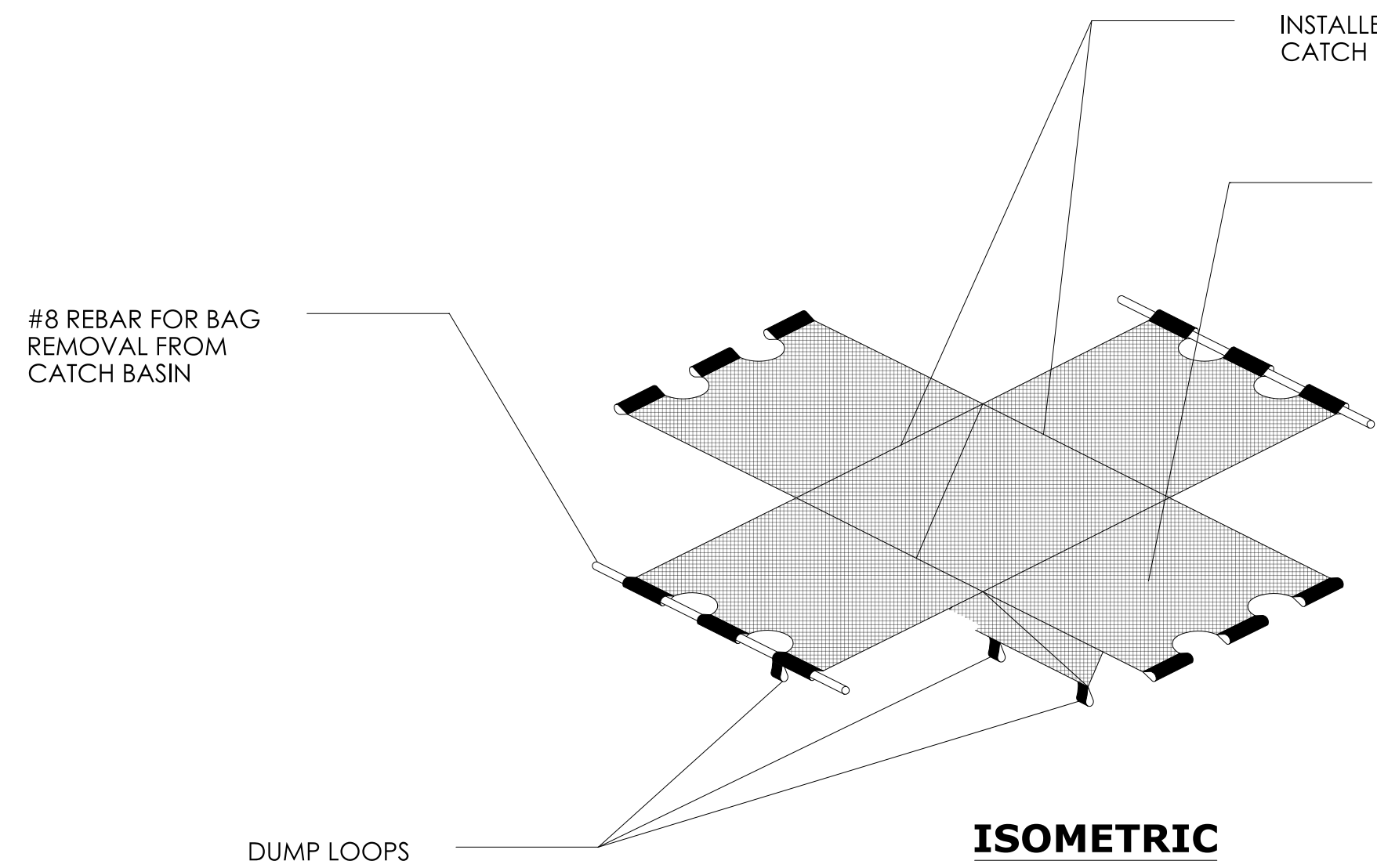
- LEGEND**
- FEMA 100-YEAR FLOOD (CALCULATED)
 - ORDINARY HIGH WATER (OHW)
 - STATE/ FEDERAL WETLANDS
 - SEDIMENTATION CONTROL SYSTEM
 - CONTROL AND REMOVAL OF INVASIVE VEGETATION
 - TYPE C RIPRAP APRON
 - CRUSHED STONE FOR SLOPE PROTECTION
 - PROPOSED TREE
 - PROPOSED SHRUBS
 - EXISTING TREE TO BE REMOVED

PLANT LIST

| KEY | BOTANICAL NAME | COMMON NAME | SIZE | QTY. | SPACING | WETLAND INDICATOR |
|-----|--|-------------------|--------------|-----------|---------------|-------------------|
| AR | ACER RUBRUM | RED MAPLE | 8-10 FT TREE | 8 | FIELD LOCATED | FAC & FACU |
| CA | CLETHRA ALNIFOLIA | SWEET PEPPERBUSH | 3 GAL. CONT. | 10 | FIELD LOCATED | FAC+ |
| CA* | CORNUS AMOMUM | SILKY DOGWOOD | 3 GAL. CONT. | 10 | FIELD LOCATED | FACW |
| IV | ILEX VERTICILLATA | WINTERBERRY HOLLY | 3 GAL. CONT. | 8 | FIELD LOCATED | FACW+ |
| | CONTROL AND REMOVAL OF INVASIVE VEGETATION | | | ±6,000 SF | | |
| | CONSERVATION SEEDING FOR SLOPES | | | ±6,000 SF | | |
| | WETLAND GRASS ESTABLISHMENT | | | ±230 SF | | |

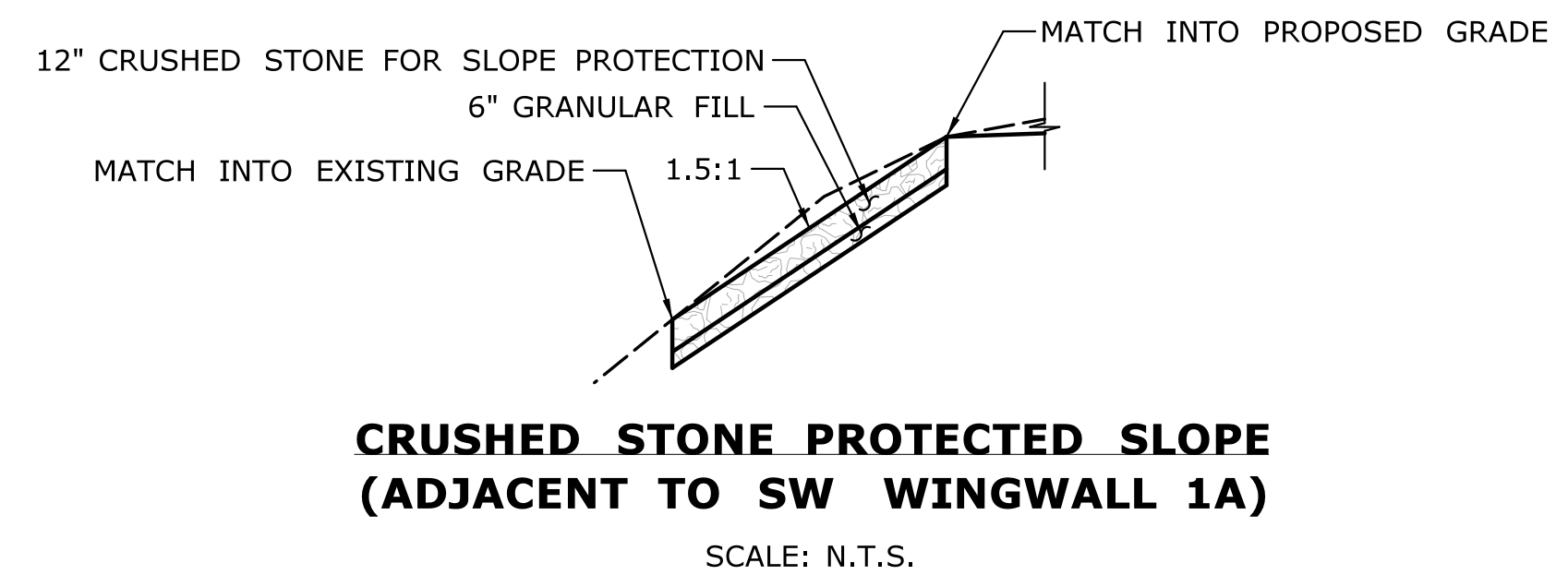
NOTES

- DISTURBED AREAS BELOW THE WETLAND LIMIT SHALL BE SEEDED WITH WETLAND GRASS ESTABLISHMENT. DISTURBED AREAS ABOVE THE WETLAND LIMIT SHALL BE SEEDED WITH CONSERVATION SEEDING FOR SLOPES OR TURF ESTABLISHMENT - LAWN, AS SPECIFIED. ALL DISTURBED AREAS SHALL BE RESTORED.
- ALL PLANT MATERIAL SHALL BE STRAIGHT SPECIES. NO VARIETIES OR CULTIVARS WILL BE ACCEPTED.
- NO PLANTINGS TO BE PLACED IN MOW AREA IN COORDINATION WITH ENGINEER.
- PROJECT AREA TO BE TREATED FOR INVASIVES AND PROPERLY PREPARED FOR FINAL PLANTING, SEEDING, AND RESTORATION.
- REMOVAL AND RESETTING OF EXIST. BOULDERS SHALL BE PAID FOR UNDER ITEM "CLEARING AND GRUBBING" (SEE SPECIAL PROVISIONS).

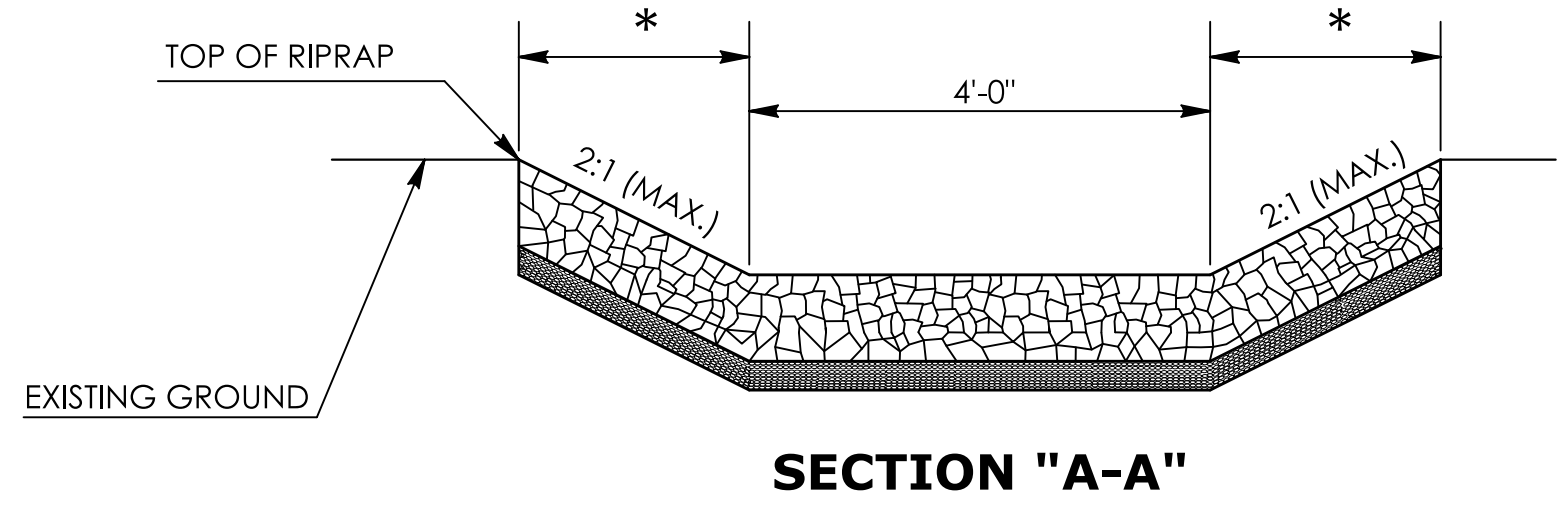


- NOTES:**
1. INSTALL SILT SACKS IN ALL CATCH BASINS BEFORE COMMENCING EXCAVATION AND TRENCHING WORK.
 2. GRATE TO BE PLACED OVER SILT SACK.
 3. SILT SACK SHALL BE INSPECTED PERIODICALLY AND AFTER ALL STORM EVENTS AND CLEANING OR REPLACEMENT SHALL BE PERFORMED PROMPTLY AS NEEDED. MAINTAIN UNTIL DISTURBED AREAS HAVE BEEN PERMANENTLY STABILIZED
 4. SILT SACK TO BE PAID FOR UNDER ITEM "SEDIMENT CONTROL SYSTEM AT CATCH BASIN. SEE SPECIAL PROVISIONS.

SEDIMENT CONTROL SYSTEM AT CATCH BASIN
NOT TO SCALE

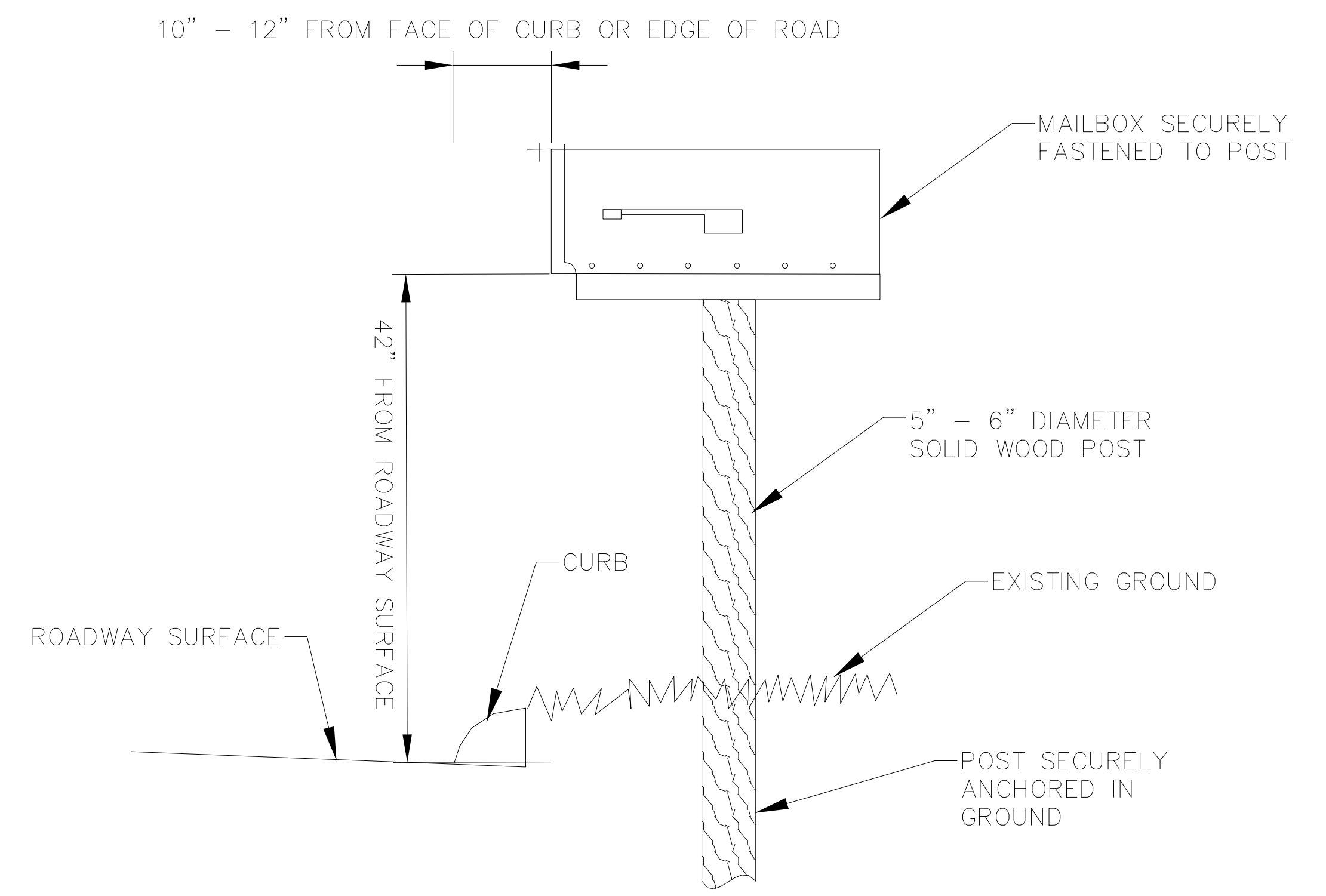


CRUSHED STONE PROTECTED SLOPE (ADJACENT TO SW WINGWALL 1A)
SCALE: N.T.S.



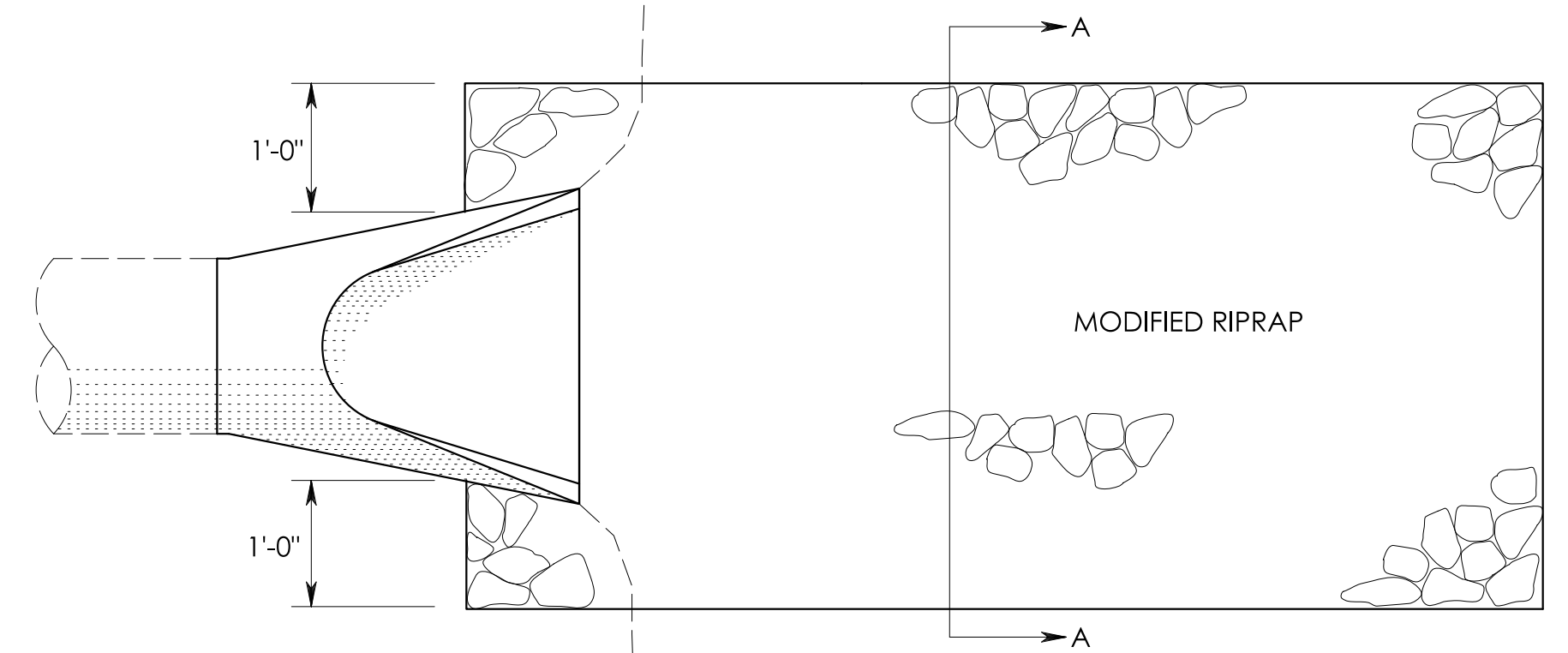
* WIDTH AS REQUIRED

TYPE C RIPRAP APRON

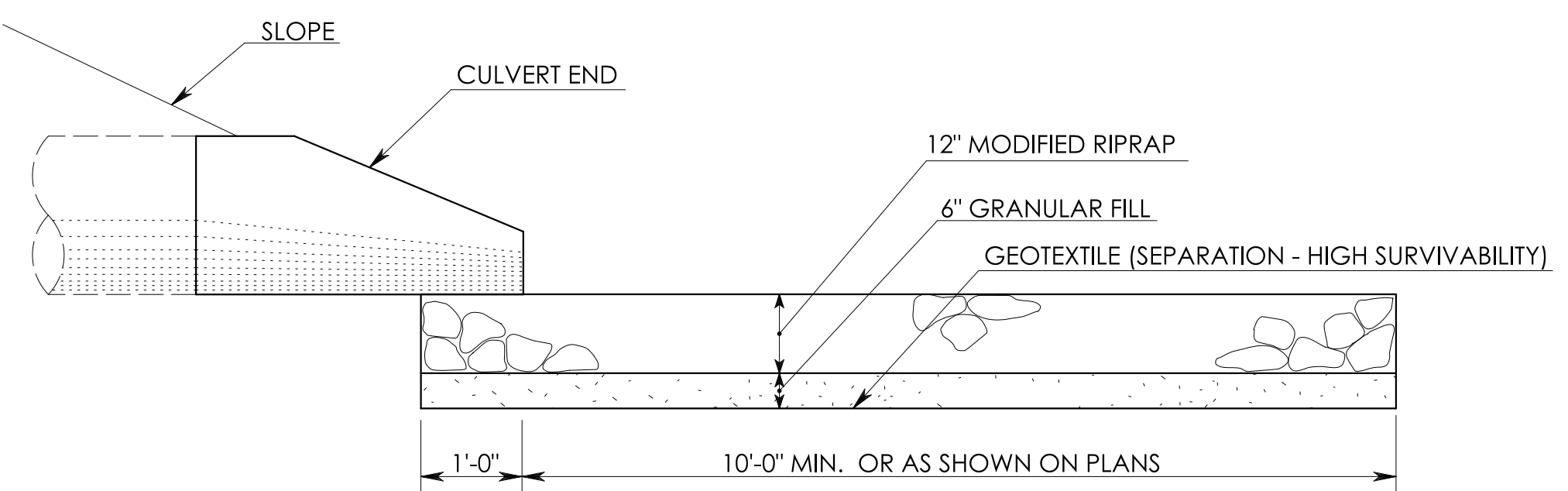


RESET MAILBOX

- NOT TO SCALE
- NOTES:**
1. MAILBOX RESET WILL BE PAID UNDER RESET MAILBOX. SEE SPECIAL PROVISIONS.
 2. PRIOR TO CONSTRUCTION, EXISTING MAILBOX SHALL BE TEMPORARILY RESET OUTSIDE WORK AREA. MAILBOX SHALL BE PERMANENTLY RESET AT ITS ORIGINAL LOCATION AFTER CONSTRUCTION AS ORDERED BY THE ENGINEER.

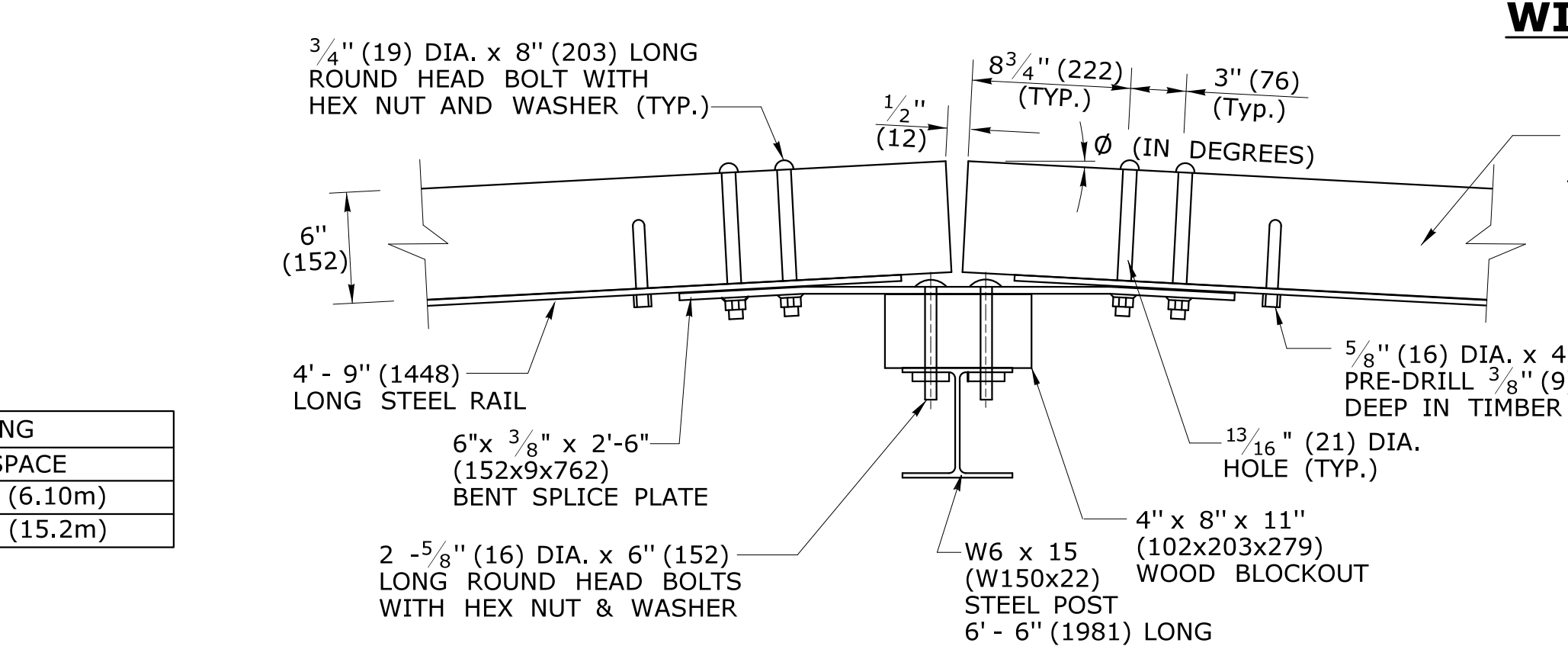
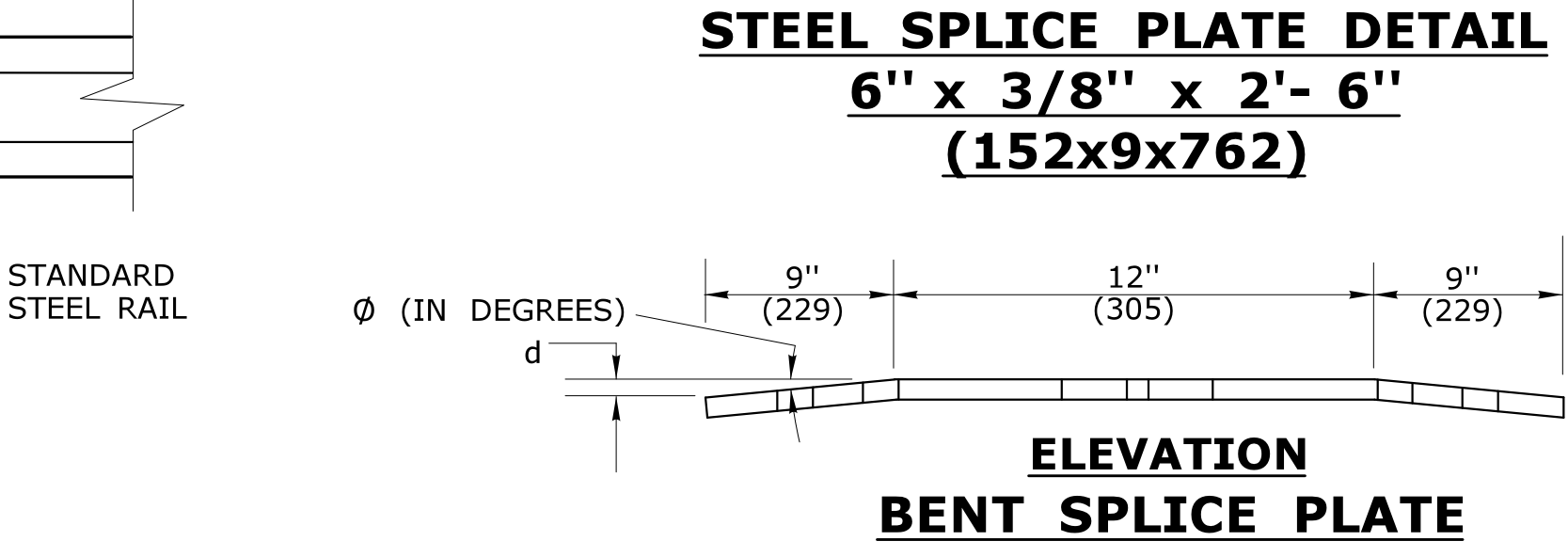
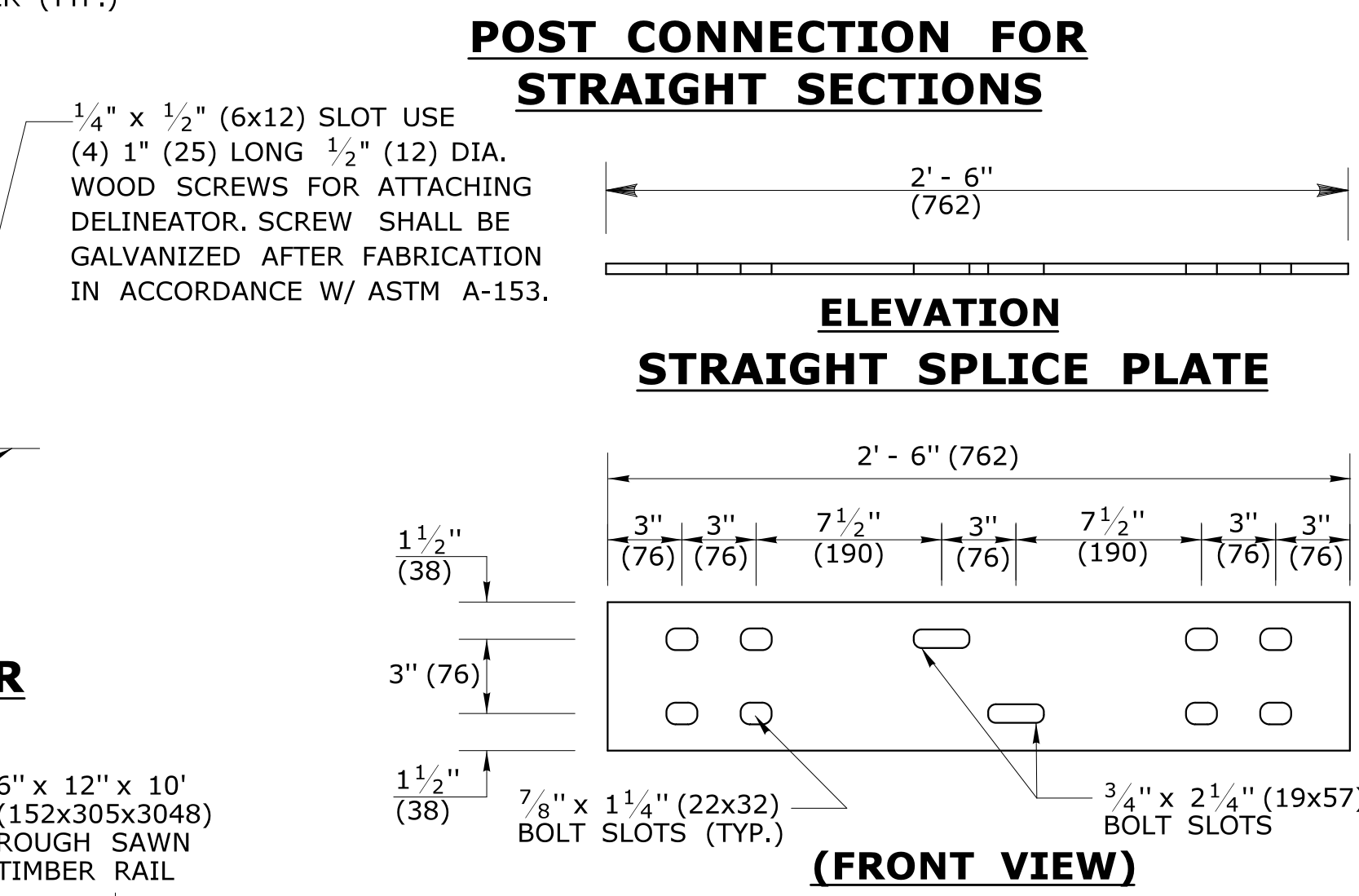
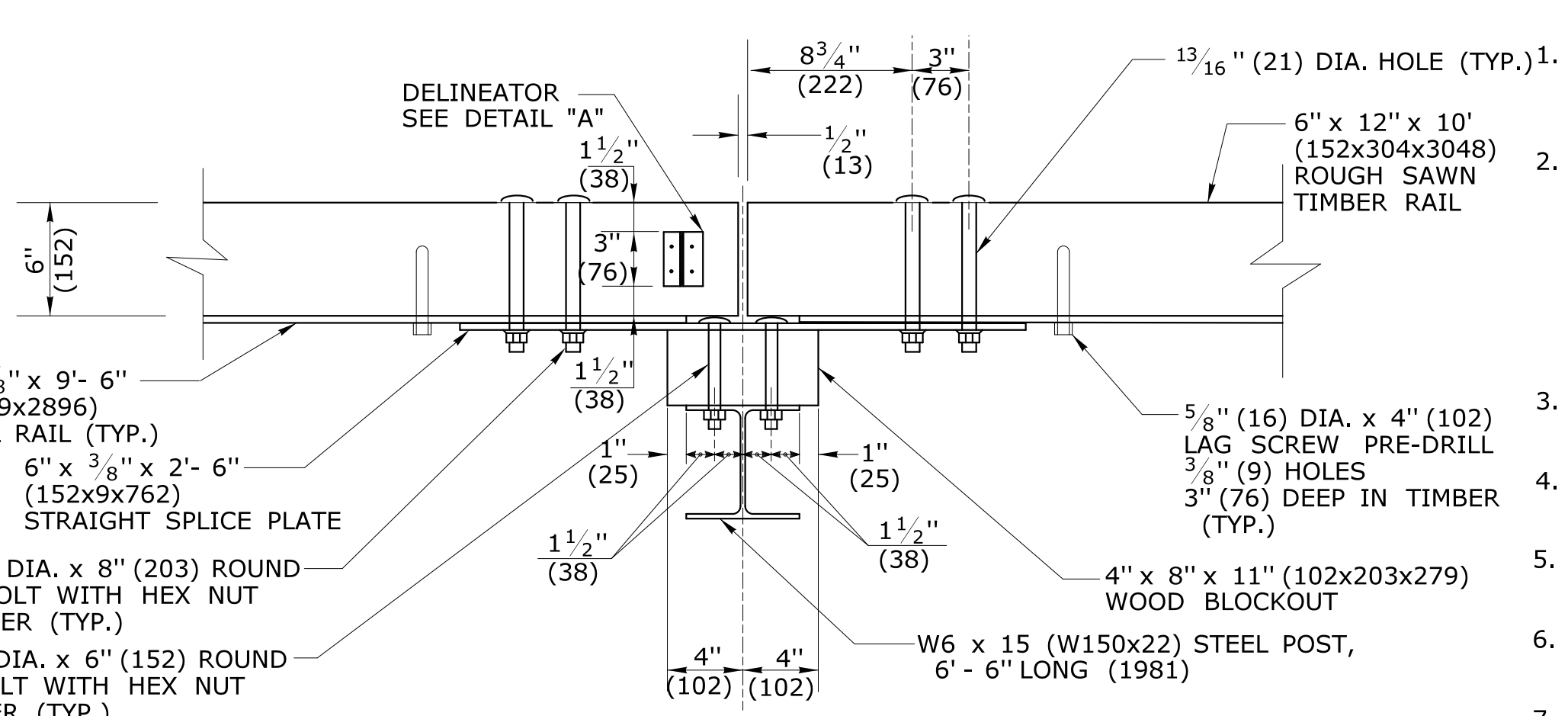
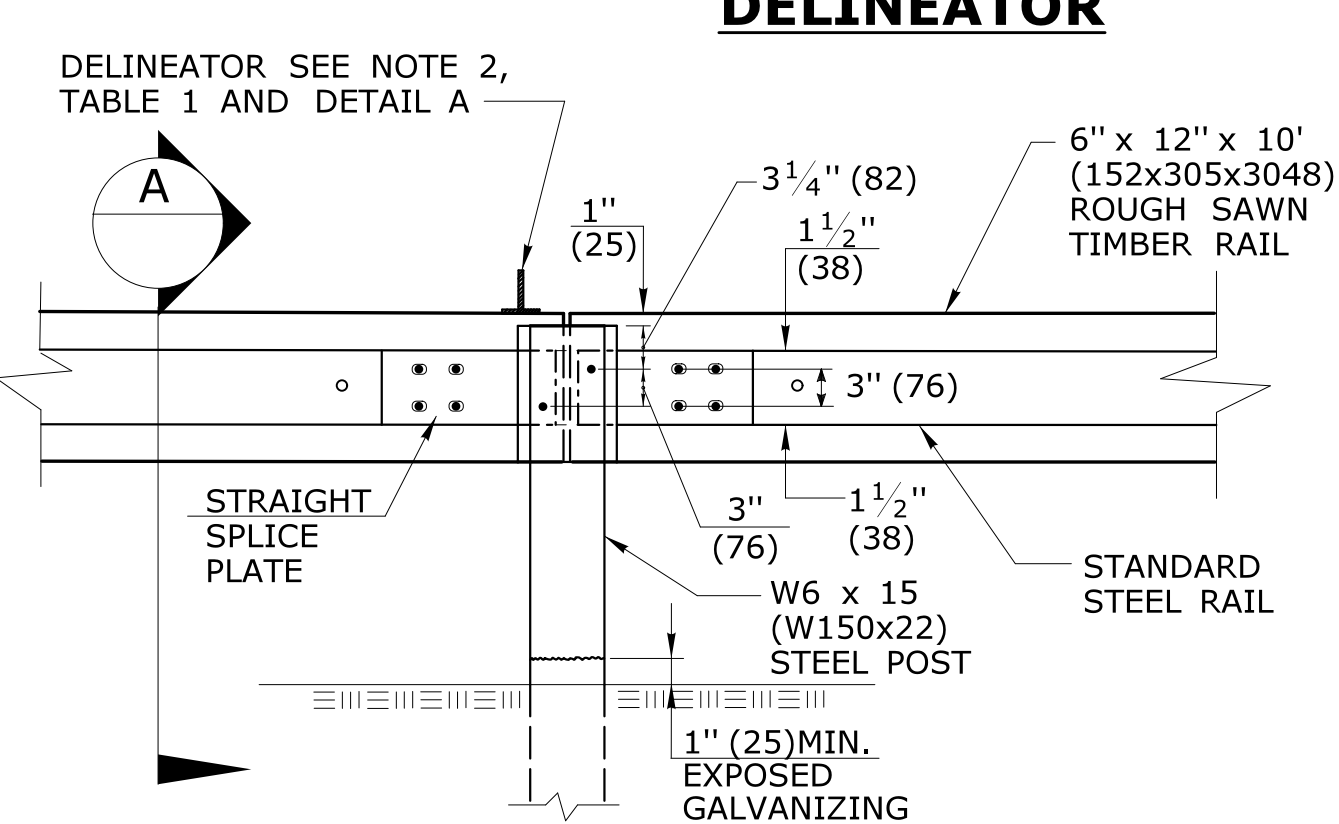
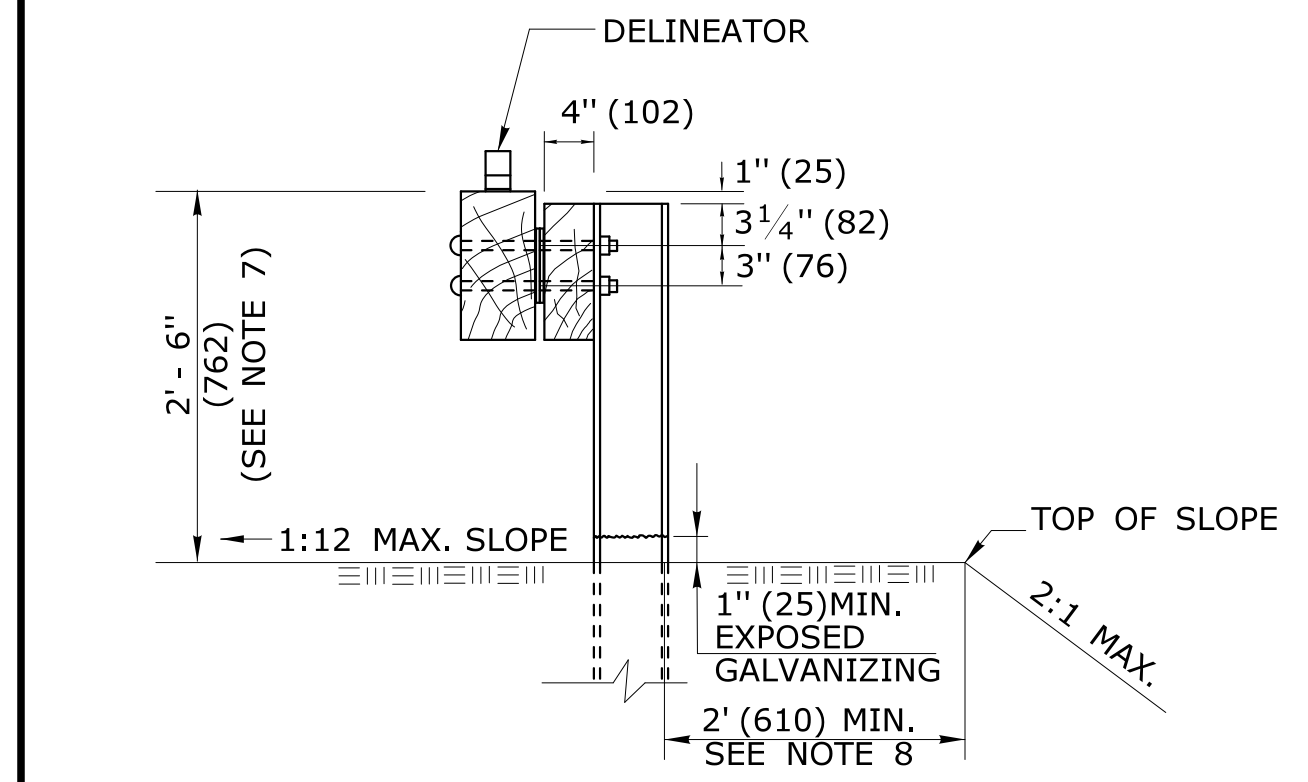
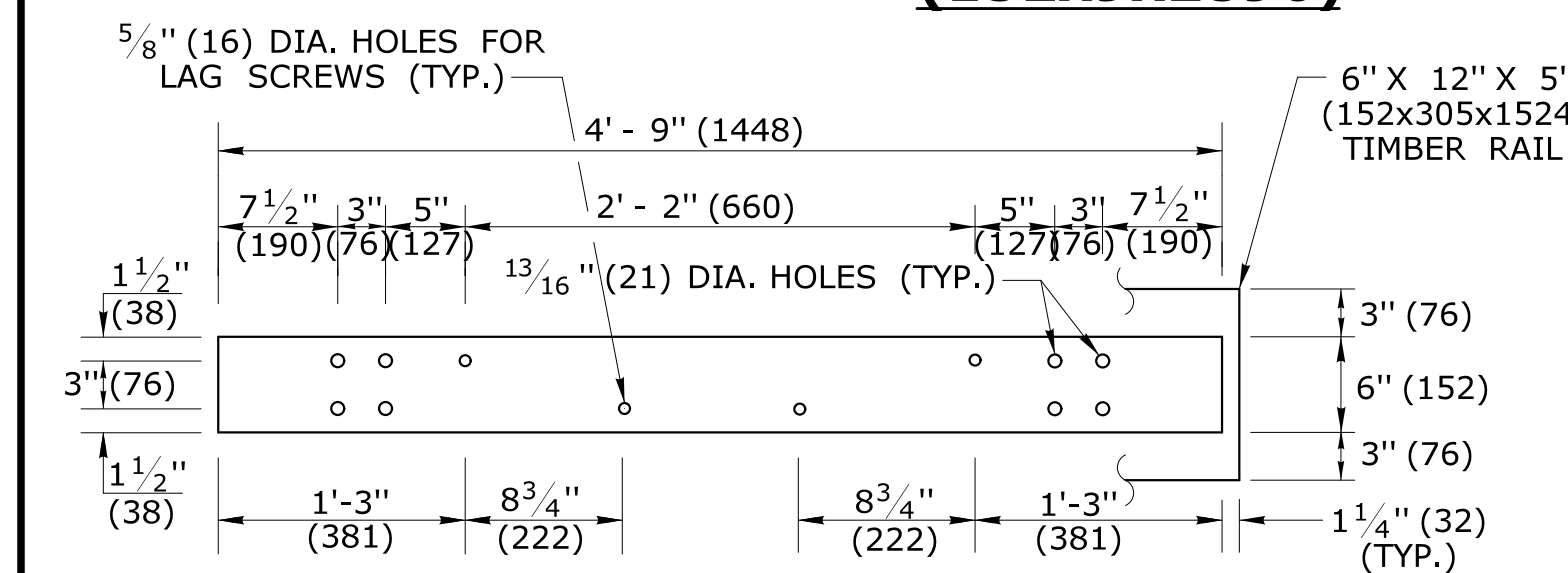
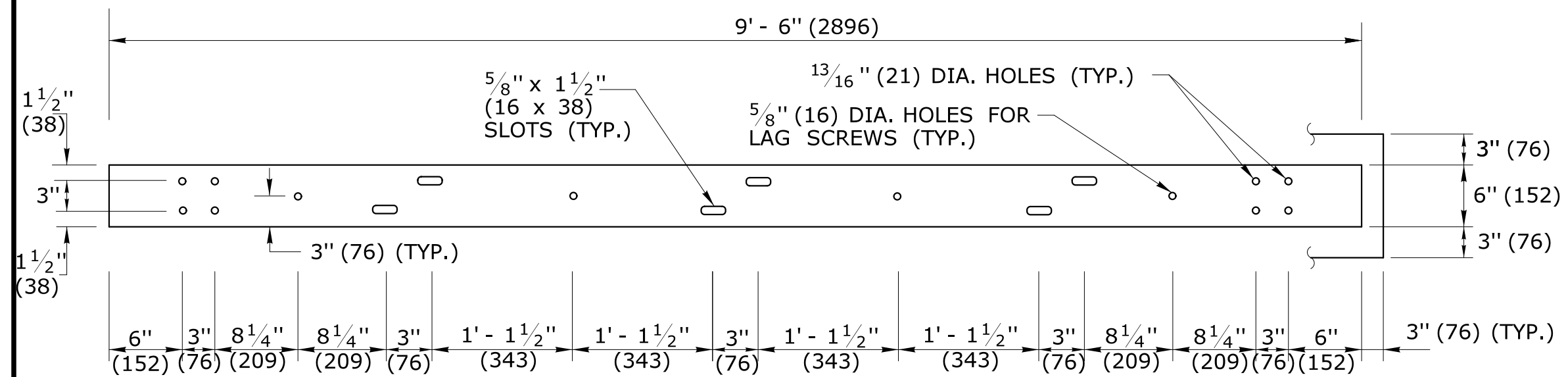


PLAN

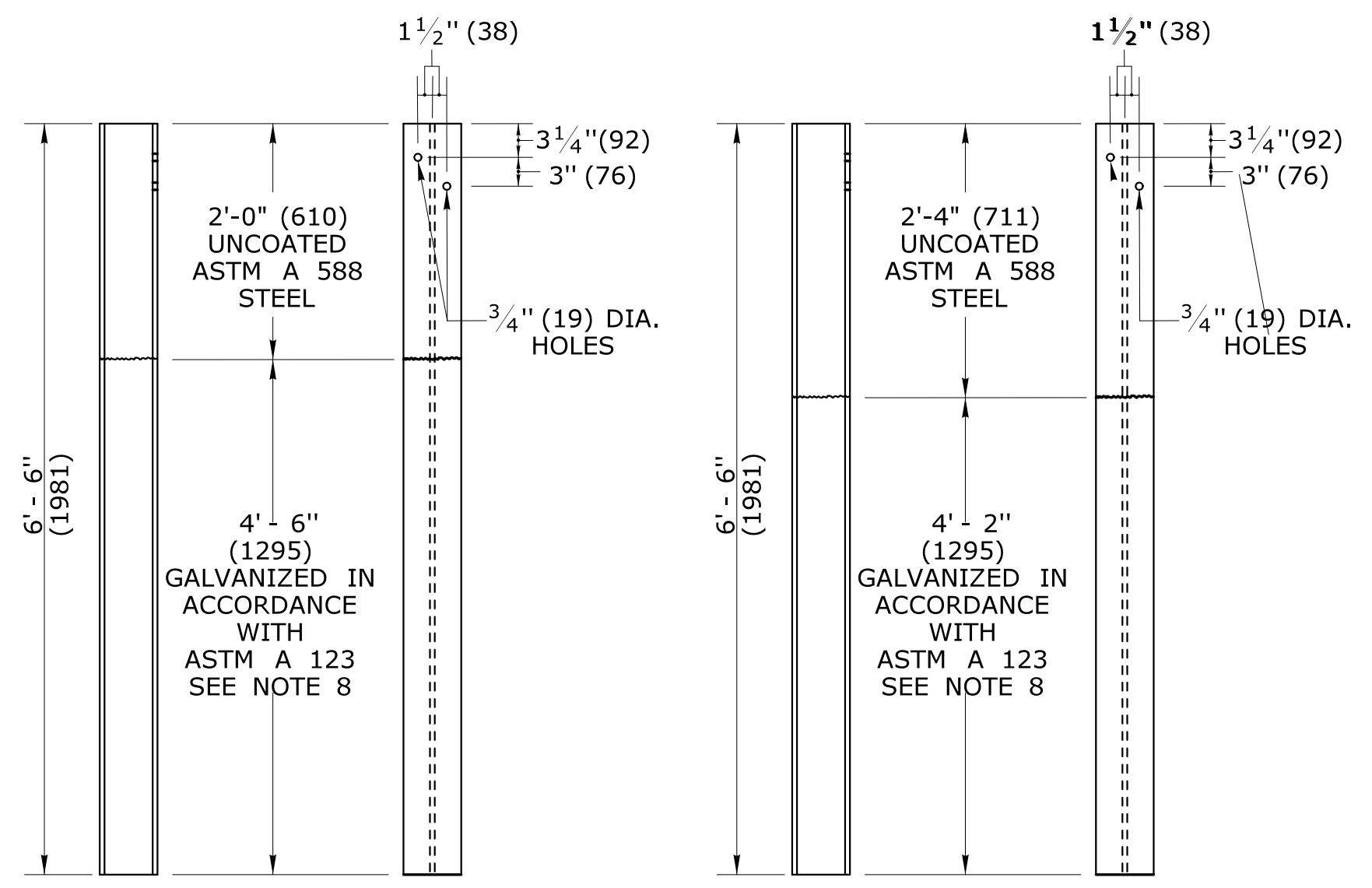


ELEVATION

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |



- GENERAL NOTES:**
 MATERIALS AND CONSTRUCTION OF THE STEEL-BACKED TIMBER GUIDERAIL SHALL CONFORM TO SPECIAL PROVISION PROVIDED WITH THE PROJECT AND SHALL BE PAID FOR UNDER THE ITEM "STEEL-BACKED TIMBER GUIDERAIL - TYPE A".
- DELINEATORS SHALL BE PLASTIC INVERTED T-SECTIONS IN ACCORDANCE WITH DETAIL "A". REFLECTORS SHALL BE SPACED IN ACCORDANCE WITH TABLE 1, AND POSITIONED PERPENDICULAR TO THE ADJACENT EDGE OF LANE. DO NOT ATTACH REFLECTORS ON FLARE OR TERMINAL SECTIONS. REFLECTIVE SHEETING SHALL BE SILVER-WHITE ON ALL RAIL SECTIONS ADJACENT TO THE RIGHT SHOULDER, AND YELLOW ON RAIL SECTIONS ADJACENT TO THE LEFT SHOULDER OF TRAVEL LANES. DELINEATORS SHALL BE INCLUDED IN THE COST FOR "STEEL-BACKED TIMBER GUIDERAIL - TYPE A".
 - TWO ADDITIONAL 5/8" (16) DIA. X 4" (102) LONG LAG SCREWS AND WASHERS SHALL BE INSTALLED AT ALL MID-SPAN POINTS FOR STANDARD SYSTEMS.
 - ALL CONNECTION HARDWARE SHALL BE SUFFICIENTLY TIGHTENED TO ACCOMMODATE FOR SHRINKAGE OF THE WOOD ELEMENTS.
 - WHEN FURNISHING THE SHOP BENT SPLICE PLATES, USE THE MINIMUM BEND ANGLE AS SHOWN IN TABLE 2.
 - FOR RAILING SET ON A CURVE WITH RADII < 70' (21.3m) SPACE POSTS AT 5' (1524).
 - GUIDERAIL HEIGHT MAY VARY UP TO 2" (51) PLUS OR MINUS THE HEIGHT SHOWN IN THE DETAILS TO ACHIEVE A SMOOTH AND CONTINUOUS GUIDERAIL PROFILE.
 - 7'-6" (2286) LONG POSTS MAY BE USED IN LOCATIONS WHERE 2' (610) MIN SHELF IS NOT ATTAINABLE. GALVANIZED COATING LIMITS SHALL BE INCREASED BY 1" (305). PAYMENT FOR EXTRA LONG POSTS SHALL BE INCLUDED WITH THE ITEM FOR "STEEL-BACKED TIMBER GUIDERAIL - TYPE A".



SECTION (NO CURB)

REAR VIEW ELEVATION POST CONNECTION

(FRONT VIEW) STEEL SPLICE PLATE DETAIL 6" x 3/8" x 2'-6" (152x9x762)

SIDE VIEW FRONT VIEW POST DETAIL W6 X 15 (W150x22) WITH CURBING

SIDE VIEW FRONT VIEW POST DETAIL W6 X 15 (W150x22) NO CURBING

TABLE 2

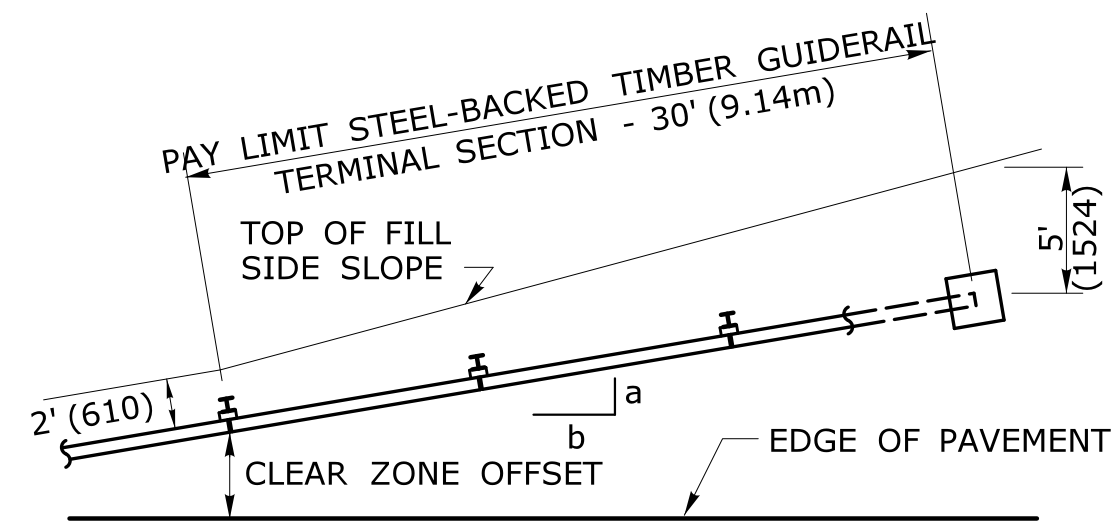
| Radius R (ft.)(m) | Ø (Degrees) | d (in.)(mm) |
|-------------------|-------------|-------------|
| 35 (10.7)Min. | 4.10 | 5/8(16) |
| 40 (12.2) | 3.58 | 9/16(14) |
| 45 (13.7) | 3.18 | 1/2(13) |
| 50 (15.2) | 2.86 | 7/16(11) |
| 55 (16.8) | 2.60 | 7/16(11) |
| 60 (18.3) | 2.40 | 3/8(9) |
| 65 (19.8) | 2.20 | 3/8(9) |
| 70 (21.3) | 2.05 | 5/16(8) |
| Over 70 (21.3) | Flat | 0 |

TABLE 1

| DELINEATOR SPACING | |
|--------------------|-------------|
| CURVE RADIUS | SPACE |
| < 300' (91.4m) | 20' (6.10m) |
| ≥ 300' (91.4m) | 50' (15.2m) |

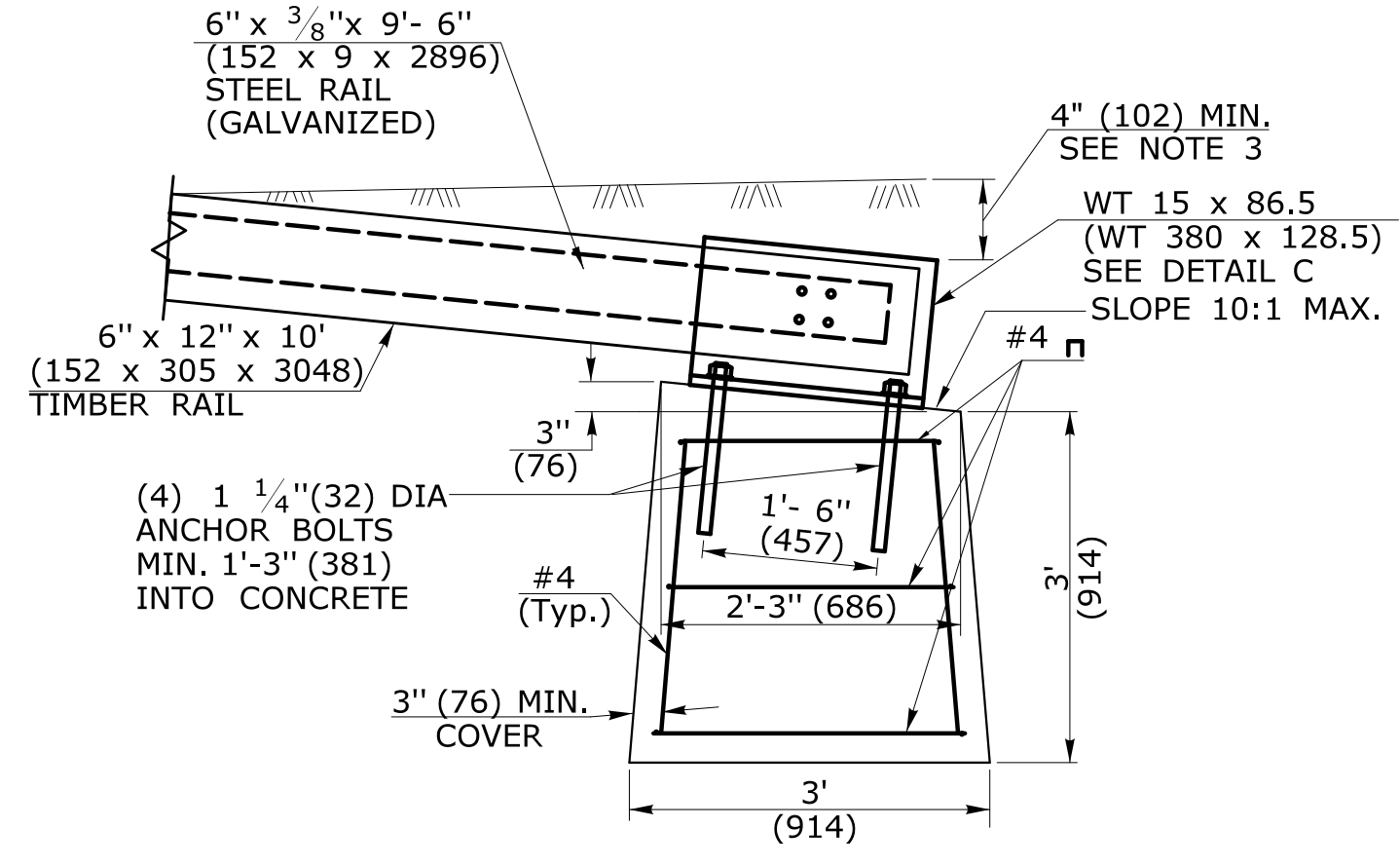
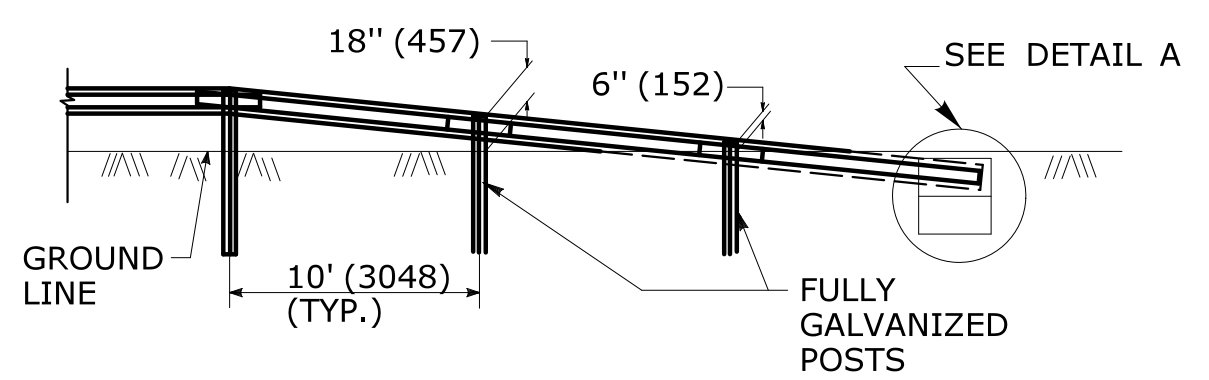
ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

| | | | | | |
|------------------------------|-----------------------|------------------|--|--|------------------------------------|
| DESIGNER/DRAFTER: A. VOMACKA | CHECKED BY: S. BEARCE | SIGNATURE/BLOCK: | STATE OF CONNECTICUT TOWN OF WILTON | PROJECT NUMBER: 0161-0142 PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVERS LANE OVER COMSTOCK BROOK TOWN(S): WILTON DRAWING TITLE: MISCELLANEOUS DETAILS - 2 | DRAWING NO. HWY-11 SHEET NO. |
|------------------------------|-----------------------|------------------|--|--|------------------------------------|



PLAN **ELEVATION**
STEEL-BACKED TIMBER GUIDERAIL - TERMINAL SECTION
(BURIED END ANCHORAGE TYPE I)

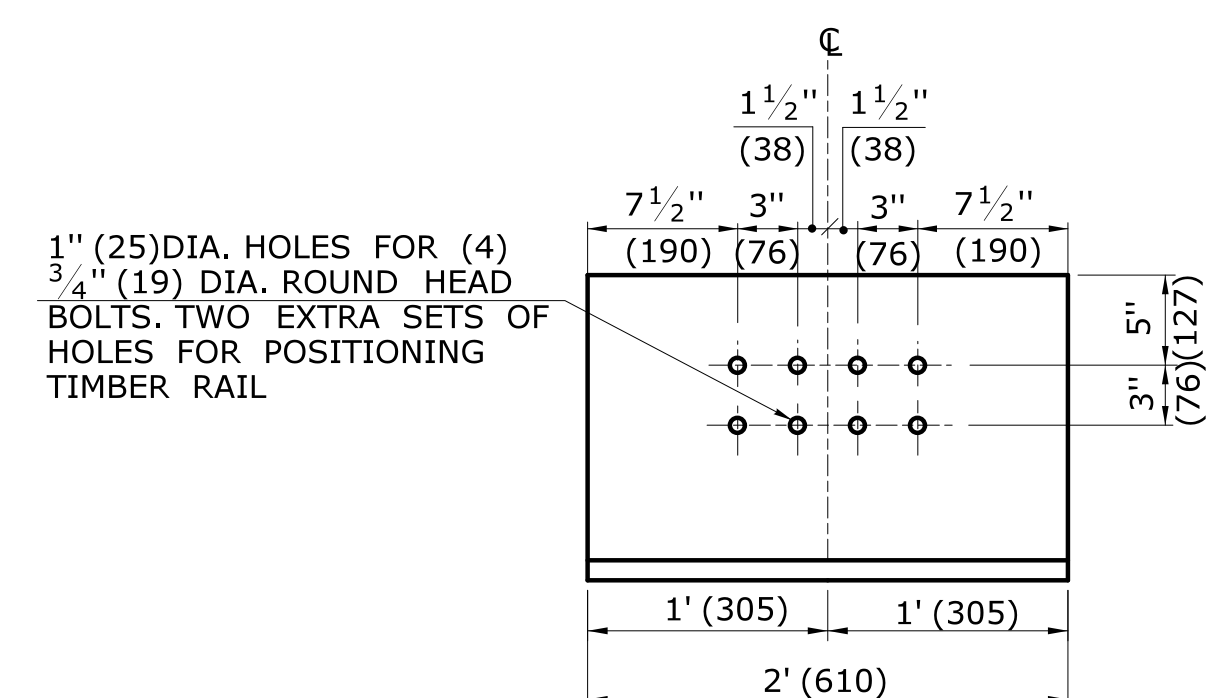
NOTE: SEE PLAN SHEETS FOR FLARE RATE a:b.



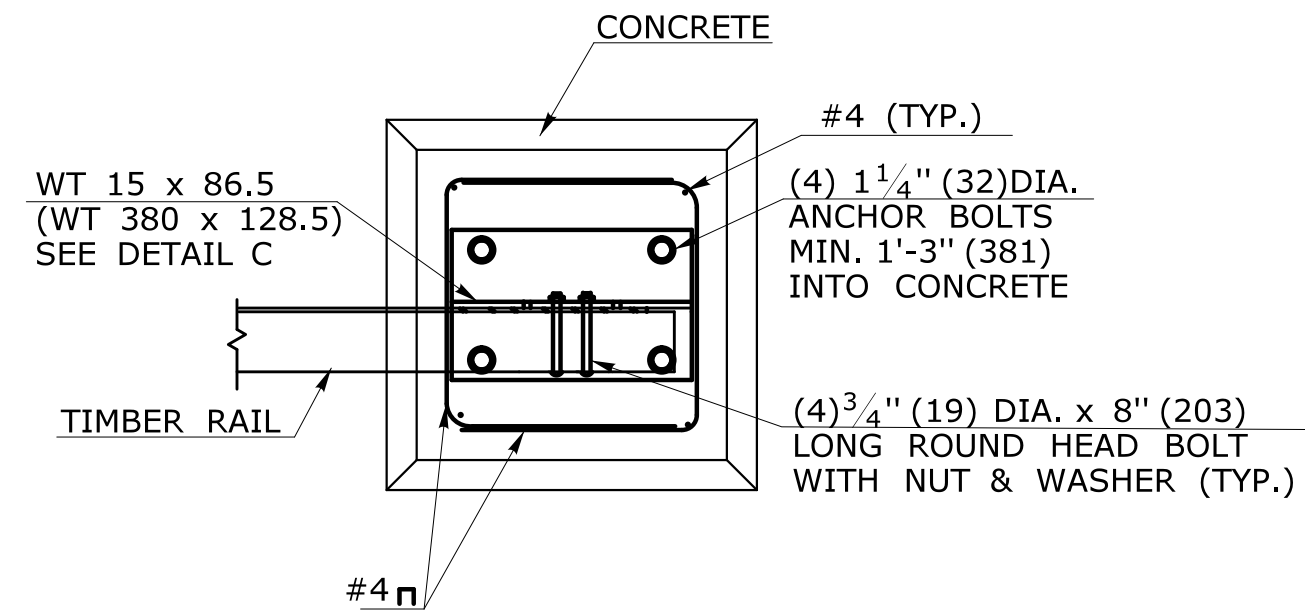
ELEVATION
DETAIL A

GENERAL NOTES:

1. MATERIALS AND CONSTRUCTION OF THE STEEL-BACKED TIMBER GUIDERAIL TERMINAL SECTIONS SHALL CONFORM TO THE SPECIAL PROVISIONS PROVIDED WITH THE PROJECT AND SHALL BE PAID FOR UNDER THE ITEM "STEEL-BACKED TIMBER GUIDERAIL - TERMINAL SECTION". ALL HARDWARE IN CONTACT WITH THE GROUND SHALL BE GALVANIZED IN ACCORDANCE WITH THE SPECIAL PROVISION.
3. FOR TYPE 1 TERMINAL SECTION, EXTEND THE FLARE OUTSIDE BURY THE TERMINAL SECTION AND ELEMENT TO OBTAIN A MINIMUM COVER OF 4". SEE DETAIL A.
4. BEGIN THE FLARE AT THE NEAREST POST TO A TRANSITION POINT BETWEEN FILL AND CUT AS DIRECTED BY THE ENGINEER.
5. THE GUIDERAIL FLARE SHOWN ON THE PLAN SHEETS IS THE MINIMUM LENGTH AND RATE REQUIRED AS DIRECTED BY THE ENGINEER.



ELEVATION
DETAIL C
WT 15 x 86.5 (GALVANIZED)
(WT 380 X 128.5)



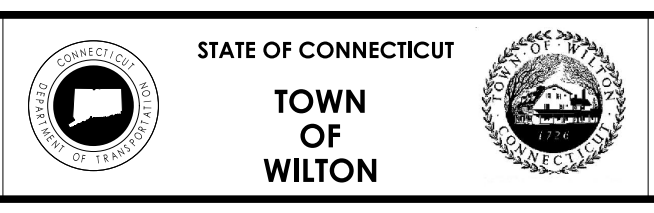
PLAN
DETAIL D
PLAN VIEW FOR
TYPE I ANCHOR

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |
| | | |

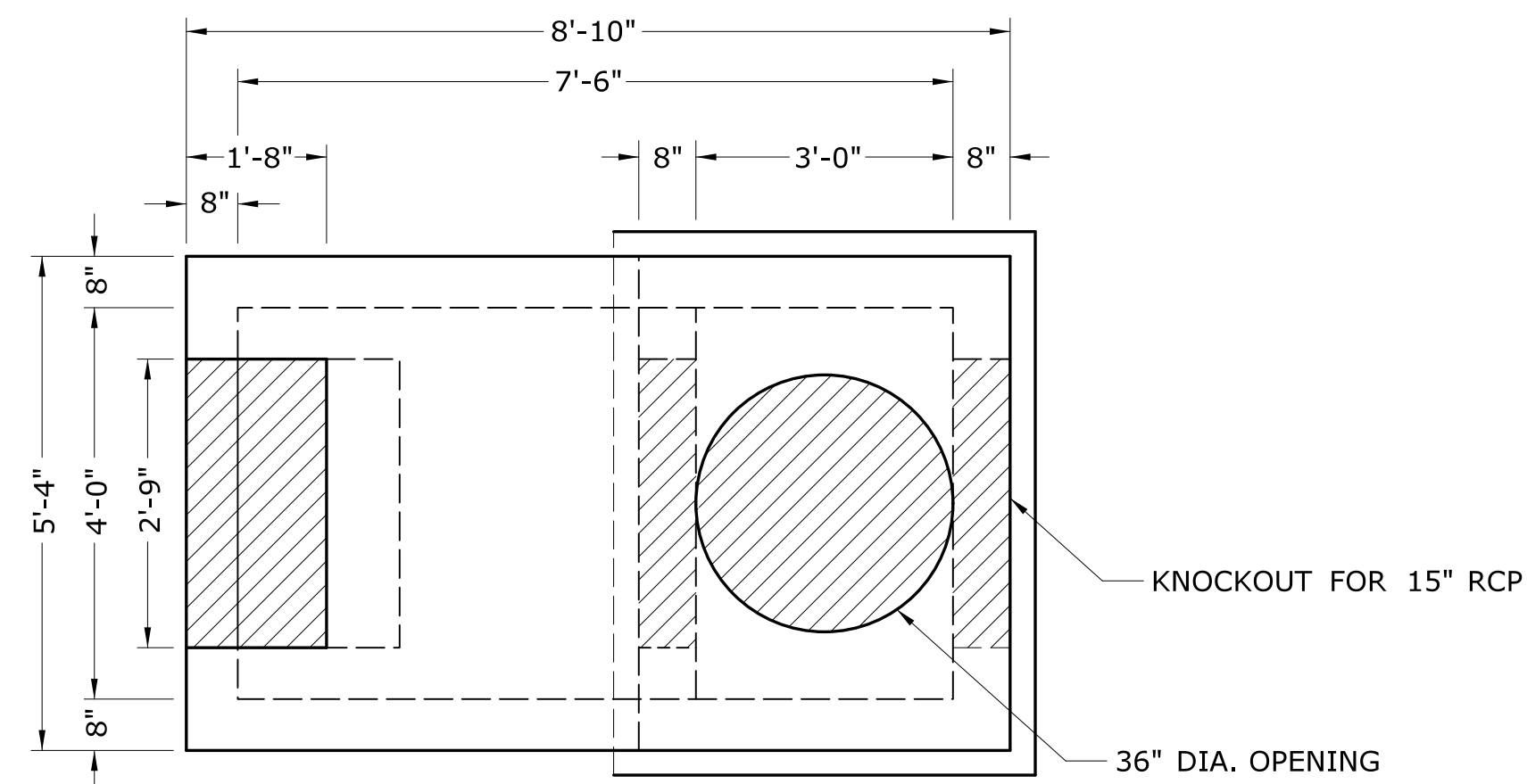
DESIGNER/DRAFTER: A. VOMACKA CHECKED BY: S. BEARCE
 NOT TO SCALE

SIGNATURE/
 BLOCK:

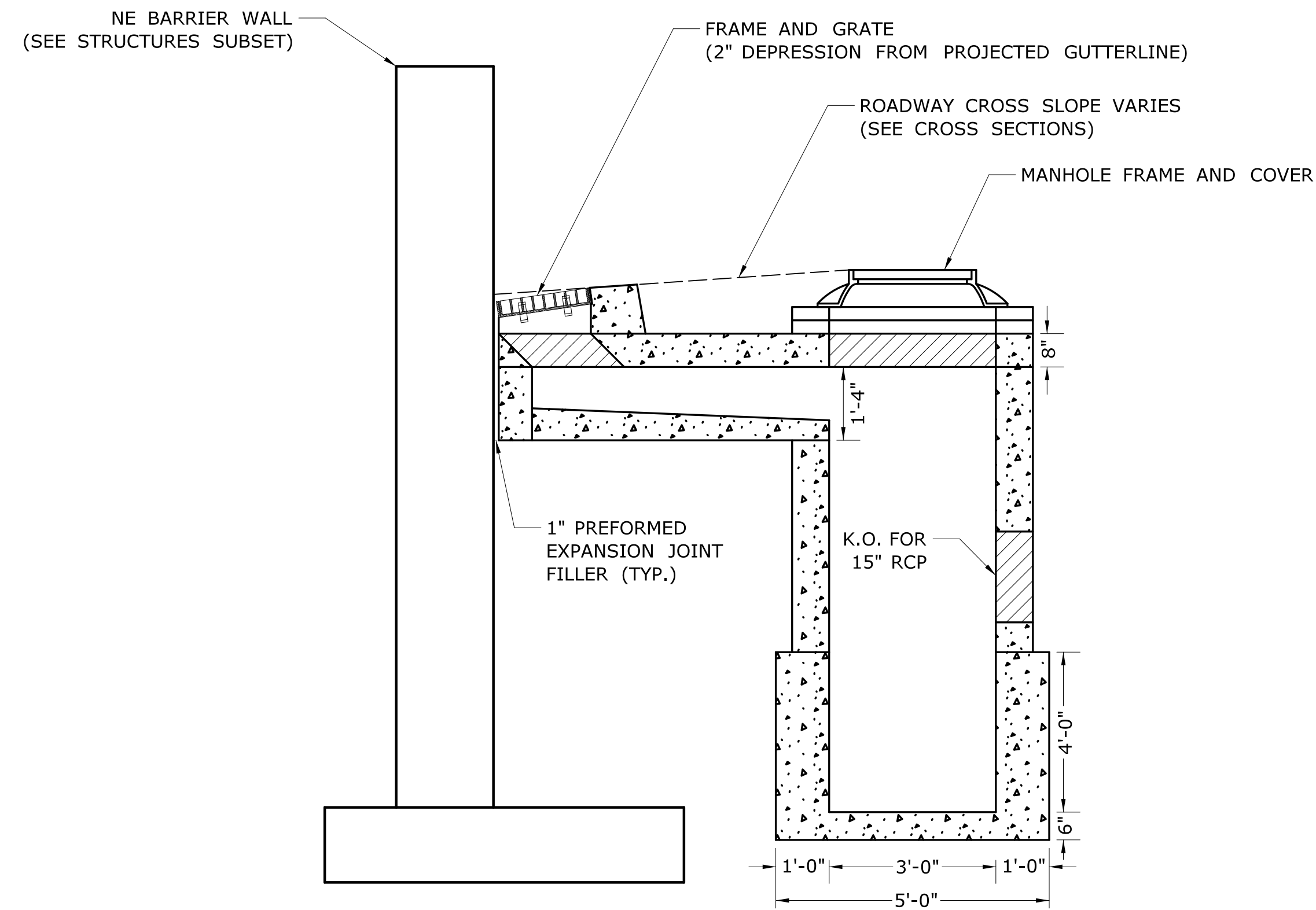


PROJECT NUMBER: 0161-0142
 PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVERS LANE OVER COMSTOCK BROOK
 TOWN(S): WILTON
 DRAWING TITLE: MISCELLANEOUS DETAILS - 3

DRAWING NO.
 HWY-12
 SHEET NO.

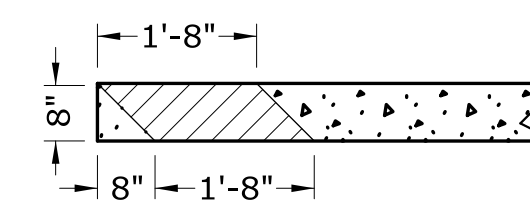


PLAN

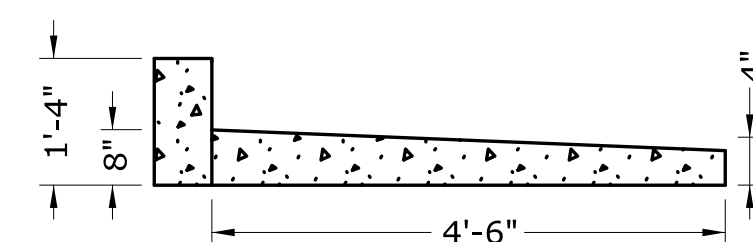


ELEVATION

**OFFSET TYPE "C-M" CATCH BASIN
DOUBLE GRATE TYPE 2 (4' SUMP)**



CB OPENING IN ROOF



SLOPED FLOOR IN OFFSET RISER

NOTES:

1. CATCH BASIN TOP SHALL BE TYPE 'C-M' BARRIER CURB TOP. SEE CTDOT STANDARD SHEET NO. HW-586.07d.
2. SEE CTDOT STANDARD SHEET NO. HW-586.08 FOR DETAILS OF FRAME AND GRATE.
3. SEE CTDOT STANDARD SHEET NO. HW-586.10a FOR MANHOLE FRAME AND COVER.
4. THE COST OF 1" PREFORMED EXPANSION JOINT FILLER SHALL BE INCLUDED IN THE COST OF EACH CATCH BASIN.
4. REINFORCEMENT SHALL CONFORM TO ASTM A615, GRADE 60.
6. ALL REINFORCEMENT SHALL HAVE A MINIMUM COVER OF 2" EXCEPT FOR BENEATH BOTTOM REINFORCEMENT IN TOP SLABS WHERE THE MINIMUM MAY BE 1 1/2".

PRECAST CONCRETE UNIT NOTES:

PRECAST CONCRETE UNITS MAY BE USED IN PLACE OF CLASS PCC03340 CONCRETE WITH THE FOLLOWING CONDITIONS:

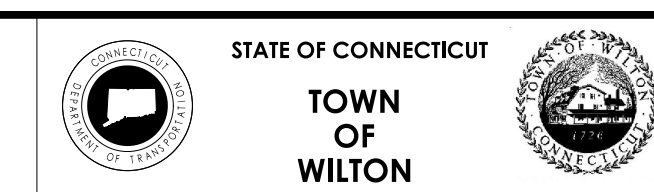
1. WALLS SHALL HAVE THE SAME THICKNESS AND REINFORCEMENT AS DETAILED FOR CLASS PCC03340 CONCRETE CONSTRUCTION.
2. JOINTS BETWEEN PRECAST UNITS SHALL BE HORIZONTAL AND MORTARED.
3. THE CONTRACTOR SHALL SUBMIT ALTERNATE DESIGNS IN ACCORDANCE WITH ASTM C-913 FOR WALL THICKNESSES LESS THAN 8" FOR BASINS UP TO 10FT DEEP.
 - A. REINFORCEMENT AND KEYWAYS WILL BE NECESSARY.
 - B. THE DIMENSIONS MAY BE VARIED, PROVIDED THAT THE INSIDE DIMENSIONS OF THE UNITS ARE GREATER THAN THE MINIMUM INSIDE DIMENSIONS SHOWN.

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |

DESIGNER/DRAFTER: A. VOMACKA CHECKED BY: S. BEARCE

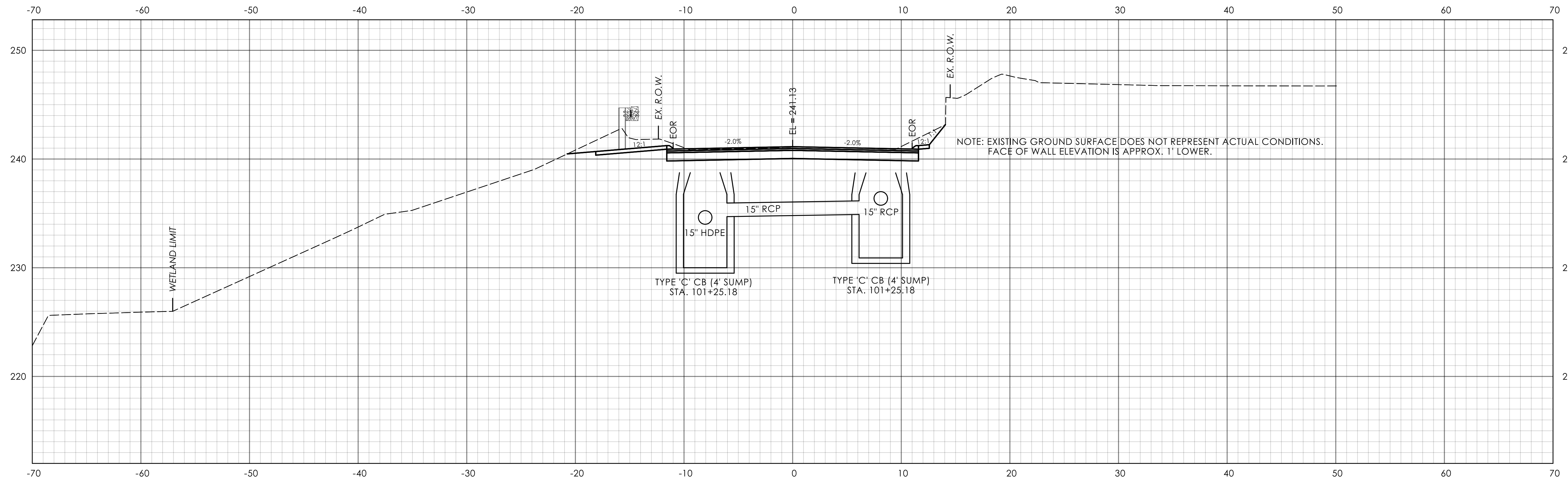
NOT TO SCALE

SIGNATURE/BLOCK:

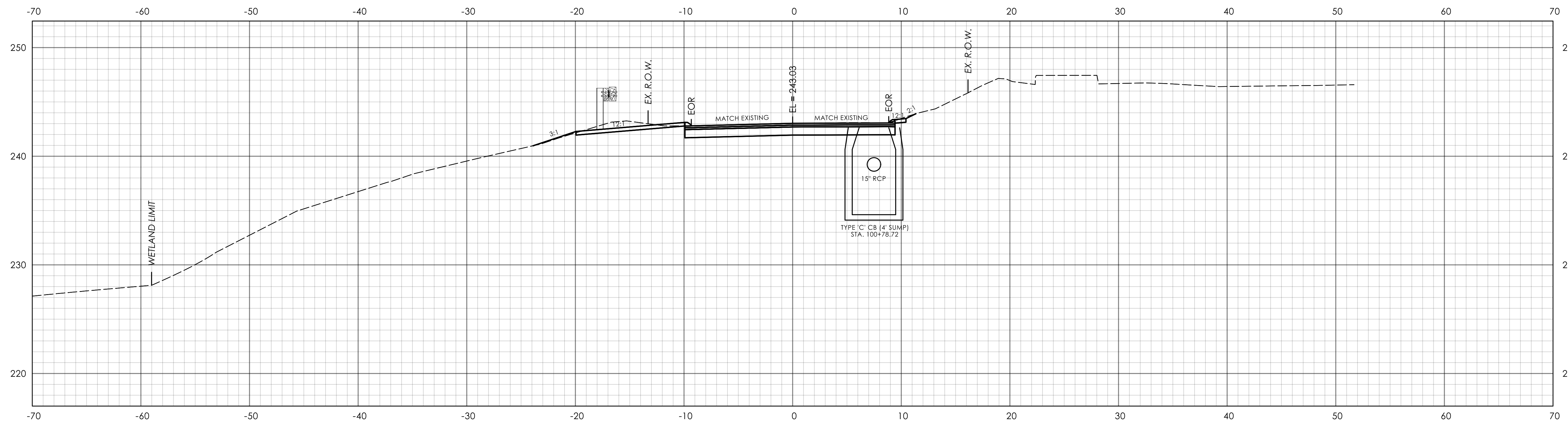


PROJECT NUMBER: 0161-0142
 PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVERS LANE OVER COMSTOCK BROOK
 TOWN(S): WILTON
 DRAWING TITLE: MISCELLANEOUS DETAILS - 4

DRAWING NO. HWY-13
 SHEET NO.



STA.101+00.00



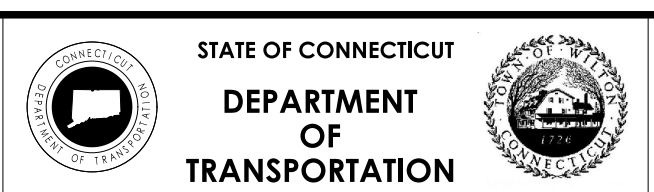
STA.100+75.00

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |

DESIGNER/DRAFTER: A. VOMACKA CHECKED BY: S. BEARCE

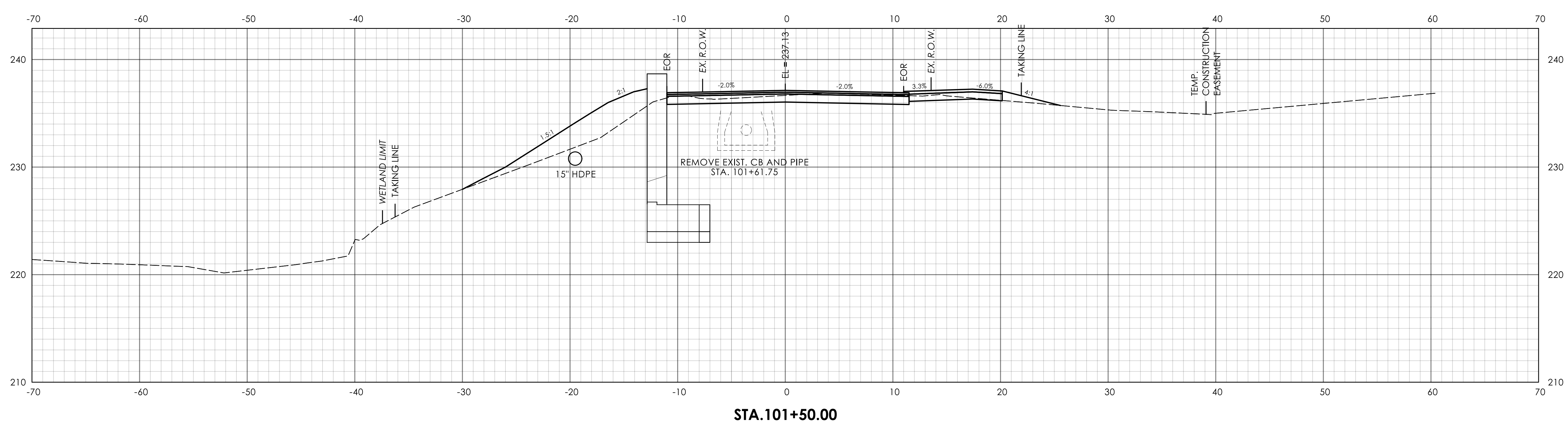
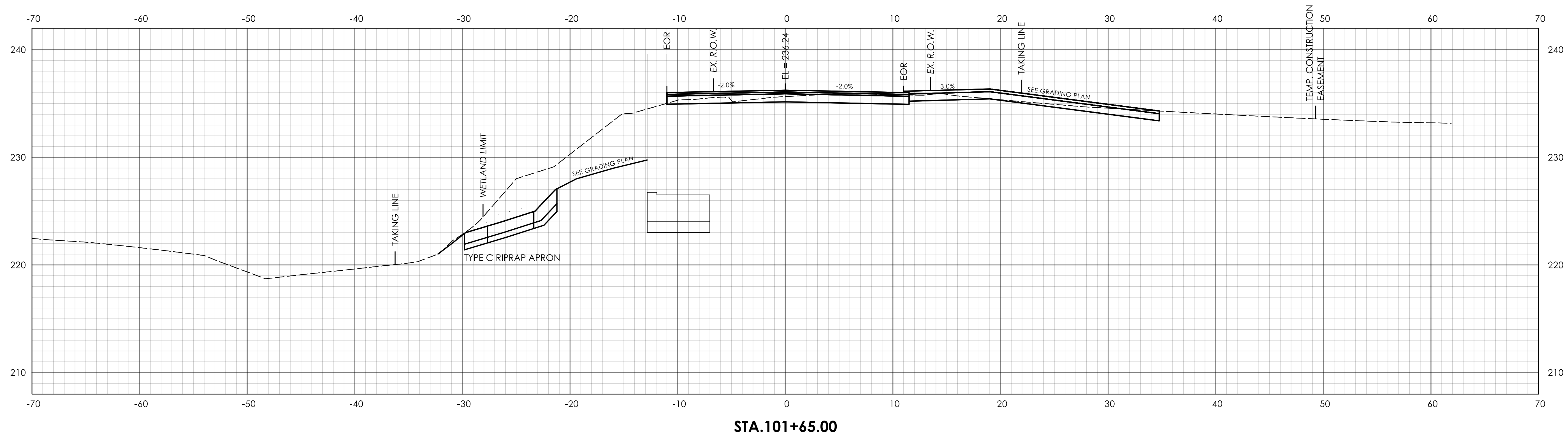
HORIZONTAL SCALE 1" = 5'

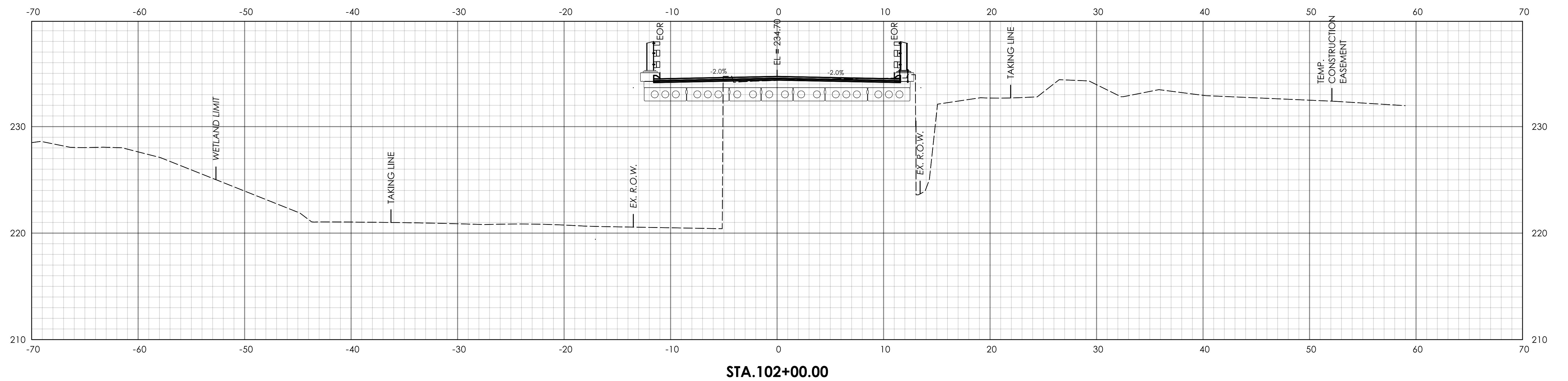
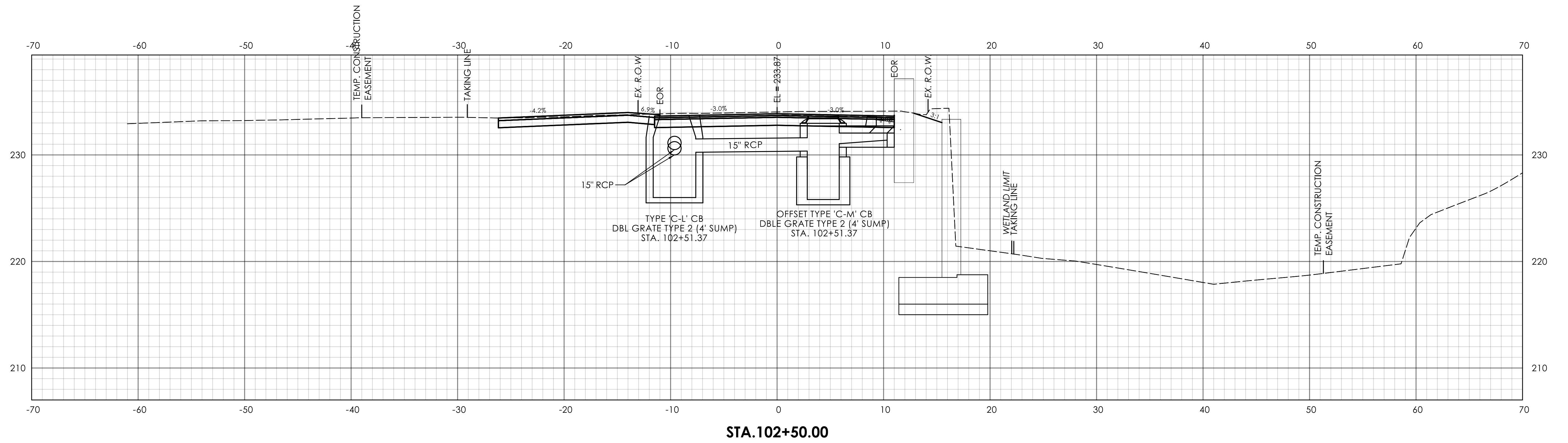
SIGNATURE/BLOCK:

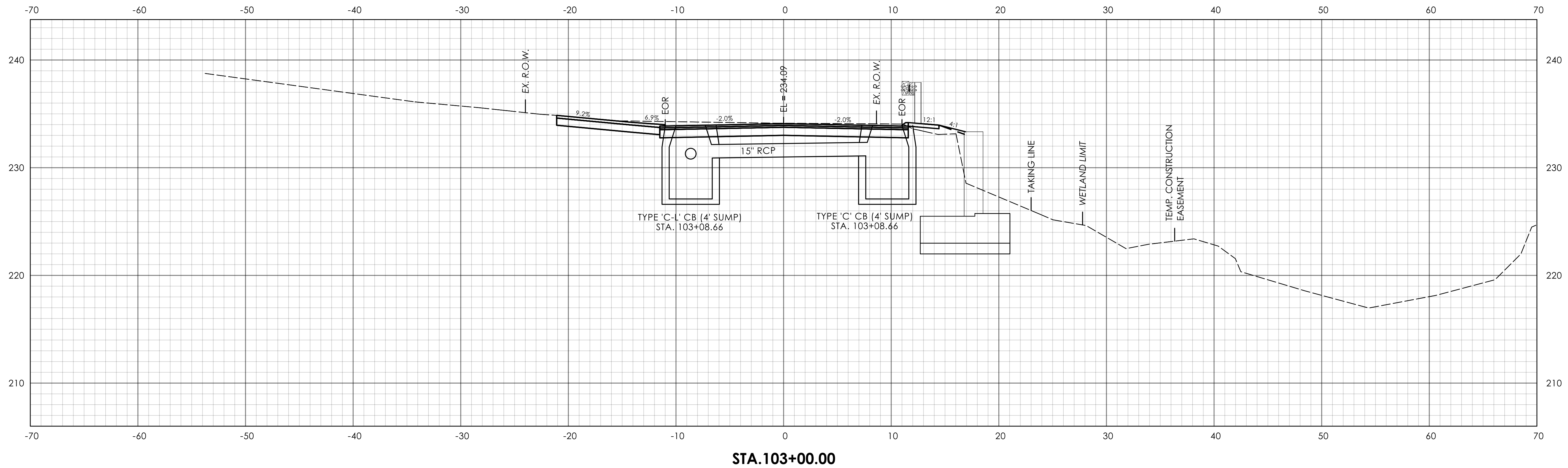


PROJECT NUMBER: 0161-0142
 PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVERS LANE OVER COMSTOCK BROOK
 TOWN(S): WILTON
 DRAWING TITLE: ROADWAY CROSS SECTIONS 1

DRAWING NO. HWY-14
 SHEET NO.







| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |

DESIGNER/DRAFTER: A. VOMACKA CHECKED BY: S. BEARCE

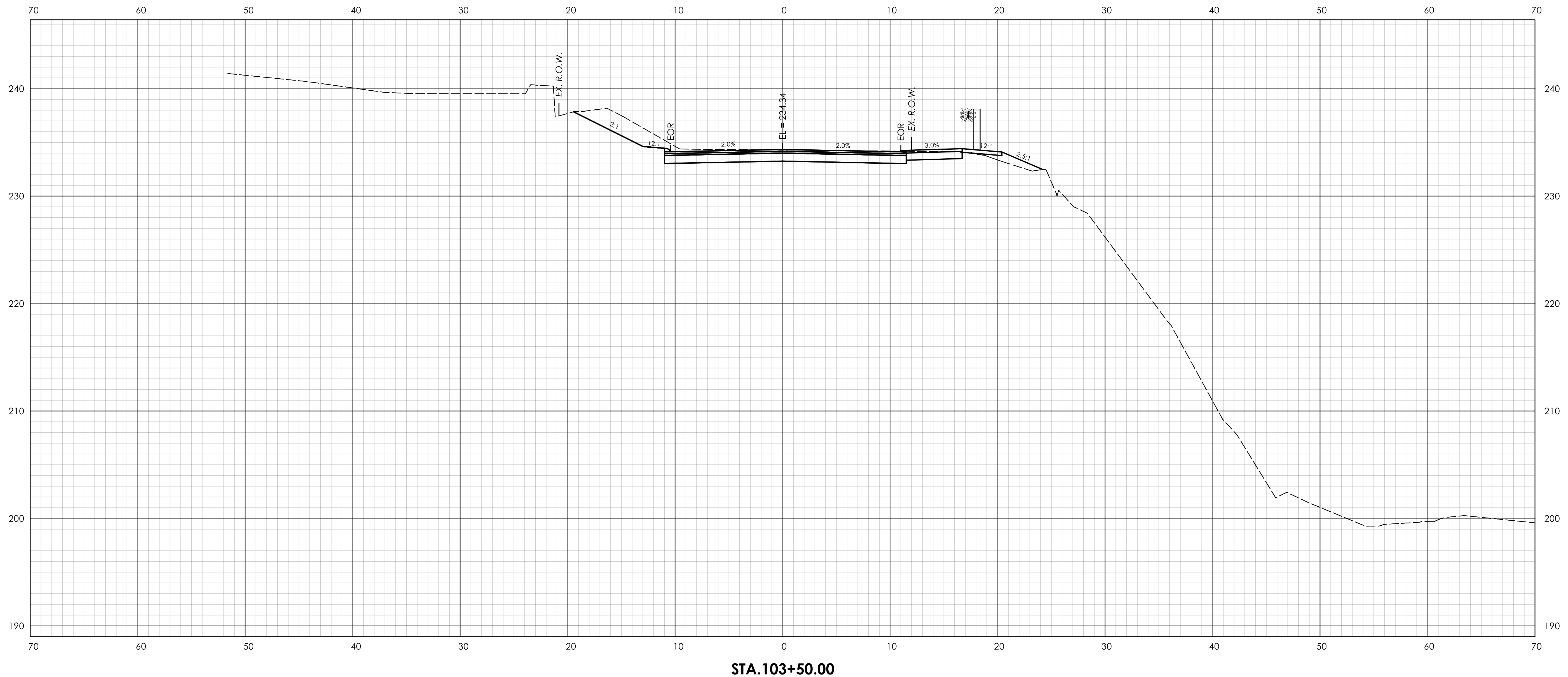
HORIZONTAL SCALE
1" = 5'

SIGNATURE/
BLOCK:



PROJECT NUMBER: 0161-0142
 PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVERS LANE OVER COMSTOCK BROOK
 TOWN(S): WILTON
 DRAWING TITLE: ROADWAY CROSS SECTIONS 4

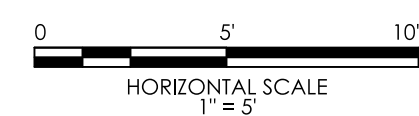
DRAWING NO.
HWY-17
 SHEET NO.



STA.103+50.00

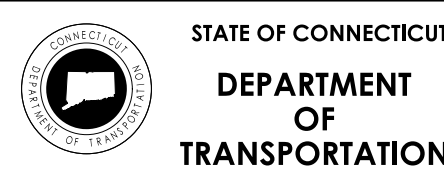
| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |

DESIGNER/DRAFTER: A. VOMACKA CHECKED BY: S. BEARCE



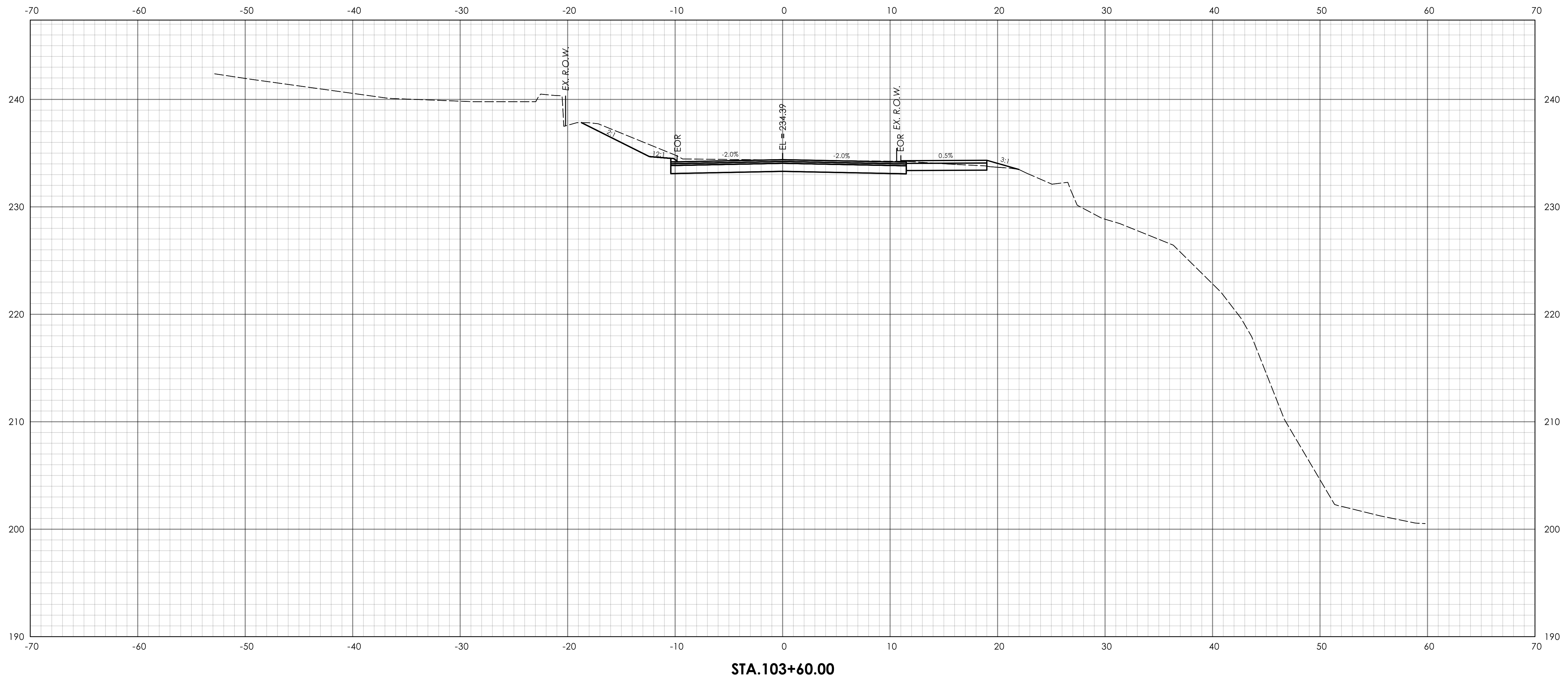
SIGNATURE/
BLOCK:

LASTED SAVED BY: avomacka FILE NAME: J:\DWG\F2016\0182\010\0161-0142\Highways\Contract_Plans\HW_CP_0161_0142_XSC.dgn
PLOTTED DATE: 10/20/2022



PROJECT NUMBER: 0161-0142
PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVERS LANE OVER COMSTOCK BROOK
TOWN(S): WILTON
DRAWING TITLE: ROADWAY CROSS SECTIONS 5

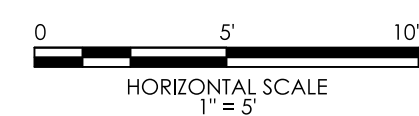
DRAWING NO.
HWY-18
SHEET NO.



STA.103+60.00

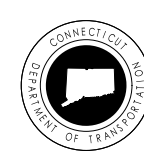
| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |

DESIGNER/DRAFTER: A. VOMACKA CHECKED BY: S. BEARCE



SIGNATURE/
BLOCK:

LASTED SAVED BY: avomacka FILE NAME: J:\DWG\2016\0182\010\0161-0142\Highways\Contract_Plans\HW_CP_0161_0142_XSC.dgn
PLOTTED DATE: 10/20/2022

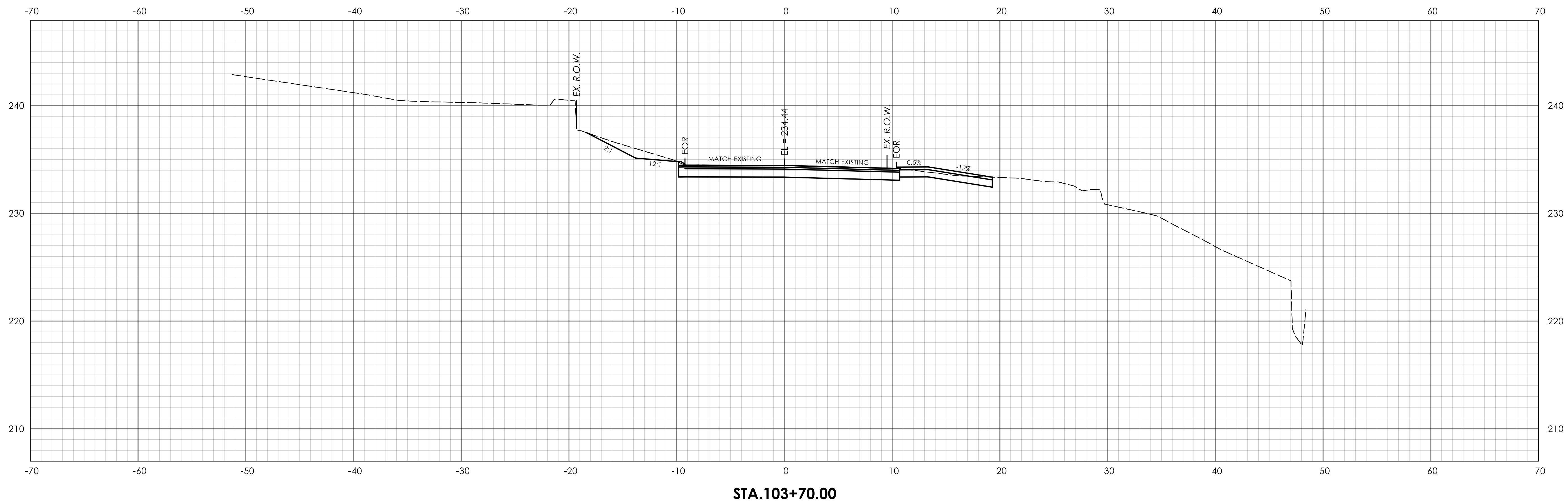


STATE OF CONNECTICUT
DEPARTMENT
OF
TRANSPORTATION



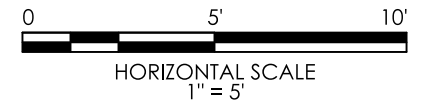
PROJECT NUMBER: 0161-0142
PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVERS LANE OVER COMSTOCK BROOK
TOWN(S): WILTON
DRAWING TITLE: ROADWAY CROSS SECTIONS 6

DRAWING NO.
HWY-19
SHEET NO.



STA.103+70.00

DESIGNER/DRAFTER: A. VOMACKA
 CHECKED BY: S. BEARCE



SIGNATURE/
 BLOCK:



PROJECT NUMBER: 0161-0142
 PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVERS LANE OVER COMSTOCK BROOK
 TOWN(S): WILTON
 DRAWING TITLE: ROADWAY CROSS SECTIONS 7

DRAWING NO.
 HWY-20
 SHEET NO.

LASTED SAVED BY: avomacka FILE NAME: J:\DWG\F2016\0182\010\0161-0142\Highways\Contract_Plans\HW_CP_0161_0142_XSC.dgn
 PLOTTED DATE: 10/20/2022

**04 - STRUCTURE
INDEX OF DRAWINGS**

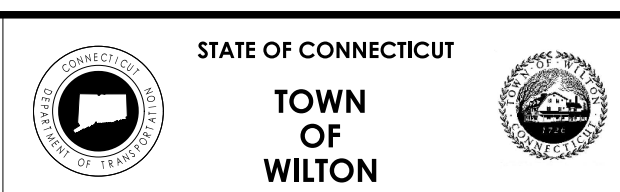
| DRAWING NUMBER | DRAWING TITLE | DRAWING NUMBER | DRAWING TITLE |
|----------------|--|----------------|---|
| S-01 | INDEX OF DRAWINGS | S-21 | 3 - TUBE CURB MOUNTED BRIDGE RAIL DETAILS - 1 |
| S-02 | GENERAL PLAN AND ELEVATION | S-22 | 3 - TUBE CURB MOUNTED BRIDGE RAIL DETAILS - 2 |
| S-03 | LAYOUT PLAN | S-23 | 3 - TUBE CURB MOUNTED BRIDGE RAIL DETAILS - REINFORCEMENT |
| S-04 | BORING LOGS 1 | | |
| S-05 | BORING LOGS 2 | | |
| S-06 | BORING LOGS 3 | | |
| S-07 | BORING LOGS 4 | | |
| S-08 | BORING LOGS 5 | | |
| S-09 | STAGING PLAN 1 | | |
| S-10 | STAGING PLAN 2 | | |
| S-11 | ROCK EXCAVATION PLAN | | |
| S-12 | SOUTH ABUTMENT 1 | | |
| S-13 | NORTH ABUTMENT 2 | | |
| S-14 | WINGWALL DETAILS | | |
| S-15 | SUBSTRUCTURE DETAILS | | |
| S-16 | FRAMING PLAN | | |
| S-17 | BEAM DETAILS | | |
| S-18 | DECK DETAILS | | |
| S-19 | BEARING DETAILS | | |
| S-20 | ASPHALTIC PLUG EXPANSION JOINT DETAILS | | |

DESIGNED BY:
FUSS & O'NEILL INC.
146 HARTFORD ROAD
MANCHESTER, CT 06040

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |

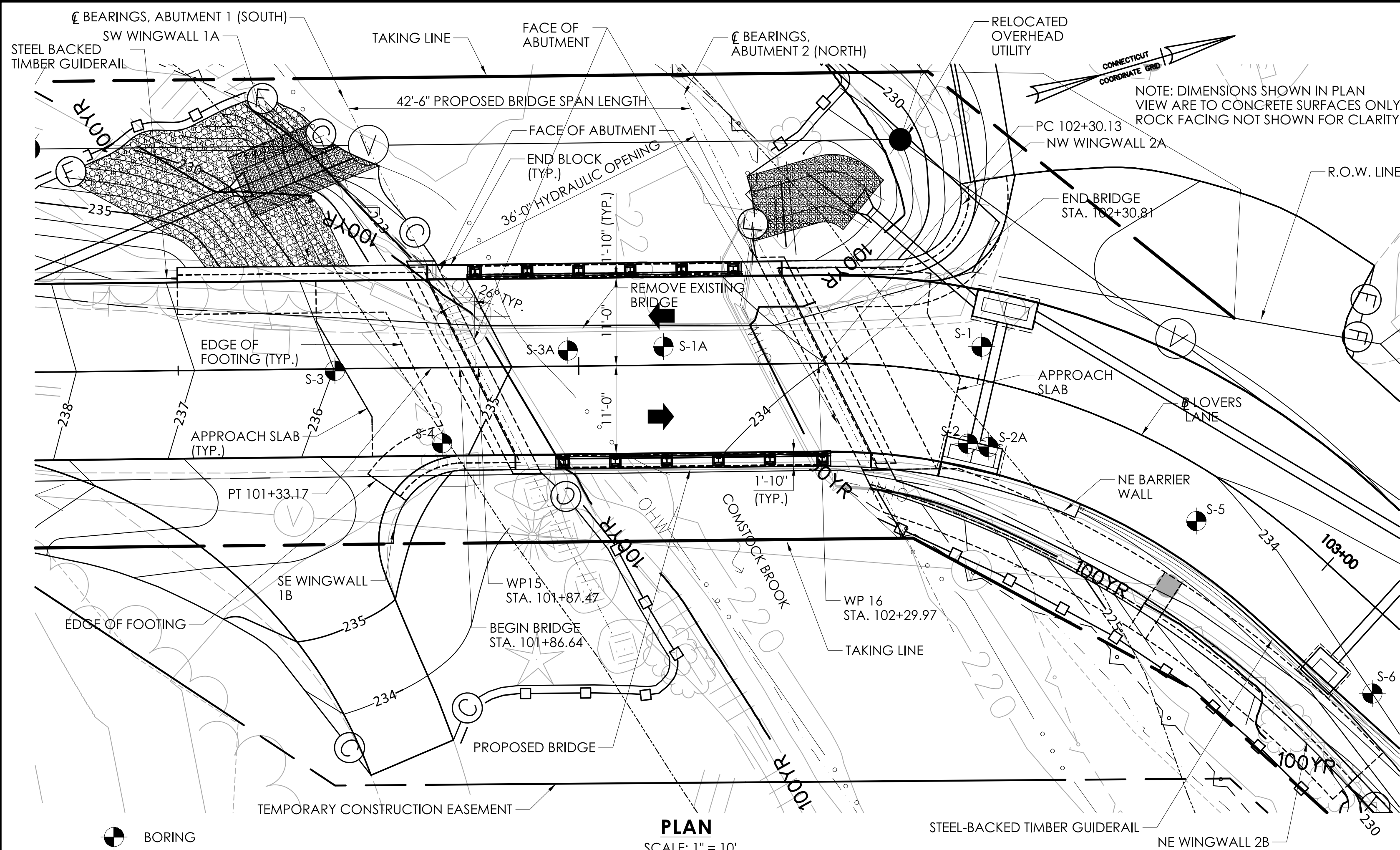
DESIGNER/DRAFTER: JT CHECKED BY: DW

SIGNATURE/
BLOCK:



PROJECT NUMBER: 0161-0142
PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04875 LOVERS LANE OVER COMSTOCK BROOK
TOWN(S): WILTON
DRAWING TITLE: INDEX OF DRAWINGS

DRAWING NO.
S-01
SHEET NO.



GENERAL NOTES:

SPECIFICATIONS: CONNECTICUT DEPARTMENT OF TRANSPORTATION FORM 818 (2020), SUPPLEMENTAL SPECIFICATIONS DATED JANUARY 2022 AND SPECIAL PROVISIONS.

DESIGN SPECIFICATIONS: AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS (9TH EDITION - 2020), AS SUPPLEMENTED BY THE CONNECTICUT DEPARTMENT OF TRANSPORTATION BRIDGE DESIGN MANUAL (2003) WITH REVISIONS DATED 2019.

MATERIAL STRENGTHS:

CONCRETE:
 CLASS PCC 03340 f_c = 3,000 PSI
 CLASS PCC 04462 f_c = 4,000 PSI
 PRC08062 (PRECAST CONCRETE) f_c = 8,000 PSI

THE SPECIFIED CONCRETE STRENGTH USED IN DESIGN, f_c, OF THE CONCRETE COMPONENTS IS NOTED ABOVE. THE MINIMUM COMPRESSIVE STRENGTH OF THE CONCRETE IN THE CONSTRUCTED COMPONENTS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 6.01 CONCRETE FOR STRUCTURES.

REINFORCEMENT (ASTM A615 GRADE 60) F_y = 60,000 PSI

LIVE LOAD: HL-93, LEGAL AND PERMIT VEHICLES

FUTURE PAVING ALLOWANCE: NONE

BITUMINOUS CONCRETE OVERLAY: THIS SHALL CONSIST OF TWO COURSES OF TRAFFIC LEVEL 2 MIXES. THE BOTTOM COURSE SHALL BE 1" HMA S0.25 AND THE TOP COURSE SHALL BE 2" HMA S0.5.

DIMENSIONS: WHEN DIMENSIONS ARE GIVEN TO LESS THAN THREE DECIMAL PLACES, THE OMITTED DIGITS SHALL BE ASSUMED TO BE ZEROS.

EXISTING DIMENSIONS: DIMENSIONS OF THE EXISTING STRUCTURE SHOWN ON THESE PLANS ARE FOR GENERAL REFERENCE ONLY. THEY HAVE BEEN TAKEN FROM THE ORIGINAL DESIGN DRAWINGS AND ARE NOT GUARANTEED. THE CONTRACTOR SHALL TAKE ALL FIELD MEASUREMENTS NECESSARY TO ASSURE PROPER FIT OF THE FINISHED WORK AND SHALL ASSUME FULL RESPONSIBILITY FOR THEIR ACCURACY. WHEN SHOP DRAWINGS BASED ON FIELD MEASUREMENTS ARE SUBMITTED FOR REVIEW, THE FIELD MEASUREMENTS SHALL ALSO BE SUBMITTED FOR REFERENCE BY THE REVIEWER.

MASH TEST LEVEL: THE 3 TUBE CURB MOUNTED BRIDGE RAILS MEET THE TL-4 CRITERIA FOR MASH 2016.

CONCRETE NOTES:

REMAIN-IN-PLACE FORMS: THE USE OF REMAIN-IN-PLACE FORMS ON THIS STRUCTURE IS NOT ALLOWED.

PENETRATING SEALER: PENETRATING SEALER PROTECTIVE COMPOUND SHALL BE APPLIED TO ALL EXPOSED SURFACES, INCLUDING CURBS AND END BLOCKS. SEE SPECIAL PROVISIONS.

EXPOSED EDGES: EXPOSED EDGES OF CONCRETE SHALL BE CHAMFERED 1" x 1" UNLESS DIMENSIONED OTHERWISE.

CONCRETE COVER: ALL REINFORCEMENT IN CAST-IN-PLACE AND PRECAST CONCRETE SHALL HAVE TWO INCHES COVER UNLESS DIMENSIONED OTHERWISE.

REINFORCEMENT: ALL REINFORCEMENT SHALL BE ASTM A615 GRADE 60.

GALVANIZED REINFORCING BARS: ALL REINFORCEMENT SHALL BE GALVANIZED AFTER FABRICATION UNLESS NOTED OTHERWISE. ALL REINFORCEMENT SHALL CONFORM TO THE REQUIREMENTS OF ASTM A767, CLASS 1, INCLUDING SUPPLEMENTAL REQUIREMENTS. THE COST OF FURNISHING AND PLACING THIS REINFORCEMENT SHALL BE INCLUDED IN THE ITEM "DEFORMED STEEL BARS - GALVANIZED."

CONSTRUCTION JOINTS: CONSTRUCTION JOINTS, OTHER THAN THOSE SHOWN ON THE PLANS WILL NOT BE PERMITTED WITHOUT THE PRIOR APPROVAL OF THE ENGINEER.

PREFORMED EXPANSION JOINT FILLER: THE COST OF FURNISHING AND INSTALLING PREFORMED EXPANSION JOINT FILLER IS PAID FOR AS "1" PREFORMED JOINT FILLER FOR BRIDGES".

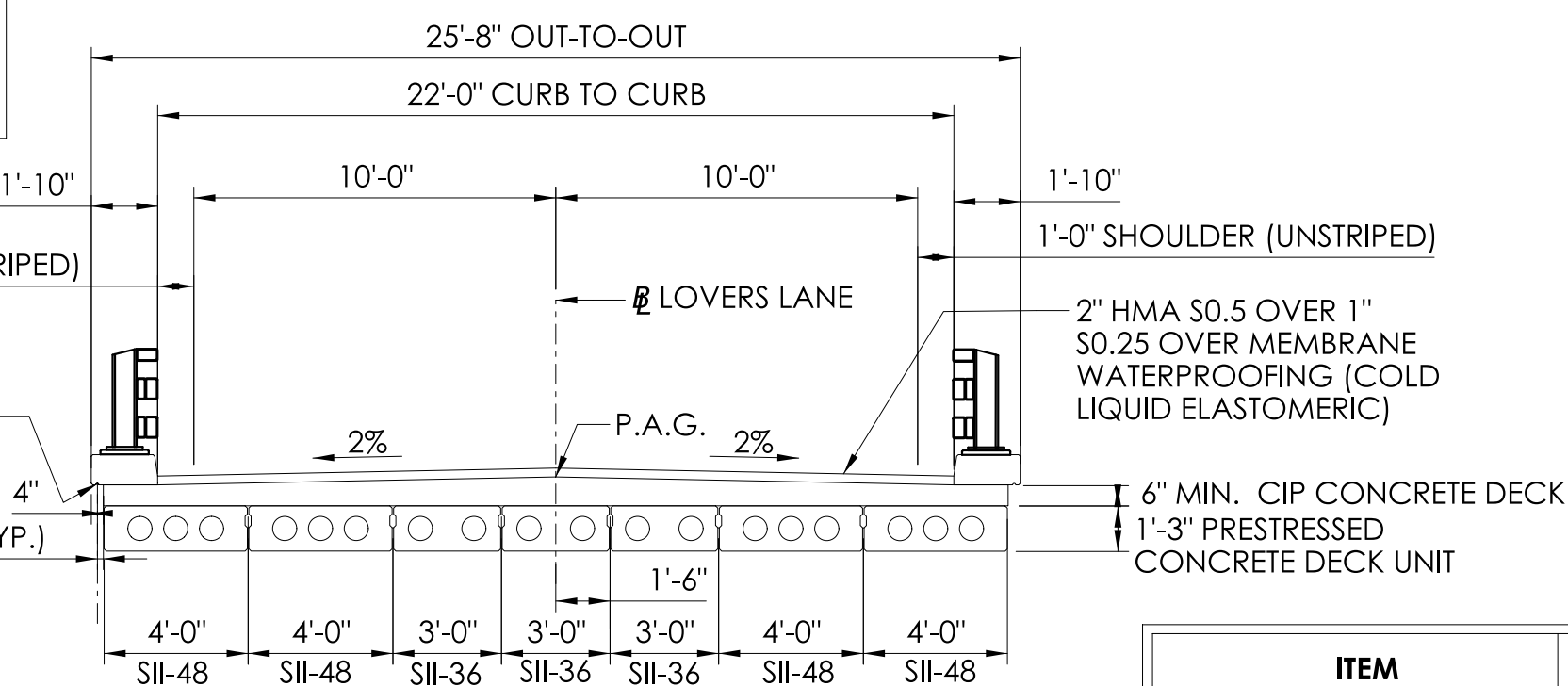
CLOSED CELL ELASTOMER: FURNISHING AND INSTALLED CLOSED CELL ELASTOMER SHALL BE INCLUDED IN THE ITEM "1" CLOSED CELL ELASTOMER".

PRECAST CONCRETE NOTES:

THE CONTRACTOR SHALL MANUFACTURE AND CONSTRUCT PRESTRESSED DECK UNITS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR "PRESTRESSED DECK UNITS" TO THE INSIDE DIMENSIONS, LENGTH AND DETAILS SHOWN ON THESE PLANS.

ALL INSERTS OR HOLES CAST INTO THE DECK UNITS FOR THE SOLE PURPOSE OF HANDLING AND SETTING THE UNITS SHALL BE SEALED WITH GROUT TO A SMOOTH FINISH UPON COMPLETION OF THE WORK.

THE COST OF FURNISHING AND INSTALLING THE INSERTS SHALL BE INCLUDED IN THE ITEM "PRESTRESSED DECK UNIT" CORRESPONDING TO EACH SIZE. ALL INSERTS SHALL HAVE A CORROSIVE RESISTANT COATING.



BRIDGE PLACARDS

BRIDGE NUMBER PLACARDS: THIS SIGN IS 4"x12" ALUMINUM SHEET METAL WITH WHITE REFLECTIVE LETTERS ON A GREEN REFLECTIVE BACKGROUND WITH 5 NUMERALS (04975). IT SHOULD BE LOCATED AT THE LEADING END OF EACH END BLOCK MOUNTED TO THE FRONT FACE OF THE END BLOCK WITH THE SIGN AND LEGEND READING HORIZONTALLY.

| ITEM | BRIDGE COMPONENTS | PCC CLASS |
|----------------------------|--|-----------|
| FOOTING CONCRETE | ABUTMENT AND WINGWALL FOOTINGS | PCC03340 |
| ABUTMENT AND WALL CONCRETE | ABUTMENT AND WINGWALL STEMS, BARRIER WALLS, CHEEKWALLS | PCC03340 |
| BRIDGE DECK CONCRETE | BRIDGE DECK | PCC04462 |
| PARAPET CONCRETE | END BLOCKS, CURBS | PCC04462 |
| APPROACH SLAB CONC. | APPROACH SLABS | PCC04462 |
| FLOWABLE FILL | SUBFOOTING | PCC03340 |

NOTICE TO BRIDGE INSPECTORS

THE DEPARTMENT'S BRIDGE SAFETY PROCEDURES REQUIRE THE BRIDGE TO BE INSPECTED FOR, BUT NOT LIMITED TO, ALL APPROPRIATE COMPONENTS INDICATED IN THE GOVERNING MANUALS FOR BRIDGE INSPECTION. ATTENTION MUST BE GIVEN TO INSPECTING THE FOLLOWING SPECIAL COMPONENTS AND DETAILS. (THE LISTING FOR COMPONENTS FOR SPECIFIC ATTENTION SHALL NOT BE CONSTRUCTED TO REDUCE THE IMPORTANCE OF INSPECTION OF ANY OTHER COMPONENT OF THE STRUCTURE). THE FREQUENCY OF INSPECTION OF THIS STRUCTURE SHALL BE IN ACCORDANCE WITH THE GOVERNING MANUALS FOR BRIDGE INSPECTION, UNLESS OTHERWISE DIRECTED BY THE MANAGER OF BRIDGE SAFETY AND EVALUATION.

| COMPONENT OR DETAIL | STRUCTURE SHEET REFERENCE |
|---------------------|---------------------------|
| NONE | N/A |

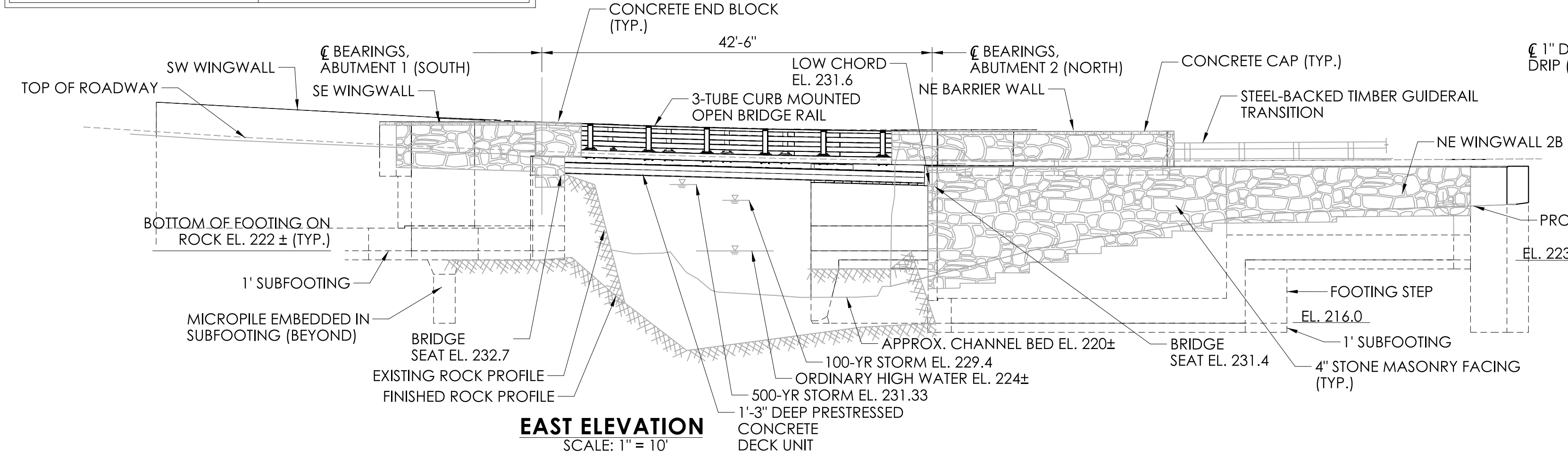
HYDRAULIC SUMMARY DATA

| | |
|-------------------------------------|---------------------------------|
| DRAINAGE AREA (MI ²) | 7.36 |
| DESIGN FREQUENCY (YEAR) | 100 |
| DESIGN DISCHARGE (CFS) | 1865 |
| AVERAGE DAILY FLOW ELEVATION (FT) | 221.9 |
| DESIGN WATER SURFACE ELEVATION (FT) | UPSTREAM 229.8 DOWNSTREAM 229.4 |
| MAXIMUM SCOUR ELEVATION (FT) | NOT CALCULATED |
| WORST CASE SCOUR SUBSTRUCTURE UNIT | NORTHERN ABUTMENT |

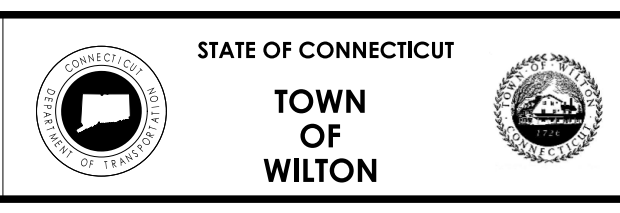
SHALLOW BEDROCK PRESENT AT SITE (WITHIN 7 FEET OF CHANNEL BED) WHICH IS THE LIMIT OF POTENTIAL SCOUR. BRIDGE FOUNDATIONS WILL BE DESIGNED TO BE STABLE TO MAXIMUM POTENTIAL SCOUR TO TOP OF BEDROCK.

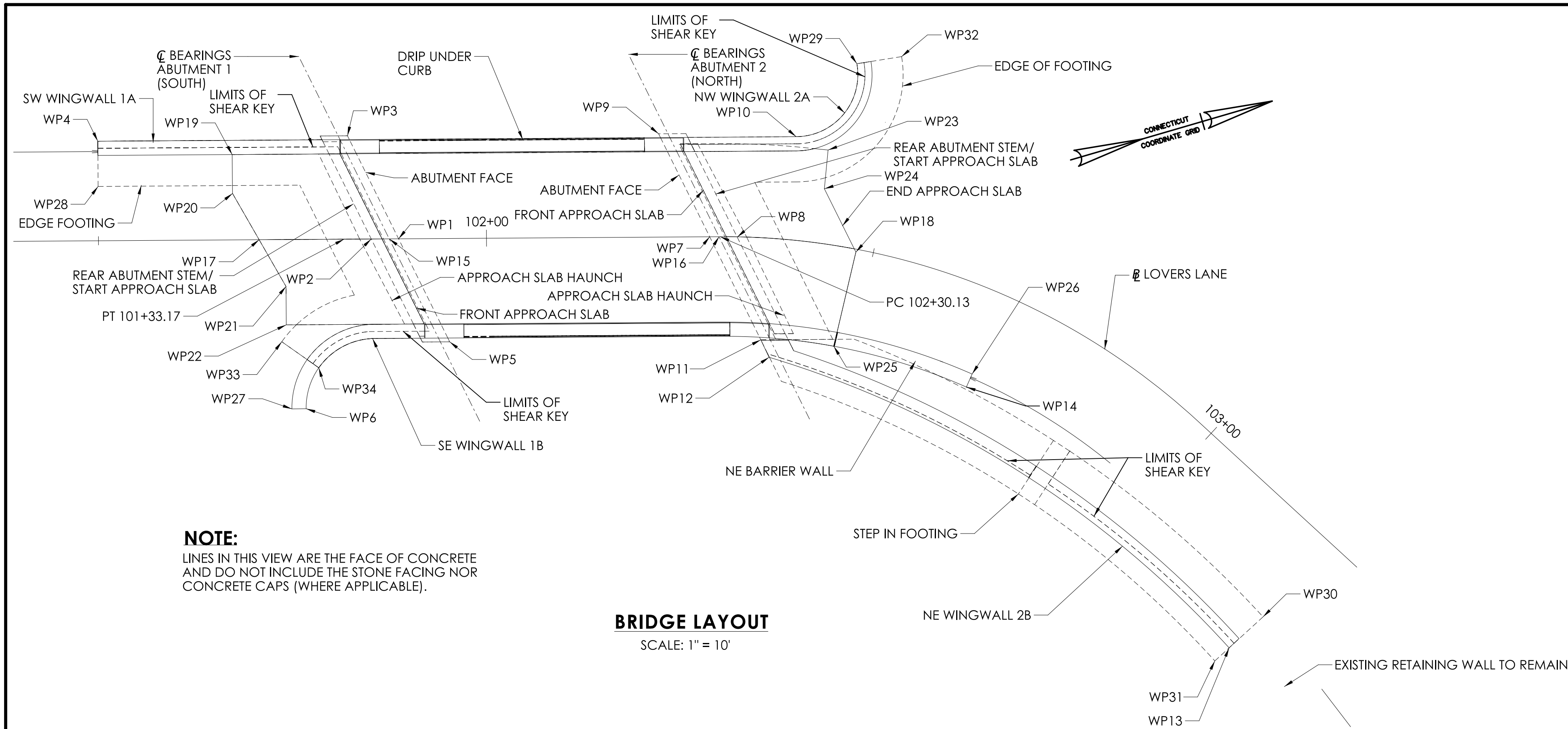
CONCRETE DISTRIBUTION

| | | |
|----------------|------|-----|
| SUPERSTRUCTURE | C.Y. | 56 |
| SUBSTRUCTURE | C.Y. | 147 |
| APPROACH SLAB | C.Y. | 128 |
| FOOTINGS | C.Y. | 186 |
| TOTAL | C.Y. | 517 |



| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |





NOTE:
 LINES IN THIS VIEW ARE THE FACE OF CONCRETE
 AND DO NOT INCLUDE THE STONE FACING NOR
 CONCRETE CAPS (WHERE APPLICABLE).

BRIDGE LAYOUT
 SCALE: 1" = 10'

| WP # | COORDINATES | | STATION | OFFSET |
|------|-------------|------------|-----------|---------|
| | NORTHING | EASTING | | |
| WP1 | 632850.615 | 810786.419 | 101+88.68 | 0.000 |
| WP2 | 632847.222 | 810785.469 | 101+85.15 | 0.000 |
| WP3 | 632847.948 | 810771.835 | 101+82.18 | -13.325 |
| WP4 | 632816.831 | 810763.632 | 101+50.00 | -12.837 |
| WP5 | 632853.282 | 810801.002 | 101+95.18 | 13.325 |
| WP6 | 632833.122 | 810804.187 | 101+76.75 | 20.297 |
| WP7 | 632889.222 | 810797.226 | 102+28.77 | 0.000 |
| WP8 | 632892.620 | 810798.203 | 102+32.30 | 0.000 |
| WP9 | 632886.555 | 810782.643 | 102+22.27 | -13.323 |
| WP10 | 632903.491 | 810787.892 | 102+38.74 | -13.305 |
| WP11 | 632891.887 | 810811.809 | 102+36.15 | 13.155 |
| WP12 | 632892.288 | 810814.265 | 102+37.59 | 15.327 |
| WP13 | 632938.938 | 810866.633 | 103+20.43 | 18.798 |
| WP14 | 632915.671 | 810824.973 | 102+68.08 | 12.833 |
| WP15 | 632849.455 | 810786.094 | 101+87.47 | 0.000 |
| WP16 | 632890.382 | 810797.549 | 102+29.97 | 0.000 |
| WP17 | 632833.310 | 810781.575 | 101+70.71 | 0.000 |
| WP18 | 632906.782 | 810804.319 | 102+47.78 | 0.383 |
| WP19 | 632832.992 | 810770.063 | 101+67.30 | -11.000 |
| WP20 | 632831.658 | 810774.895 | 101+67.31 | -5.987 |
| WP21 | 632834.978 | 810788.323 | 101+74.13 | 6.049 |
| WP22 | 632833.641 | 810793.090 | 101+74.13 | 11.000 |
| WP23 | 632906.971 | 810790.679 | 102+43.49 | -12.047 |
| WP24 | 632905.131 | 810795.349 | 102+42.65 | -7.031 |
| WP25 | 632900.699 | 810815.198 | 102+47.18 | 12.835 |
| WP26 | 632916.843 | 810823.563 | 102+68.08 | 11.000 |
| WP27 | 632831.352 | 810803.709 | 101+74.79 | 21.843 |
| WP28 | 632815.259 | 810769.247 | 101+50.00 | -7.006 |
| WP29 | 632913.607 | 810781.006 | 102+44.25 | -23.585 |
| WP30 | 632944.244 | 810863.997 | 103+20.94 | 12.819 |
| WP31 | 632936.705 | 810867.743 | 103+20.21 | 21.281 |
| WP32 | 632919.396 | 810781.713 | 102+48.58 | -25.489 |
| WP33 | 632832.486 | 810795.021 | 101+73.54 | 13.171 |
| WP34 | 632836.142 | 810799.563 | 101+78.28 | 16.559 |

| SHIPPING DATA FOR PRESTRESSED DECK UNITS | | | | |
|--|-----------------|-----------------|----------------|-----------------|
| MEMBER | SHIPPING LENGTH | SHIPPING HEIGHT | SHIPPING WIDTH | SHIPPING WEIGHT |
| SII-48 | 46'-1" | 1'-3" | 4'-0" | 25000 LBS |
| SII-36 | 46'-1" | 1'-3" | 3'-0" | 19800 LBS |

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |
| | | |

| Driller: | M. St. John | Connecticut DOT Boring Report Format | | | Hole No.: | S-1 | | |
|---|-----------------|---|-------------|--|---------------|--------------------------------|--|----------------|
| Inspector: | G. Jacobsen | Town: | Wilton | Stat./Offset: | 102+20/1 ft L | | | |
| Engineer: | Nathan Whetten | Project No.: | 161-142 | Northing: | 632910.48 | | | |
| Start Date: | 6-3-20 | Route No.: | Lovers Lane | Easting: | 810801.25 | | | |
| Finish Date: | 6-3-20 | Bridge No.: | 04975 | Surface Elevation: | 234 | | | |
| Project Description: Reconstruction of Lovers La over Comstock Brook | | | | | | | | |
| Casing Size/Type: 4-in. Casing | | Sampler Type/Size: 1-3/8 inch ID | | Core Barrel Type: NX | | | | |
| Hammer Wt.: 300lb Fall: 30in. | | Hammer Wt.: 140lb Fall: 30in. | | | | | | |
| Groundwater Observations: 12 0 hrs | | | | | | | | |
| Depth (ft) | SAMPLES | | | | | Generalized Strata Description | Material Description and Notes | Elevation (ft) |
| | Sample Type/No. | Blows on Sampler per 6 inches | Pen. (in.) | Rec. (in.) | RQD % | | | |
| 0 | | | | | | ASPHALT FILL | ASPHALT (3 in) | |
| 5 | S-1 | 18 9 10 15 | 24 | 7 | | | Brown to gray, coarse to fine SAND and m-f GRAVEL, trace silt | 230 |
| 10 | S-2 | 25 41 20 50/5" | 23 | 14 | | | Brown to gray, coarse to fine SAND, some m-f gravel, trace silt, with numerous concrete fragments | 225 |
| 15 | S-3 | 50/0" | 0 | 0 | | BEDROCK | Refusal, no recovery | 220 |
| 20 | C-1 | | 60 | 40 | 0 | | Moderately strong, moderately weathered, moderately to extremely fractured, gray graphitic SCHIST, with indistinct foliation. Primary joints low angle, planar, tight to open, weathered; several high angle joints. Coring times: 4-4-5-5 min/ft. | 215 |
| 25 | C-2 | | 36 | 33 | 14 | | Moderately strong to weak, moderately weathered, moderately to intensely fractured, gray to light gray GNEISS with layers of graphitic SCHIST. Coring times: 5-5-5 min/ft. | 210 |
| 30 | C-3 | | 48 | 44 | 21 | | Moderately strong to weak, moderately weathered, moderately to intensely fractured, gray to light gray GNEISS with layers of graphitic SCHIST. Intensely fractured zone from 19 to 22 ft. Coring times: 5-5-5 min/ft. | 205 |
| 35 | | | | | | | END OF BORING 22ft | 200 |
| Sample Type: S = Split Spoon C = Core UP = Undisturbed Piston V = Vane Shear Test | | | | | | | | |
| Proportions Used: Trace = 1 - 10%, Little = 10 - 20%, Some = 20 - 35%, And = 35 - 50% | | | | | | | | |
| Total Penetration in Earth: 10ft | | Rock: 12ft | | NOTES: Used solid augers to refusal at 10 feet, then drove 4-inch casing to 10 feet. | | Sheet 1 of 1 | | |
| No. of Soil Samples: 3 | | No. of Core Runs: 3 | | | | SM-001-M REV. 1/02 | | |

| Driller: | A. MacKernon | Connecticut DOT Boring Report Format | | | Hole No.: | S-1A | | |
|---|-----------------|---|-------------|--|---------------|---------------------------------|---|----------------|
| Inspector: | G. Jacobsen | Town: | Wilton | Stat./Offset: | 102+10/1 ft R | | | |
| Engineer: | Nathan Whetten | Project No.: | 161-142 | Northing: | 632872.36 | | | |
| Start Date: | 8-11-20 | Route No.: | Lovers Lane | Easting: | 810790.1 | | | |
| Finish Date: | 8-12-20 | Bridge No.: | 04975 | Surface Elevation: | 234.2 | | | |
| Project Description: Reconstruction of Lovers La over Comstock Brook | | | | | | | | |
| Casing Size/Type: 4-in. Casing | | Sampler Type/Size: 1-3/8 inch ID | | Core Barrel Type: NX | | | | |
| Hammer Wt.: 300lb Fall: 24in. | | Hammer Wt.: 140lb Fall: 30in. | | | | | | |
| Groundwater Observations: 13.5 0 hrs | | | | | | | | |
| Depth (ft) | SAMPLES | | | | | Generalized Strata Description | Material Description and Notes | Elevation (ft) |
| | Sample Type/No. | Blows on Sampler per 6 inches | Pen. (in.) | Rec. (in.) | RQD % | | | |
| 0 | | | | | | Pavement Structure Under Bridge | Bridge Deck (16") | |
| 5 | | | | | | | | 230 |
| 10 | | | | | | | | 225 |
| 15 | S-1 | 17 19 1431,50/1" | 25 | 10 | | Sand | Gray, coarse to fine SAND and c-f GRAVEL, little silt (Alluvium) Roller bit probable cobbles 17.1' to 20' | 220 |
| 20 | | | | | | Weathered Bedrock | Roller bit probable weathered bedrock 20' to 21'. Wash sample shows brown c-f SAND. Wash sample 21' to 22' shows light gray SILT and f-SAND. Wash sample 22' to 24' shows light gray SILT. Strong, slightly weathered, light gray, medium grained GNEISS. Several high angle joints, tight, slightly weathered planar to curved, smooth. Coring times: 16-12-13-15-12 min/ft. | 215 |
| 25 | C-1 | | 56 | 53 | 36 | | | 210 |
| 30 | | | | | | | END OF BORING 25.7ft | 205 |
| Sample Type: S = Split Spoon C = Core UP = Undisturbed Piston V = Vane Shear Test | | | | | | | | |
| Proportions Used: Trace = 1 - 10%, Little = 10 - 20%, Some = 20 - 35%, And = 35 - 50% | | | | | | | | |
| Total Penetration in Earth: 21ft | | Rock: 4.7ft | | NOTES: Used solid augers through bridge deck. Left a rough opening which would not allow 4" drive shoe to pass. Lowered Split Spoon for S1, then lowered 3" casing, but would not seat, no consistent water return. Eventually spun 4" casing through deck, and roller bit and advanced 4" casing to 20'. Cored rock with slow hydraulic motor | | Sheet 1 of 1 | | |
| No. of Soil Samples: 1 | | No. of Core Runs: 1 | | | | SM-001-M REV. 1/02 | | |

NOTE:
SEE S-02 FOR BORING LOCATIONS

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |

| | | |
|---|----------------|------------------|
| DESIGNER/DRAFTER: JT | CHECKED BY: DW | SIGNATURE/BLOCK: |
| LASTED SAVED BY: Jfor FILE NAME: J:\DWG\2016\0182\10\0161-0142\Bridg\Contract_Plans\4_SB_MSH_BR04975_0161_0142_BOR1.dgn | | |
| PLOTTED DATE: 10/21/2022 | | |



| | |
|--|------------------|
| PROJECT NUMBER: 0161-0142 | DRAWING NO. S-04 |
| PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVERS LANE OVER COMSTOCK BROOK | |
| TOWN(S): WILTON | SHEET NO. |
| DRAWING TITLE: BORING LOGS 1 | |

| Driller: M. St. John | Connecticut DOT Boring Report Format | | | Hole No.: S-2 | | | | |
|--|---|---|----|---------------|--------------------|--------------------------------|---|----------------|
| Inspector: G. Jacobsen | Town: Wilton | Stat./Offset: 102+20/10 ft R | | | | | | |
| Engineer: Nathan Whetten | Project No.: 161-142 | Northing: 632907.76 | | | | | | |
| Start Date: 6-4-20 | Route No.: Lovers Lane | Easting: 810813.5 | | | | | | |
| Finish Date: 6-4-20 | Bridge No.: 04975 | Surface Elevation: 234 | | | | | | |
| Project Description: Reconstruction of Lovers La over Comstock Brook | | | | | | | | |
| Casing Size/Type: 4-in. Casing | Sampler Type/Size: 1-3/8 inch ID | Core Barrel Type: NX | | | | | | |
| Hammer Wt.: 300lb Fall: 30in. | Hammer Wt.: 140lb Fall: 30in. | | | | | | | |
| Groundwater Observations: Not encountered | | | | | | | | |
| Depth (ft) | SAMPLES | | | | | Generalized Strata Description | Material Description and Notes | Elevation (ft) |
| | Sample Type/No. | Blows on Sampler per 6 inches | | | Pen. (in.) | | | |
| 0 | | | | | | ASPHALT FILL | ASPHALT (3 in) | |
| | S-1 | 20 | 12 | 6 | 4 | | Brown to gray, coarse to fine SAND, some c-f gravel, little silt | 230 |
| 5 | S-2 | 6 | 3 | 14 | 3 | | Brown c-f GRAVEL, some coarse to fine SAND, trace silt | |
| | S-3 | 50/3" | | | 3 | 2 | Brown coarse to fine SAND and c-f GRAVEL, little silt Cobbles and boulders from 8 to 10.5 ft | 225 |
| 10 | | | | | | | END OF BORING 10.5ft | 220 |
| 15 | | | | | | | | 215 |
| 20 | | | | | | | | 210 |
| 25 | | | | | | | | |
| Sample Type: S = Split Spoon C = Core UP = Undisturbed Piston V = Vane Shear Test Proportions Used: Trace = 1 - 10%, Little = 10 - 20%, Some = 20 - 35%, And = 35 - 50% | | | | | | | | |
| Total Penetration in Earth: 10.5ft Rock: 0ft | | NOTES: Used solid augers to refusal at 8 feet, then drove 4-inch casing to 8 feet. Roller-bitted through boulders to 10.5 ft; no water return Offset 3 ft north of S-2, drilled S-2A | | | Sheet 1 of 1 | | | |
| No. of Soil Samples: 3 | | No. of Core Runs: 0 | | | SM-001-M REV. 1/02 | | | |

| Driller: M. St. John | Connecticut DOT Boring Report Format | | | Hole No.: S-2A | | | | |
|--|---|---|--|----------------|--------------------|--------------------------------|--|----------------|
| Inspector: G. Jacobsen | Town: Wilton | Stat./Offset: 102+31/7 ft R | | | | | | |
| Engineer: Nathan Whetten | Project No.: 161-142 | Northing: 632905.49 | | | | | | |
| Start Date: 6-4-20 | Route No.: Lovers Lane | Easting: 810812.33 | | | | | | |
| Finish Date: 6-4-20 | Bridge No.: 04975 | Surface Elevation: 234 | | | | | | |
| Project Description: Reconstruction of Lovers La over Comstock Brook | | | | | | | | |
| Casing Size/Type: 4-in. Casing | Sampler Type/Size: 1-3/8 inch ID | Core Barrel Type: NX | | | | | | |
| Hammer Wt.: 300lb Fall: 30in. | Hammer Wt.: 140lb Fall: 30in. | | | | | | | |
| Groundwater Observations: 13 0 hrs | | | | | | | | |
| Depth (ft) | SAMPLES | | | | | Generalized Strata Description | Material Description and Notes | Elevation (ft) |
| | Sample Type/No. | Blows on Sampler per 6 inches | | | Pen. (in.) | | | |
| 0 | | | | | | ASPHALT FILL | ASPHALT (3 in) See boring S-2 for soil descriptions | 230 |
| 5 | | | | | | | Roller bit through cobbles and boulders from 9 to 10.5 ft | 225 |
| 10 | C-1 | | | | | BEDROCK | Moderately strong, moderately weathered, moderately to extremely fractured, gray medium grained GNEISS, with graphite schist layers. Foliation very thin, low angle. Primar joints shallow dipping, planar, tight to open, moderately to extremely weathered. Coring times: 4-4-4-4 min/ft. | 220 |
| 15 | C-2 | | | | | | Moderately strong, moderately weathered, moderately to extremely fractured, gray medium grained GNEISS, with graphite schist layers. Foliation very thin, low angle. Primar joints shallow dipping, planar, tight to open, moderately to extremely weathered. Coring times: 4-4-4-4 min/ft. | 215 |
| 20 | | | | | | | END OF BORING 20.5ft | 210 |
| 25 | | | | | | | | |
| Sample Type: S = Split Spoon C = Core UP = Undisturbed Piston V = Vane Shear Test Proportions Used: Trace = 1 - 10%, Little = 10 - 20%, Some = 20 - 35%, And = 35 - 50% | | | | | | | | |
| Total Penetration in Earth: 10.5ft Rock: 10ft | | NOTES: Offset 3 ft north of S-2, drilled S-2A Used solid augers to refusal at 9 feet, then drove 4-inch casing to 9 feet. Roller-bitted and drove casing through cobbles and boulders to 10.5 ft. | | | Sheet 1 of 1 | | | |
| No. of Soil Samples: 0 | | No. of Core Runs: 2 | | | SM-001-M REV. 1/02 | | | |

NOTE:
SEE S-02 FOR BORING LOCATIONS

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |

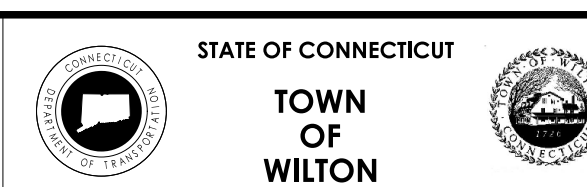
| | | | | | | | | |
|--|---|-------------------------------|---------------|--|-------|--------------------------------|--|--------------------|
| Driller: R. Posa | Connecticut DOT Boring Report Format | | Hole No.: S-3 | | | | | |
| Inspector: G. Jacobsen | Town: Wilton | Stat./Offset: 101+68/2 ft R | | | | | | |
| Engineer: Nathan Whetten | Project No.: 161-142 | Northing: 632832.14 | | | | | | |
| Start Date: 6-3-20 | Route No.: Lovers Lane | Easting: 810781.5 | | | | | | |
| Finish Date: 6-3-20 | Bridge No.: 04975 | Surface Elevation: 234.5 | | | | | | |
| Project Description: Reconstruction of Lovers La over Comstock Brook | | | | | | | | |
| Casing Size/Type: 4-in. Casing | Sampler Type/Size: 1-3/8 inch ID | Core Barrel Type: NX | | | | | | |
| Hammer Wt.: 300lb Fall: 30in. | Hammer Wt.: 140lb Fall: 30in. | | | | | | | |
| Groundwater Observations: 12 0 hrs | | | | | | | | |
| Depth (ft) | SAMPLES | | | | | Generalized Strata Description | Material Description and Notes | Elevation (ft) |
| | Sample Type/No. | Blows on Sampler per 6 inches | Pen. (in.) | Rec. (in.) | RQD % | | | |
| 0 | | | | | | ASPHALT FILL | ASPHALT (3 in) | |
| | S-1 | 23 11 10 12 | 24 | 14 | | | Brown to gray, coarse to fine SAND, some silt | |
| 5 | S-2 | 100/5" | 5 | 3 | | SILT | 4.0 to 5.5 ft.: Yellow-brown SILT, little fine sand (Subsoil) | 230 |
| | | | | | | BEDROCK | Roller bit through decomposed bedrock from 5.5 to 10 ft, cuttings light brown | |
| 10 | C-1 | | 60 | 60 | 40 | | Moderately strong, moderately weathered, moderately to extremely fractured, pink and dark gray medium grained GNEISS. Foliation very thin, shallow dipping. Primary joints low angle, planar, tight to open, moderately weathered, with sand filling | 225 |
| 15 | C-2 | | 60 | 59 | 8 | | Several vertical joints and cracks, some healed. Dark gray layer from 13.1 to 14 ft. Coring times: 7-7-7-7 min/ft. | 220 |
| 20 | | | | | | | Moderately strong, moderately weathered, moderately to extremely fractured, pink and dark gray medium grained GNEISS. Foliation very thin, shallow dipping. Primary joints low angle, planar, tight to open, moderately weathered, with sand filling | 215 |
| 25 | | | | | | | Coring times: 7-7-7-7 min/ft. | |
| | | | | | | | END OF BORING 20ft | 210 |
| Total Penetration in Earth: 5.5ft Rock: 14.5ft | | | | NOTES: Used solid augers to refusal at 9 feet, then drove 4-inch casing to 5.5 feet. Roller-bitted and drove casing through decomposed bedrock at 10 ft. | | | | Sheet 1 of 1 |
| No. of Soil Samples: 2 No. of Core Runs: 2 | | | | | | | | SM-001-M REV. 1/02 |

| | | | | | | | | |
|--|---|-------------------------------|----------------|---|-------|--------------------------------|---|--------------------|
| Driller: A. MacKernon | Connecticut DOT Boring Report Format | | Hole No.: S-3A | | | | | |
| Inspector: G. Jacobsen | Town: Wilton | Stat./Offset: 101+99/1 ft R | | | | | | |
| Engineer: Nathan Whetten | Project No.: 161-142 | Northing: 632860.75 | | | | | | |
| Start Date: 8-12-20 | Route No.: Lovers Lane | Easting: 810787.23 | | | | | | |
| Finish Date: 8-13-20 | Bridge No.: 04975 | Surface Elevation: 234.4 | | | | | | |
| Project Description: Reconstruction of Lovers La over Comstock Brook | | | | | | | | |
| Casing Size/Type: 4-in. Casing | Sampler Type/Size: 1-3/8 inch ID | Core Barrel Type: NX | | | | | | |
| Hammer Wt.: 300lb Fall: 30in. | Hammer Wt.: 140lb Fall: 30in. | | | | | | | |
| Groundwater Observations: 13.5 0 hrs | | | | | | | | |
| Depth (ft) | SAMPLES | | | | | Generalized Strata Description | Material Description and Notes | Elevation (ft) |
| | Sample Type/No. | Blows on Sampler per 6 inches | Pen. (in.) | Rec. (in.) | RQD % | | | |
| 0 | | | | | | Pavement Structure | Bridge Deck (22") | |
| | | | | | | Under Bridge | | |
| 5 | | | | | | | | 230 |
| 10 | | | | | | | | 225 |
| 15 | C-1 | | 12 | 1 | 0 | Sand | Water at 13.5' Cuttings shows c-f SAND. | 220 |
| | C-2 | | 24 | 22 | 0 | Weathered Bedrock | Roller bit to 15.8', cuttings show c-f SAND. | |
| 20 | C-3 | | 51 | 28 | 22 | Bedrock | [C1] Core barrel jammed at 16.8'. Coring times: 7 min/ft. | 215 |
| | | | | | | | [C2] Extremely fractured to 17.5', then: Strong, slightly weathered, pinkish gray, medium grained GNEISS. Primary joints high angle, planar, tight to open, weathered, rough to smooth. Coring times: 6-6 min/ft. | |
| 25 | | | | | | | [C3] Strong, slightly weathered, pinkish gray, medium grained GNEISS. Primary joints high angle, planar, tight to open, weathered, rough to smooth. Probably left core behind. Coring times: 6-7-7-8-5/4" min/ft. | 210 |
| | | | | | | | END OF BORING 23ft | |
| Total Penetration in Earth: 15.8ft Rock: 7.2ft | | | | NOTES: Augered to 1', then spun 4" casing through deck. Dropped casing to bottom of stream. | | | | Sheet 1 of 1 |
| No. of Soil Samples: 0 No. of Core Runs: 3 | | | | Initially spun 4" casing 14' to 14.5', then incrementally roller bit and drove casing. | | | | SM-001-M REV. 1/02 |
| | | | | Cored rock with high speed motor | | | | |

NOTE:
SEE S-02 FOR BORING LOCATIONS

| | | |
|------|------|----------------------|
| REV. | DATE | REVISION DESCRIPTION |
| | | |
| | | |

| | | |
|----------------------|----------------|------------------|
| DESIGNER/DRAFTER: JT | CHECKED BY: DW | SIGNATURE/BLOCK: |
|----------------------|----------------|------------------|



| |
|--|
| PROJECT NUMBER: 0161-0142 |
| PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVERS LANE OVER COMSTOCK BROOK |
| TOWN(S): WILTON |
| DRAWING TITLE: BORING LOGS 3 |

| |
|------------------|
| DRAWING NO. S-06 |
| SHEET NO. |

| Driller: | R. Posa | Connecticut DOT Boring Report Format | | Hole No.: | S-4 | | | |
|--|-----------------|---|--|----------------------|----------------|--------------------------------|---|----------------|
| Inspector: | G. Jacobsen | Town: | Wilton | Stat./Offset: | 101+83/14 ft R | | | |
| Engineer: | Nathan Whetten | Project No.: | 161-142 | Northing: | 632842.46 | | | |
| Start Date: | 6-4-20 | Route No.: | Lovers Lane | Easting: | 810793.93 | | | |
| Finish Date: | 6-4-20 | Bridge No.: | 04975 | Surface Elevation: | 235 | | | |
| Project Description: Reconstruction of Lovers La over Comstock Brook | | | | | | | | |
| Casing Size/Type: 4-in. Casing | | Sampler Type/Size: 1-3/8 inch ID | | Core Barrel Type: NX | | | | |
| Hammer Wt.: 300lb Fall: 30in. | | Hammer Wt.: 140lb Fall: 30in. | | | | | | |
| Groundwater Observations: 12 0 hrs | | | | | | | | |
| Depth (ft) | SAMPLES | | | | | Generalized Strata Description | Material Description and Notes | Elevation (ft) |
| | Sample Type/No. | Blows on Sampler per 6 inches | Pen. (in.) | Rec. (in.) | ROD % | | | |
| 0 | | | | | | ASPHALT FILL | ASPHALT (3 in) | 235 |
| | S-1 | 16 24 23 10 | 24 | 18 | | | Brown, coarse to fine SAND and SILT, little gravel | |
| | S-2 | 9 10 50/3" | 15 | 14 | | | 3 to 3.5 ft: Brown m-f GRAVEL, some c-f sand, little silt. 3.5 to 4.0 ft.: Yellow-brown SILT and fine SAND | |
| 5 | | | | | | BEDROCK | Probable decomposed bedrock from 4.0 to 6.0 ft | 230 |
| | C-1 | | 60 | 55 | 52 | | Strong, fresh, moderately fractured, pink and dark gray medium grained GNEISS. Foliation very thin, shallow dipping. Primary joints low angle, planar, tight, slightly weathered. Several vertical joints and cracks, some healed | 225 |
| | C-2 | | 60 | 60 | 72 | | Strong, fresh, moderately fractured, pink and dark gray medium grained GNEISS. Foliation very thin, shallow dipping. Primary joints low angle, planar, tight, slightly weathered. Several vertical joints and cracks, some healed Dark gray layer 14.1 to 14.9 ft. Coring times: 6-7-7-7 min/ft. | 220 |
| 20 | | | | | | | END OF BORING 16ft | 215 |
| 25 | | | | | | | | 210 |
| Sample Type: S = Split Spoon C = Core UP = Undisturbed Piston V = Vane Shear Test Proportions Used: Trace = 1 - 10%, Little = 10 - 20%, Some = 20 - 35%, And = 35 - 50% | | | | | | | | |
| Total Penetration in Earth: 4ft Rock: 12ft | | | NOTES: Used solid augers to 5 feet, then roller-bitted and drove casing to 6 ft. | | | Sheet 1 of 1 | | |
| No. of Soil Samples: 2 No. of Core Runs: 2 | | | | | | SM-001-M REV. 1/02 | | |

| Driller: | R. Posa | Connecticut DOT Boring Report | | Hole No.: | S-5 | | | |
|--|-----------------|--------------------------------------|---|----------------------|-----------------|--------------------------------|---|----------------|
| Inspector: | J. Herpich | Town: | Wilton | Stat./Offset: | 102+84/6.6 ft R | | | |
| Engineer: | Nathan Whetten | Project No.: | 161-142 | Northing: | 632935.1 | | | |
| Start Date: | 1-5-22 | Route No.: | Lovers Lane | Easting: | 810826.3 | | | |
| Finish Date: | 1-5-22 | Bridge No.: | 04975 | Surface Elevation: | 234 | | | |
| Project Description: Replacement of Lover's Lane over Comstock Brook | | | | | | | | |
| Casing Size/Type: 4-in. Casing | | Sampler Type/Size: 1-3/8 inch ID | | Core Barrel Type: NX | | | | |
| Hammer Wt.: 300lb Fall: 30in. | | Hammer Wt.: 140lb Fall: 30in. | | | | | | |
| Groundwater Observations: @Not encountered | | | | | | | | |
| Depth (ft) | SAMPLES | | | | | Generalized Strata Description | Material Description and Notes | Elevation (ft) |
| | Sample Type/No. | Blows on Sampler per 6 inches | Pen. (in.) | Rec. (in.) | ROD % | | | |
| 0 | | | | | | Asphalt Fill | ASPHALT (5 in) | |
| | S-1 | 17 29 19 12 | 24 | 18 | | | Top: Dark brown c-f SAND, some silt, trace f gravel; Bottom: Gray-brown c-f SAND and m-f GRAVEL, little silt | |
| | S-2 | 11 13 21 16 | 24 | 12 | | | Brown c-f SAND, little SILT, little m-f gravel | 230 |
| 5 | | | | | | | Top: Gray-brown clayey SILT, some m gravel, little c-f sand (moist); Bottom: Gray-brown c-m GRAVEL, some m-f sand, little silt | |
| | S-3 | 8 8 5 29 | 24 | 12 | | Glacial Till | Grey c-m GRAVEL, little m-f sand | 225 |
| | S-4 | 60 100/5" | 11 | 8 | | | | |
| 10 | S-5 | 50/1" | 1 | 1 | | Bedrock | Gray pulverized rock | 220 |
| | C-1 | | 60 | 60 | 40 | | GRANITE, gray, white, and pink, medium grained, massive bedding, moderately fractured, fresh, very strong. Penetration rate (ft/min): 2,3,3,3,7 | 215 |
| 15 | | | | | | | | |
| | C-2 | | 60 | 60 | 85 | | GRANITE, gray, white, and pink, medium grained, massive bedding, moderately fractured, fresh, very strong. Penetration rate (ft/min): 9,3,3,3,5 | 210 |
| 20 | | | | | | | END OF BORING 21ft | |
| 25 | | | | | | | | |
| Sample Type: S = Split Spoon C = Core UP = Undisturbed Piston V = Vane Shear Test Proportions Used: Trace = 1 - 10%, Little = 10 - 20%, Some = 20 - 35%, And = 35 - 50% | | | | | | | | |
| Total Penetration in Earth: 11ft Rock: 10ft | | | NOTES: Used solid augers to refusal at 11 feet, then drove 4-inch casing ans cored. Auger grinding from 9 to 10 feet | | | Sheet 1 of 1 | | |
| No. of Soil Samples: 5 No. of Core Runs: 2 | | | | | | SM-001-M REV. 1/02 | | |

NOTE:
SEE S-02 FOR BORING LOCATIONS

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |

DESIGNER/DRAFTER: JT CHECKED BY: DW

SIGNATURE/
BLOCK:



STATE OF CONNECTICUT
TOWN OF WILTON



PROJECT NUMBER: 0161-0142
PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVERS LANE OVER COMSTOCK BROOK
TOWN(S): WILTON
DRAWING TITLE: BORING LOGS 4

DRAWING NO.
S-07
SHEET NO.

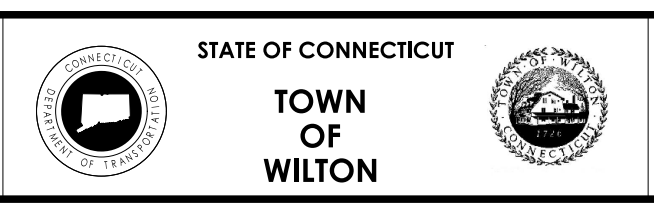
| Driller: | R. Posa | Connecticut DOT Boring Report Format | | | Hole No.: | S-6 | | | |
|---|-----------------|---|-------------|---|-------------------|--------------------------------|---|----------------------|------------|
| Inspector: | J. Herpich | Town: | Wilton | Stat./Offset: | 103+12.7/8.9 ft R | | | | |
| Engineer: | Nathan Whetten | Project No.: | 161-142 | Northing: | 632951 | | | | |
| Start Date: | 1-5-22 | Route No.: | Lovers Lane | Easting: | 810850.2 | | | | |
| Finish Date: | 1-5-22 | Bridge No.: | 04975 | Surface Elevation: | 234.2 | | | | |
| Project Description: Replacement of Lover's Lane over Comstock Brook | | | | | | | | | |
| Casing Size/Type: 4-in. Casing | | Sampler Type/Size: 1-3/8 inch ID | | Core Barrel Type: NX | | | | | |
| Hammer Wt.: 300lb Fall: 30in. | | Hammer Wt.: 140lb Fall: 30in. | | | | | | | |
| Groundwater Observations: Not encountered | | | | | | | | | |
| Depth (ft) | SAMPLES | | | | | Generalized Strata Description | Material Description and Notes | Elevation (ft) | |
| | Sample Type/No. | Blows on Sampler per 6 inches | | | | | | | Pen. (in.) |
| 0 | | | | | | Asphalt Fill | ASPHALT (5 in) | | |
| | S-1 | 16 | 23 | 15 | 15 | | Top 8": Brown c-f SAND, some silt, little m-f GRAVEL; Bottom 8": Brown c-f SAND, little f gravel, little silt | | |
| | | | | | | Glacial Till | | 230 | |
| 5 | S-2 | 50/1" | | | | 1 | 1 | Gray pulverized rock | |
| | | | | | | | END OF BORING 6ft | | |
| 10 | | | | | | | | 225 | |
| 15 | | | | | | | | 220 | |
| 20 | | | | | | | | 215 | |
| 25 | | | | | | | | 210 | |
| Sample Type: S = Split Spoon C = Core UP = Undisturbed Piston V = Vane Shear Test | | | | | | | | | |
| Proportions Used: Trace = 1 - 10%, Little = 10 - 20%, Some = 20 - 35%, And = 35 - 50% | | | | | | | | | |
| Total Penetration in Earth: 6ft Rock: 0ft | | | | NOTES: Used solid augers to refusal at 6 feet Auger grinding from 3 to 6 feet | | | Sheet 1 of 1 | | |
| No. of Soil Samples: 2 | | No. of Core Runs: 0 | | | | | SM-001-M REV. 1/02 | | |

NOTE:
SEE S-02 FOR BORING LOCATIONS

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |

DESIGNER/DRAFTER: JT CHECKED BY: DW

SIGNATURE/BLOCK:

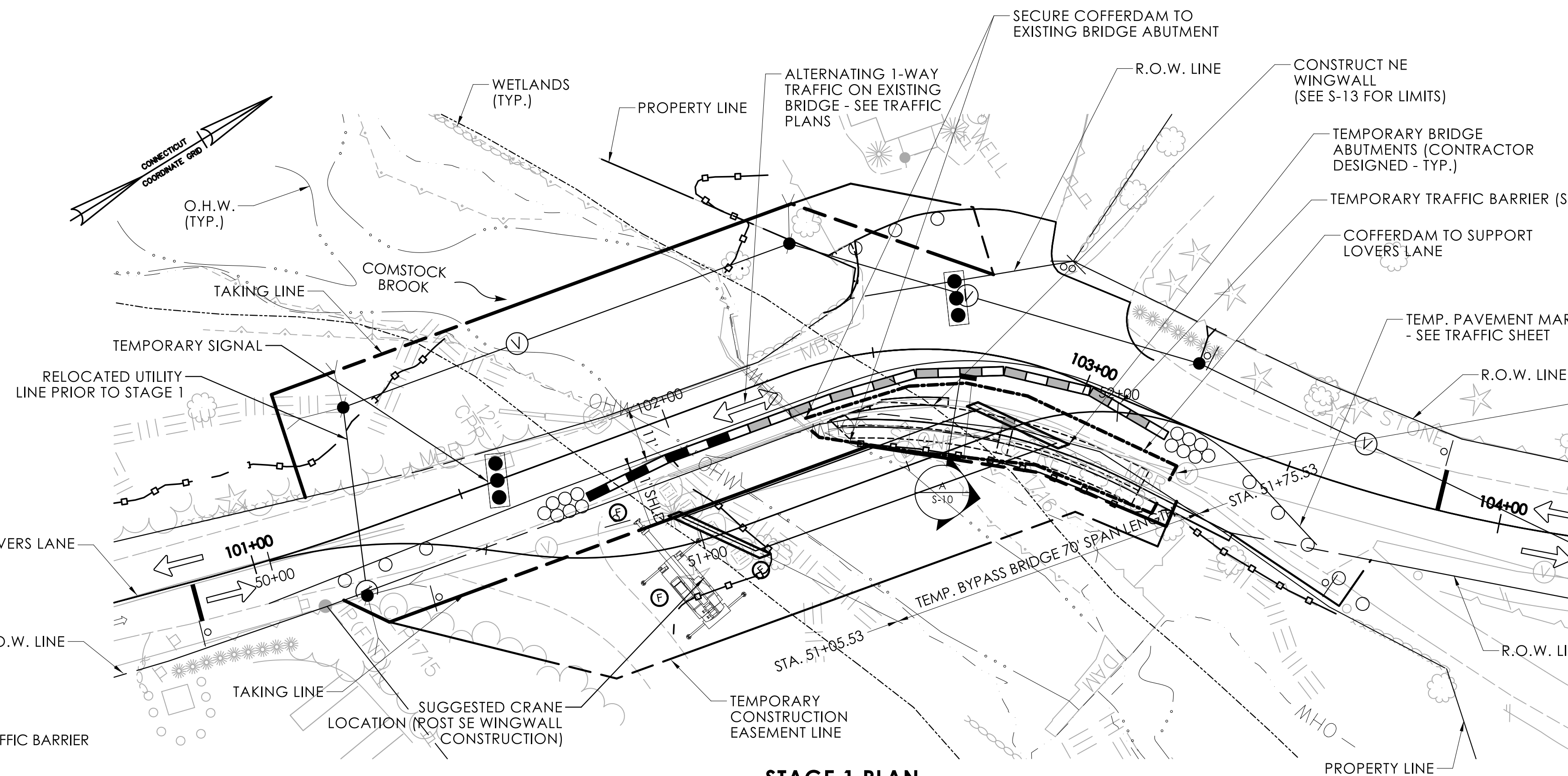


PROJECT NUMBER: 0161-0142
 PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVERS LANE OVER COMSTOCK BROOK
 TOWN(S): WILTON
 DRAWING TITLE: BORING LOGS 5

DRAWING NO. S-08
 SHEET NO.

LEGEND

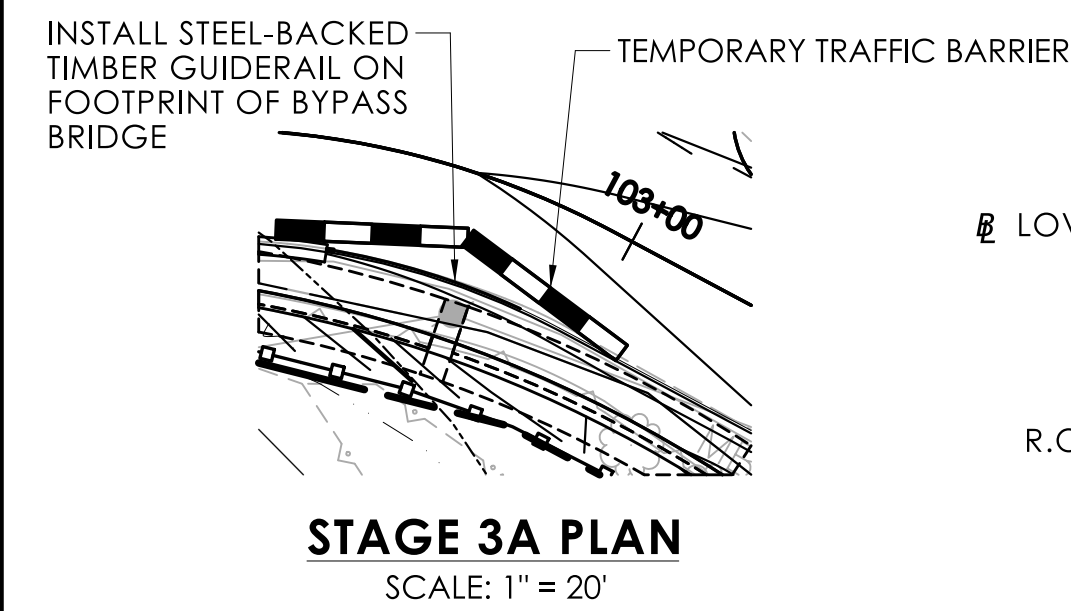
----- WATER HANDLING COFFERDAM



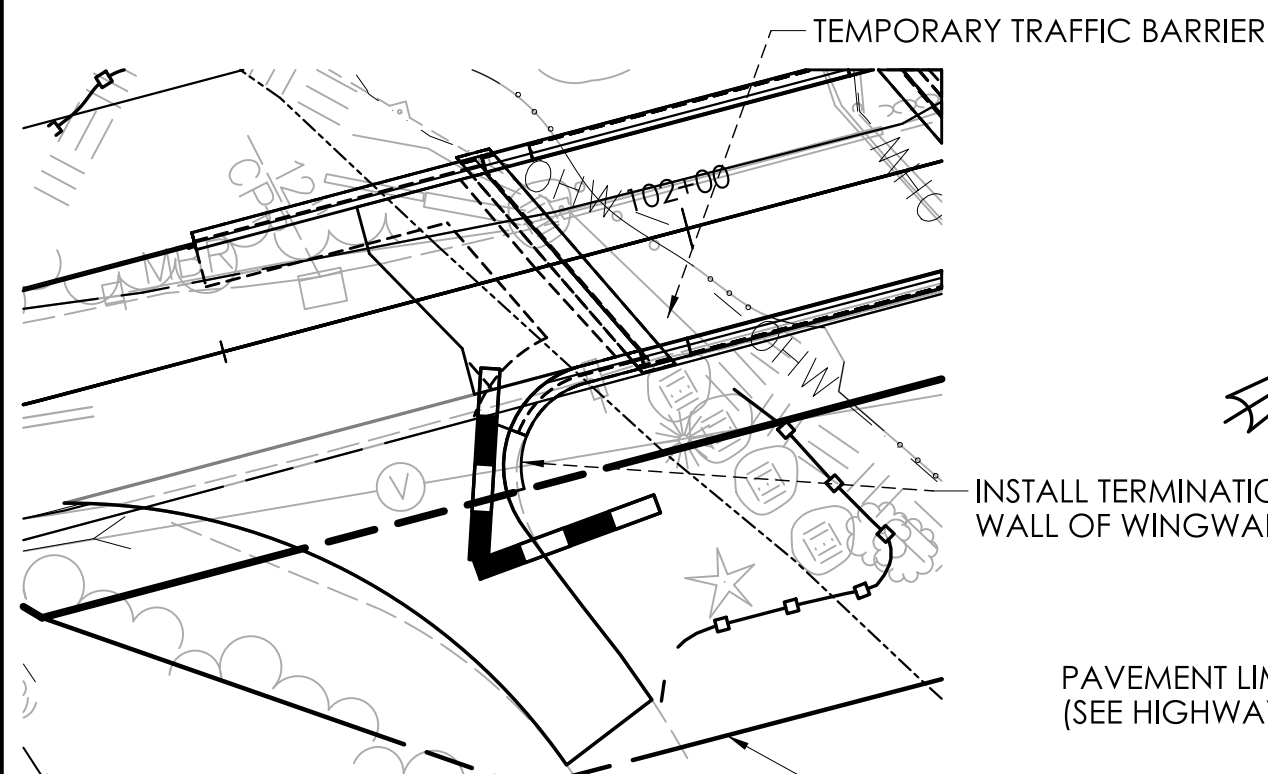
STAGE 1 PLAN
SCALE: 1" = 20'

SUGGESTED STAGE 1 CONSTRUCTION SEQUENCE

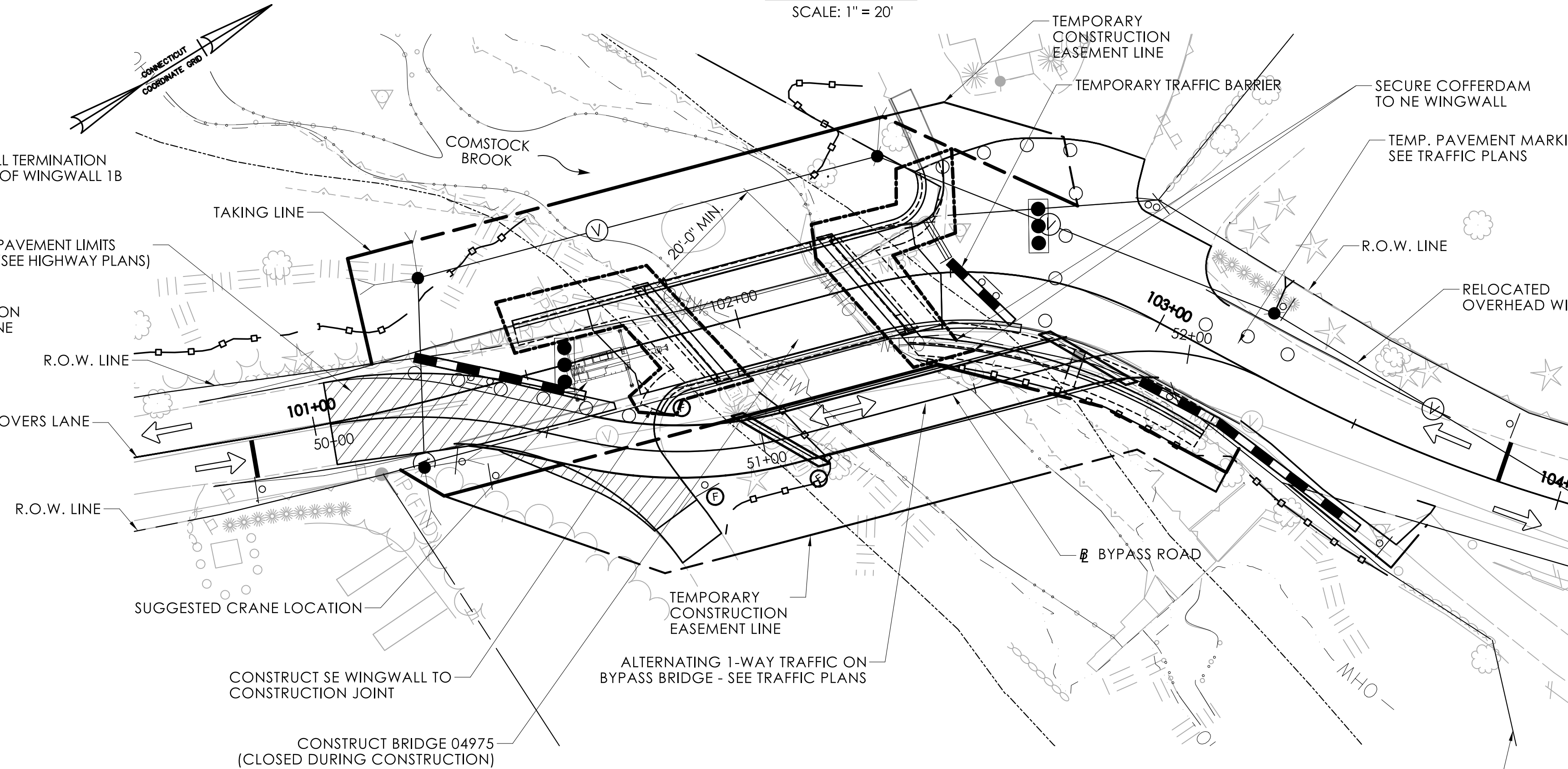
1. RELOCATE OVERHEAD UTILITY PRIOR TO STAGE 1.
2. PERFORM SITE CLEARING AND GRUBBING ACTIVITIES.
3. INSTALL SEDIMENTATION CONTROL SYSTEM.
4. IMPLEMENT STAGE 1 MPT PLAN AND SHIFT TRAFFIC TOWARD THE WEST CURB OF EXISTING BRIDGE.
5. INSTALL STAGE 1 TEMPORARY COFFERDAM.
6. DEMOLISH EXISTING NE WINGWALL (SEE S-10 FOR LIMITS AND SECTION)
7. CONSTRUCT PROPOSED NORTHEAST AND SOUTHEAST WINGWALL. (SEE S-13 FOR LIMITS AND SECTION), IF REQUIRED, ALLOW GAP IN NORTHEAST WINGWALL TO ACCEPT TEMPORARY BRIDGE SUPERSTRUCTURE
8. BACKFILL BEHIND NE WINGWALL AND REMOVE STAGE 1 TEMPORARY COFFERDAM.
9. CONSTRUCT TEMPORARY BYPASS BRIDGE ABUTMENTS IN FILL BEHIND NE WINGWALL.
10. INSTALL TEMPORARY BYPASS BRIDGE (CONTRACTOR DESIGN).
11. REMOVE STAGE 1 TRAFFIC CONTROL FEATURES.



STAGE 3A PLAN
SCALE: 1" = 20'



STAGE 3B PLAN
SCALE: 1" = 20'



STAGE 2 PLAN
SCALE: 1" = 20'

SUGGESTED STAGE 2 CONSTRUCTION SEQUENCE

1. IMPLEMENT STAGE 2 MPT PLAN AND SHIFT TRAFFIC ONTO TEMPORARY ROAD AND BRIDGE.
2. INSTALL DEBRIS SHIELD ABOVE EL. 228 (MIN.) AND REMOVE EXISTING BRIDGE SUPERSTRUCTURE (SEE S-10 FOR DEMO LIMITS).
3. INSTALL STAGE 2 TEMPORARY COFFERDAMS.
4. DEMOLISH EXISTING SUPERSTRUCTURE, ABUTMENTS AND WINGWALLS, PERFORM STRUCTURE EXCAVATION.
5. PERFORM VERIFICATION TEST ON MICROPILES AND SUBMIT TEST RESULTS FOR APPROVAL.
7. INSTALL MICROPILES AS SHOWN IN PLANS AND AS REQUIRED ELSEWHERE (SEE S-11).
8. PERFORM PROOF TEST ON 1 PILE.
9. CONSTRUCT PROPOSED FOOTINGS FOR ABUTMENTS AND WINGWALLS.
10. CONSTRUCT PROPOSED ABUTMENTS & WINGWALLS.
11. BACKFILL BEHIND ABUTMENTS UP TO APPROACH SLAB ELEVATION.
12. REMOVE STAGE 2 TEMPORARY COFFERDAMS.
13. INSTALL BEARINGS AND DECK UNITS. THEN, CONSTRUCT APPROACH SLABS, AND DECK. INSTALL VERTICAL SEGMENT BARRIER WALL PAST CB IN TRENCH BOX.
14. INSTALL BRIDGE RAILING AND END BLOCKS.
15. COMPLETE APPROACH ROADWAY WORK AND REOPEN BRIDGE TO TRAFFIC.
16. REMOVE STAGE 2 TEMPORARY TRAFFIC MARKINGS, AND SHIFT 2-WAY TRAFFIC TO BRIDGE
17. REMOVE TEMPORARY BYPASS BRIDGE AND ABUTMENTS, AND FINISH SE WINGWALL CONSTRUCTION AND INSTALL GUIDERAIL AT NE APPROACH.
18. RESTORE DISTURBED AREAS WITH TURF ESTABLISHMENT AND PLANTING.

SUGGESTED STAGE 3 CONSTRUCTION SEQUENCE

1. REMOVE TEMPORARY BYPASS BRIDGE AND ABUTMENTS. EXTEND NE RETAINING WALL AS REQUIRED.
2. INSTALL TIMBER BACKED GUARDRAIL BEHIND TEMPORARY BARRIER AS SHOWN (STAGE 3A)
3. INSTALL EXTENSION OF WINGWALL 1B IN A TRENCHBOX (STAGE 3B)
4. RESTORE DISTURBED AREA WITH TURF ESTABLISHMENT AND PLANTINGS.

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |

DESIGNER/DRAFTER: JT CHECKED BY: DW

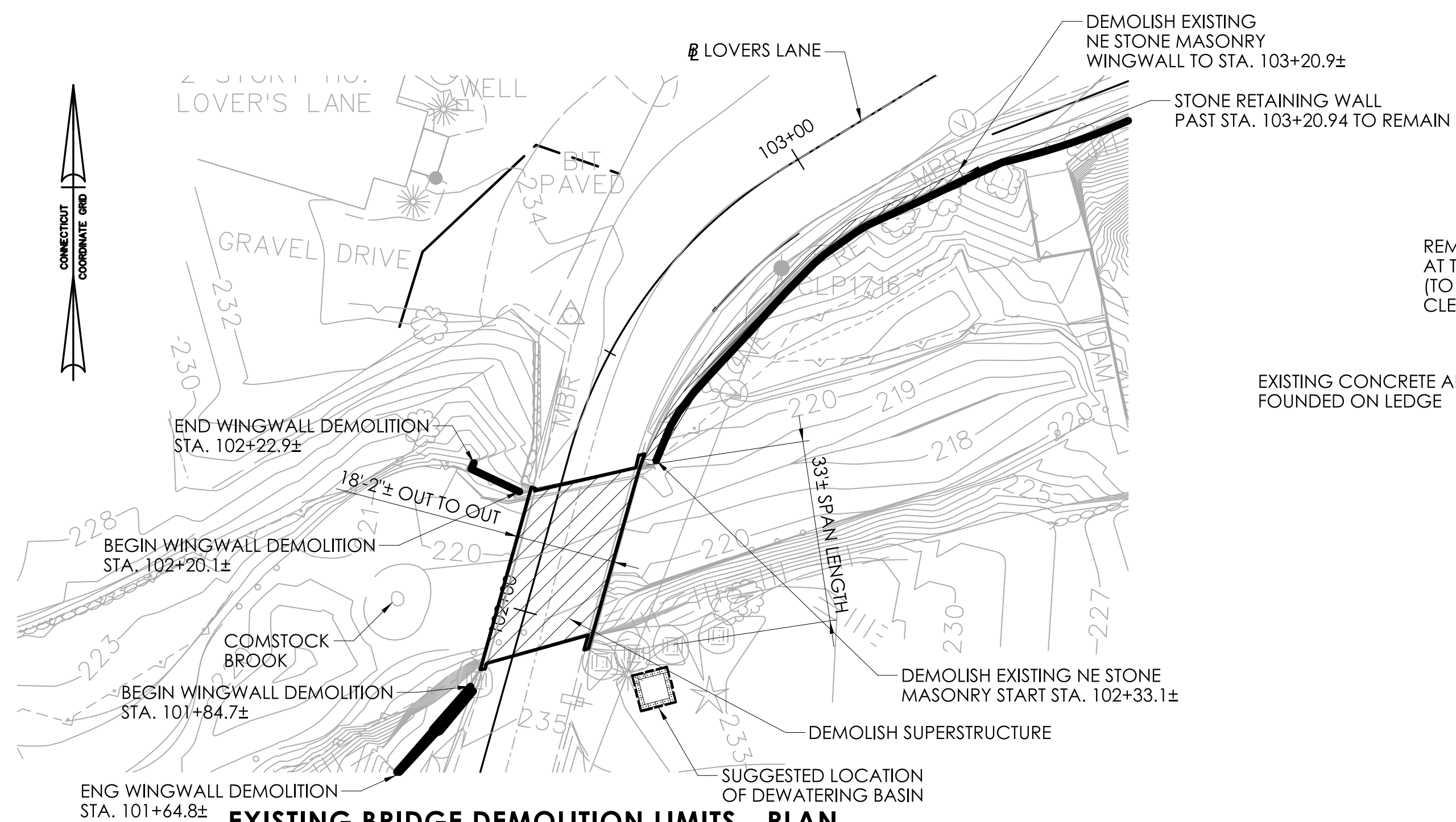
SIGNATURE/
BLOCK:

LASTED SAVED BY: DWhittemore FILE NAME: J:\DWG\2016\0182\010\0161-0142\bridge\Contract_Plans\9_S8_MSH_Br04975_0161_0142_STG1.dgn
PLOTTED DATE: 10/21/2022



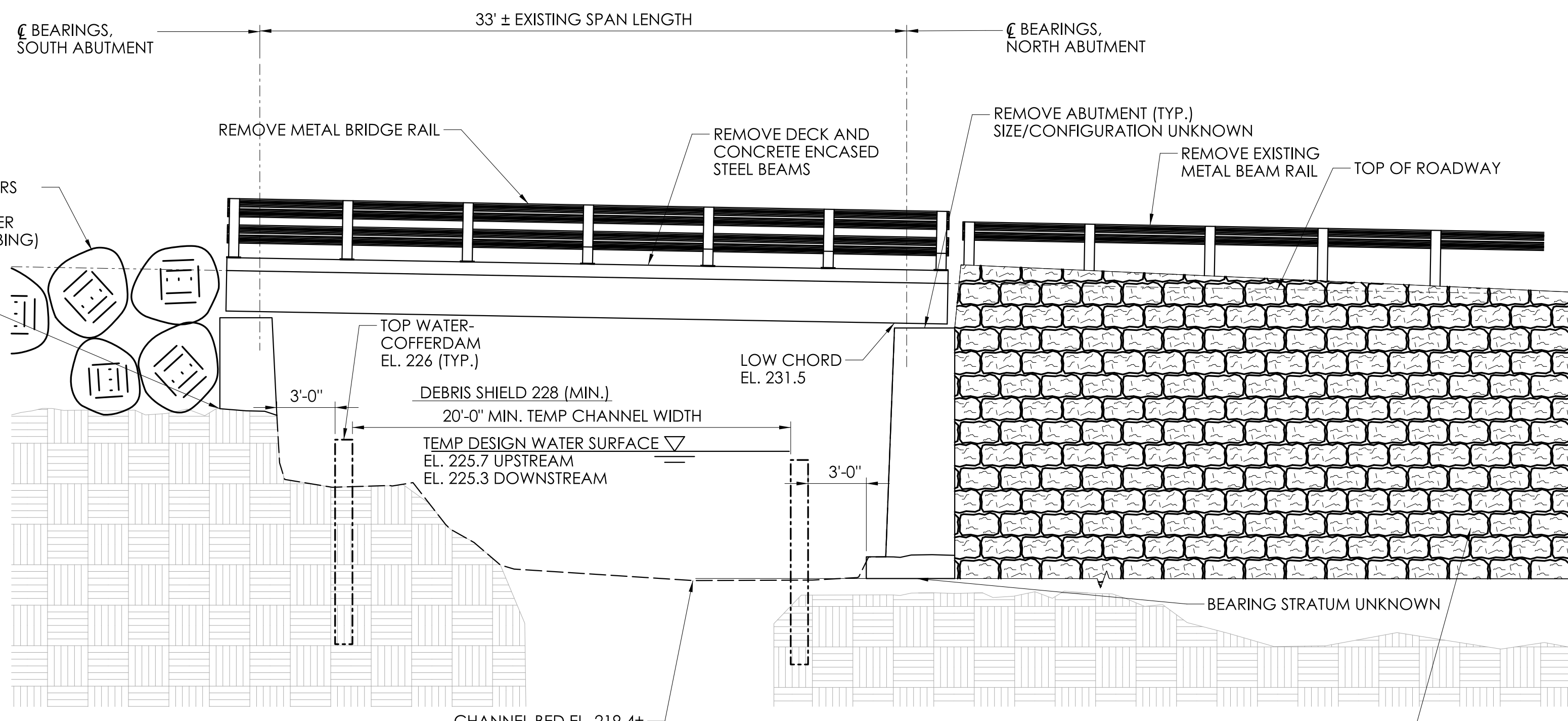
PROJECT NUMBER: 0161-0142
PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04875 LOVERS LANE OVER COMSTOCK BROOK
TOWN(S): WILTON
DRAWING TITLE: STAGING PLAN 1

DRAWING NO. S-09
SHEET NO.



EXISTING BRIDGE DEMOLITION LIMITS - PLAN

SCALE: 1" = 20'



EXISTING BRIDGE DEMOLITION ELEVATION (DOWNSTREAM)

SCALE: 1" = 5'

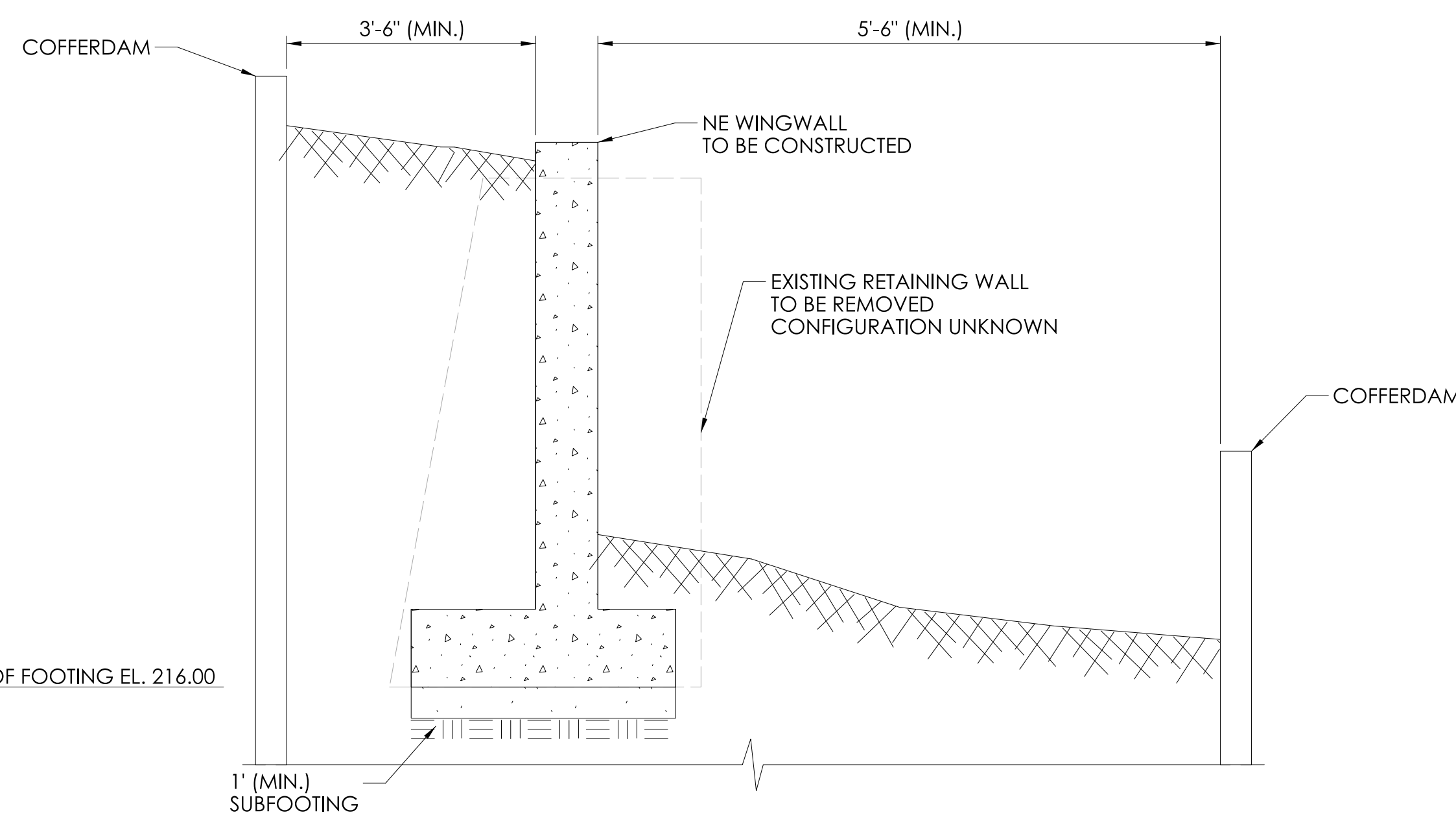
DEMOLITION NOTES

1. REMOVAL OF EXISTING SUPERSTRUCTURE IN ITS ENTIRETY INCLUDE OVERLAY, BRIDGE RAIL, DECK, STEEL BEAMS ENCASED IN CONCRETE SHALL BE PAID UNDER ITEM "REMOVAL OF SUPERSTRUCTURE".
2. A DEBRIS SHIELD SHALL BE INSTALLED AT EL. 228 MIN. PRIOR TO REMOVAL OF SUPERSTRUCTURE.
3. REMOVAL OF EXISTING SUBSTRUCTURE INCLUDING ABUTMENTS AND WINGWALLS TO BE PAID UNDER THE ITEM "REMOVAL OF MASONRY".

COFFERDAM AND DEWATERING NOTES

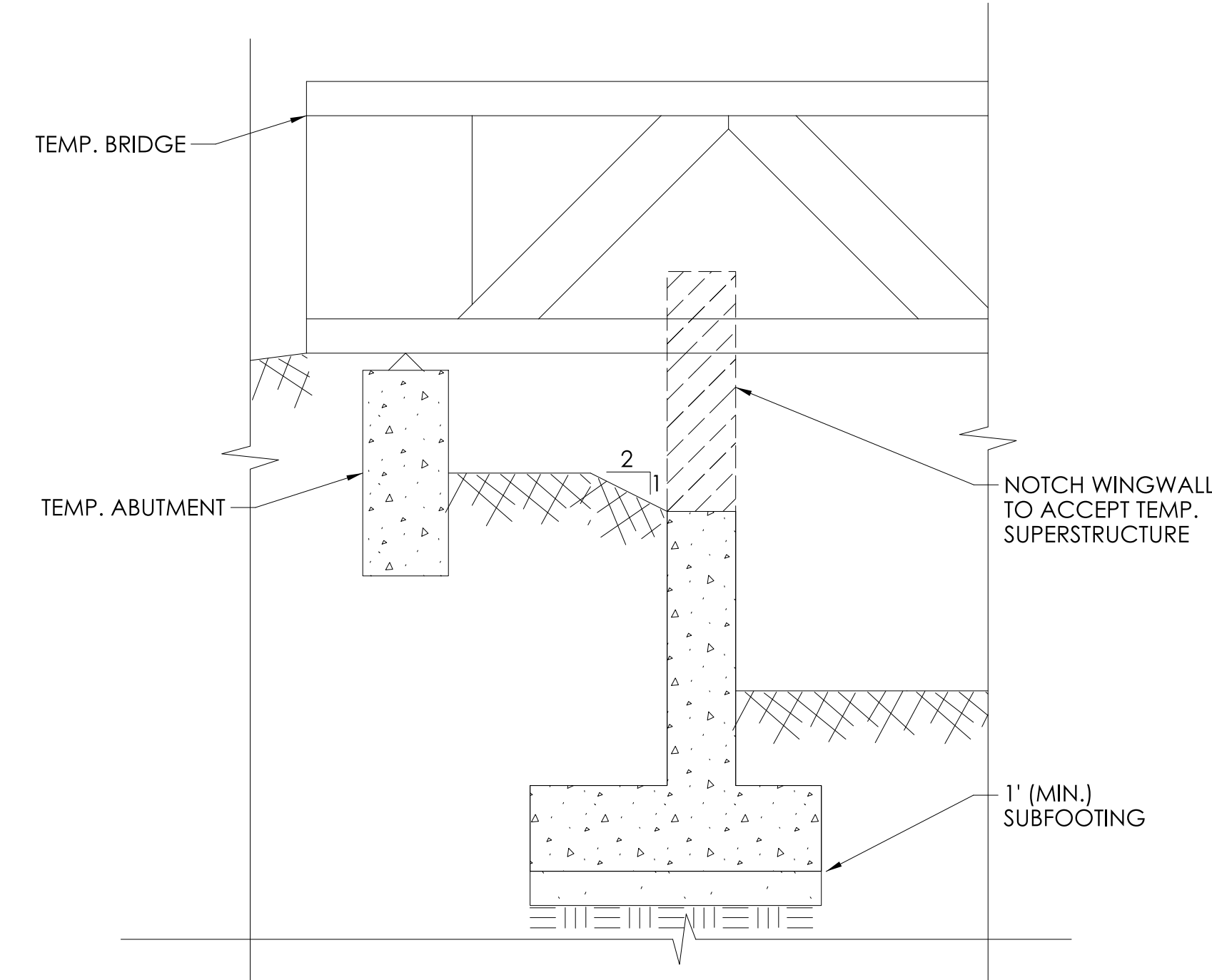
1. THE CONTRACTOR SHALL MAINTAIN WATER FLOW AND FISH PASSAGE THROUGH SITE DURING CONSTRUCTION OF THE NEW STRUCTURE.
2. EQUIPMENT SHALL NOT BE PERMITTED IN THE WATERCOURSE WHEN COFFERDAM IS NOT IN PLACE.
3. A PUMP DISCHARGE BASIN SHALL BE ESTABLISHED OUTSIDE OF THE WETLAND LIMITS AND WITHIN THE RIGHT OF WAY IF POSSIBLE. THE LOCATION OF THE DEWATERING BASIN SHOWN IN DWG. S-09 IS APPROXIMATE. THE EXACT POSITION MAY VARY BASED ON THE PUMPING DESIGN SUBMISSION AND APPROVED BY THE ENGINEER. DEWATER WORK AREAS BY PUMPING TO DEWATERING BASIN.
4. COFFERDAM SHALL CONSIST OF A SUITABLE SYSTEM THAT THE CONTRACTOR ELECTS TO USE WHICH WILL SAFELY CONVEY WATER FLOWS THROUGH THE CONSTRUCTION AREA, SHALL BE ABLE TO SUPPORT CONSTRUCTION ACTIVITY AND EXCAVATION, AND SHALL CONFORM TO PERMITS.
5. COFFERDAM AND DEWATERING SHALL BE PAID FOR UNDER THE ITEM "COFFERDAM AND DEWATERING".

| TEMPORARY HYDRAULICS SUMMARY DATA | |
|--|---------------------------------------|
| AVERAGE DAILY FLOW (CFS) | 13 |
| AVERAGE SPRING FLOW (CFS) | 26 |
| 2-YEAR FREQUENCY DISCHARGE (CFS) | 500 |
| TEMPORARY DESIGN DISCHARGE (CFS) | 500 |
| TEMPORARY DESIGN FREQUENCY | 2 YEAR FLOW |
| TEMPORARY WATER SURFACE ELEVATION (FT) | 225.7 UP STREAM, 225.3 DOWN STREAM |



EXISTING NE WINGWALL DEMOLITION

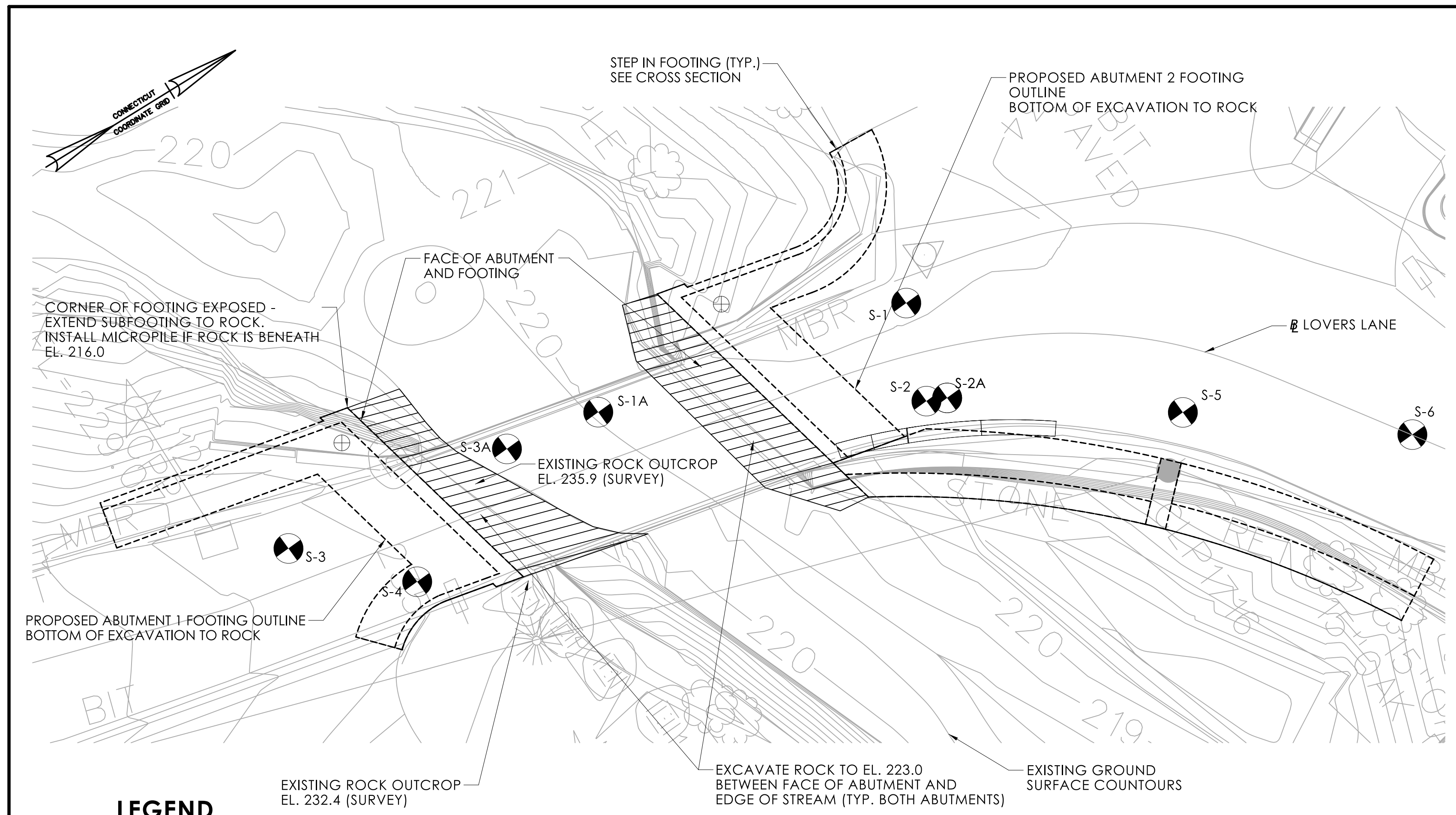
SCALE: N.T.S.



NE WINGWALL AND BYPASS BRIDGE DETAIL

SCALE: N.T.S.

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |



BEDROCK ELEVATIONS

| BORING | ELEVATION |
|--------|-----------|
| S-1 | 224 |
| S-1A | 213.2 |
| S-2 | N/A |
| S-2A | 223.5 |
| S-3 | 224.5 |
| S-3A | 216.9 |
| S-4 | 229 |
| S-5 | 224 |
| S-6 | 228 |

BEDROCK PROFILE NOTES AND EXCAVATION

- 1) WHERE THE ROCK OUTCROPPINGS WITHIN THE LIMITS OF THE PROPOSED FOOTINGS EXTEND ABOVE ELEVATION 223, ROCK SHALL BE EXCAVATED DOWN TO THIS ELEVATION.
- 2) FOR ROCK ENCOUNTERED BETWEEN ELEVATIONS 223 AND 216, THE CONTRACTOR SHALL EXCAVATE TO TOP OF BED ROCK. A CONCRETE SUBFOOTING (SEE S-12 FOR SUBFOOTING SHALL BE CONSTRUCTED TO EL. 224.0 NOTE) PRIOR TO THE CONSTRUCTION OF THE CONCRETE FOOTING.
- 3) FOR ROCK ELEVATIONS BELOW ELEVATION 216, THE CONTRACTOR SHALL EXCAVATE TO EL. 216 AND INSTALL MICROPILES AS DETAILED IN THE SUBSTRUCTURE PLANS. IT IS ANTICIPATED THAT MICROPILES WILL BE REQUIRED, AT THE NORTHWEST CORNER OF ABUTMENT 1 AND WINGWALL 2A.
- 4) ROCK OUTCROP ENCOUNTERED BETWEEN THE ABUTMENT FACE AND THE STREAM SHALL BE EXCAVATED TO EL. 224. (SEE DIAGRAM BELOW)
- 5) CONCRETE SUBFOOTINGS WILL BE CONSTRUCTED AFTER THE ROCK IS EXCAVATED, EXPOSED, OR MICROPILES ARE INSTALLED. TOP OF SUBFOOTING SHALL BE INSTALLED TO ELEVATION 224.0. ALL AREAS OF THE CONCRETE FOOTINGS WILL BE SUPPORTED ON AT LEAST 1' OF SUBFOOTING CONCRETE MATERIAL.
- 6) THE VOLUME OF ROCK EXCAVATION IS UNKNOWN AS PRECISE ROCK PROFILE IS UNKNOWN, IT IS ESTIMATED TO BE 95 CY.
- 7) ROCK EXCAVATION SHALL BE PAID UNDER THE ITEM STRUCTURE EXCAVATION - ROCK (EXCLUDING COFFERDAM AND DEWATERING). EARTH EXCAVATION SHALL BE PAID UNDER THE ITEM STRUCTURE EXCAVATION - EARTH (EXCLUDING COFFERDAM AND DEWATERING).

LEGEND

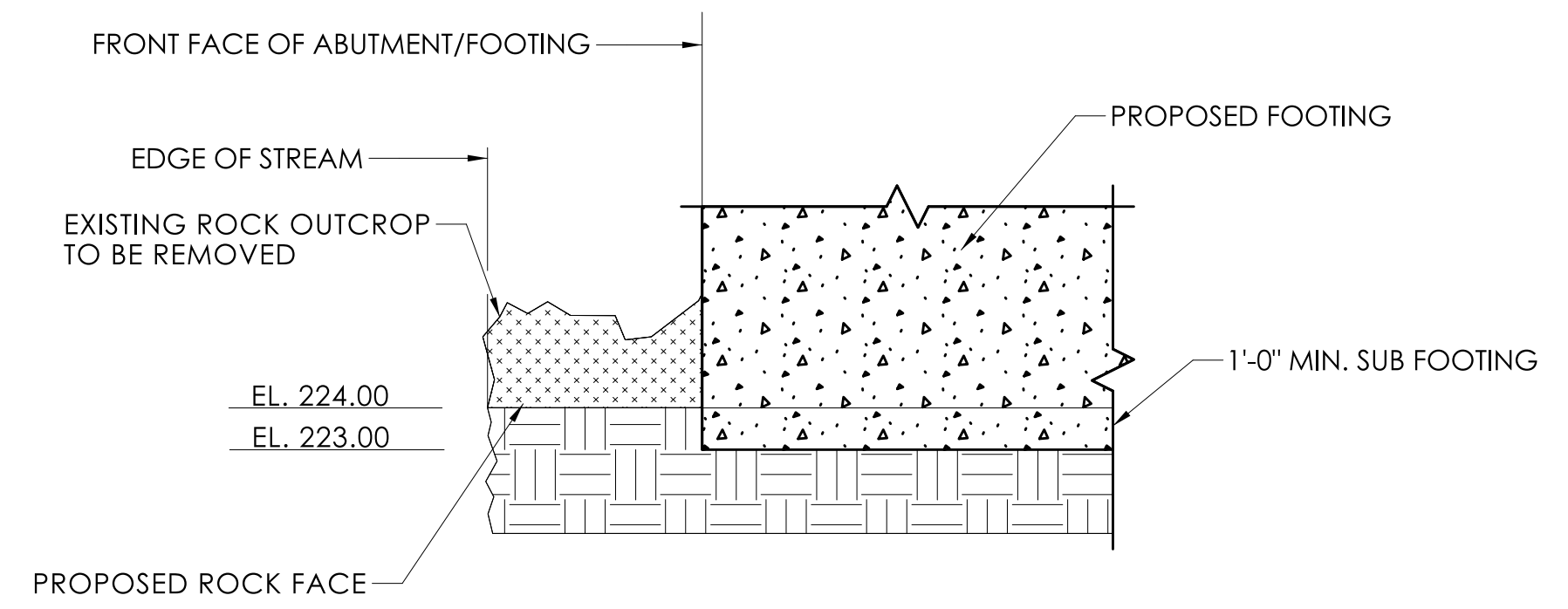
| | |
|--|--|
| | SUGGESTED MICROPILE VERIFICATION TEST LOCATION |
| | BORING LOCATION |

NOTE:
ONLY ONE VERIFICATION TEST NEEDS TO BE PERFORMED

EXISTING BEDROCK PROFILE

SCALE: 1" = 10'

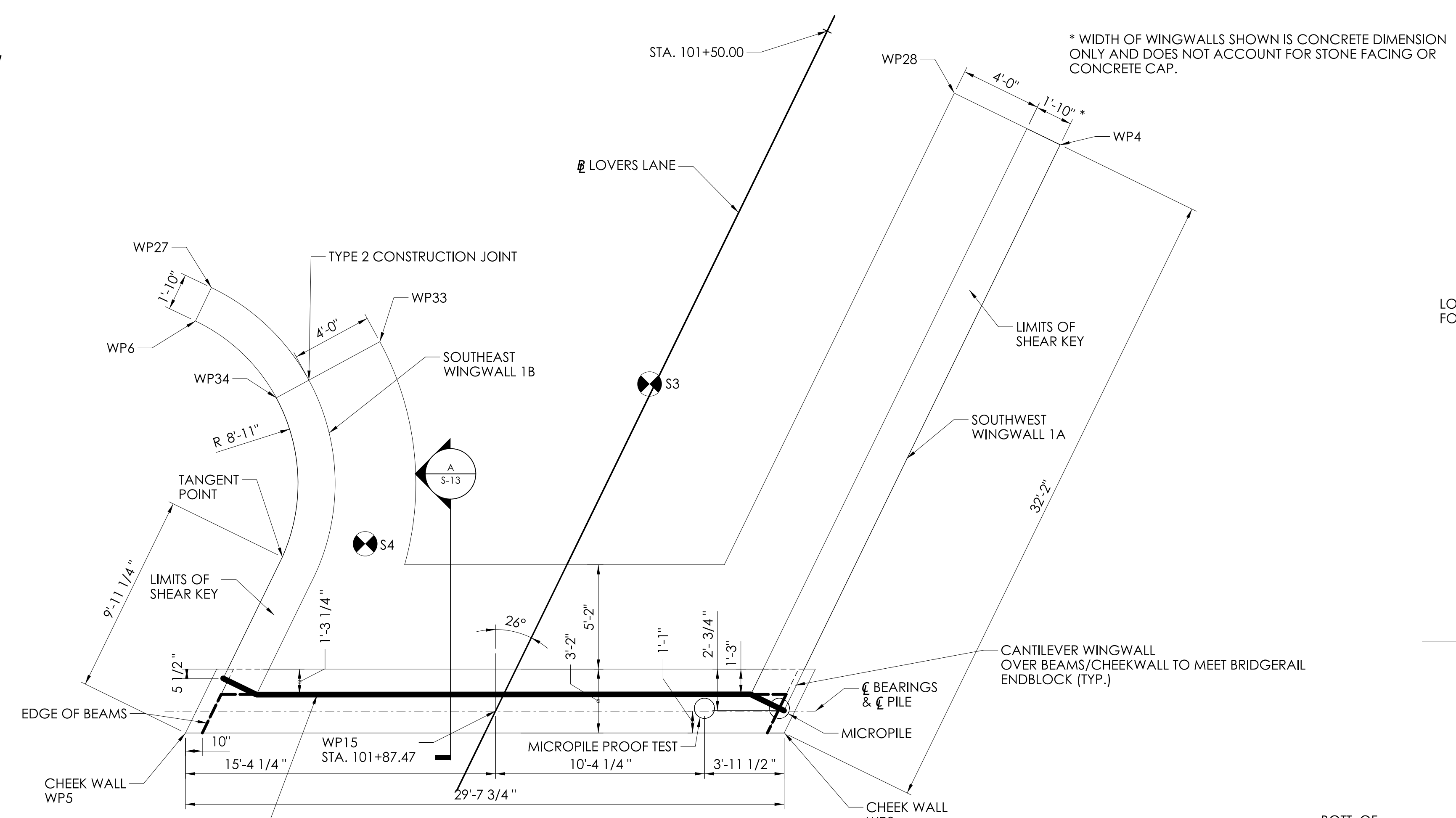
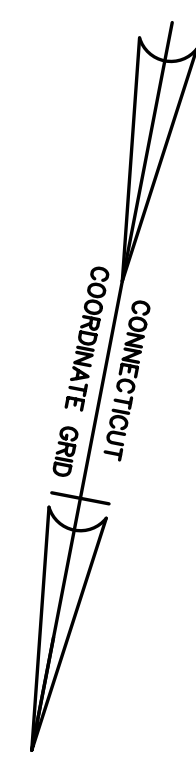
NOTE:
EXCAVATION, ROCK REMOVAL AND FOOTING PLACEMENT SHALL BE COMPLETED IN STAGE 2 (SEE S-09). STAGE 2 TRAFFIC CONTROL/BYPASS BRIDGE NOT SHOWN FOR CLARITY.



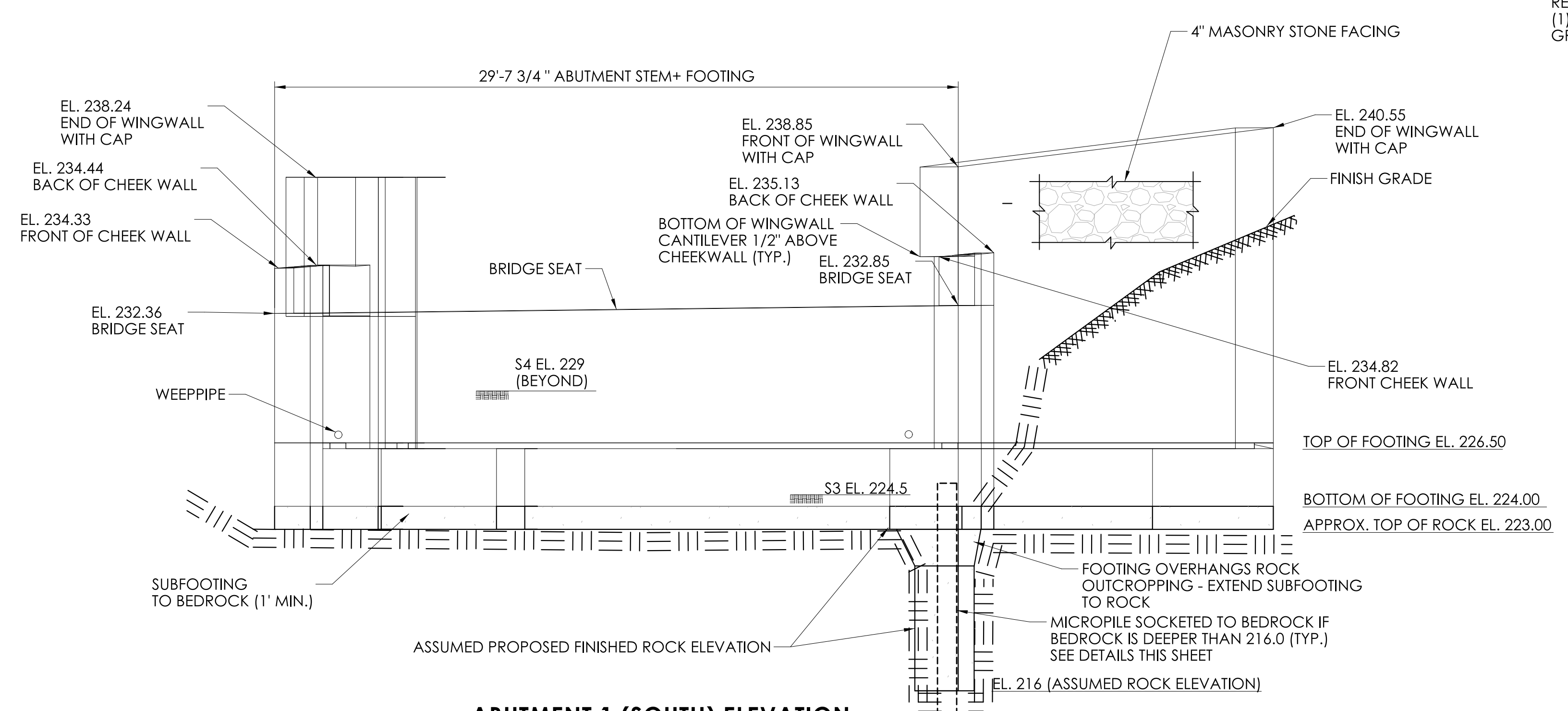
ROCK OUTCROP REMOVAL DETAIL

N.T.S.

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |



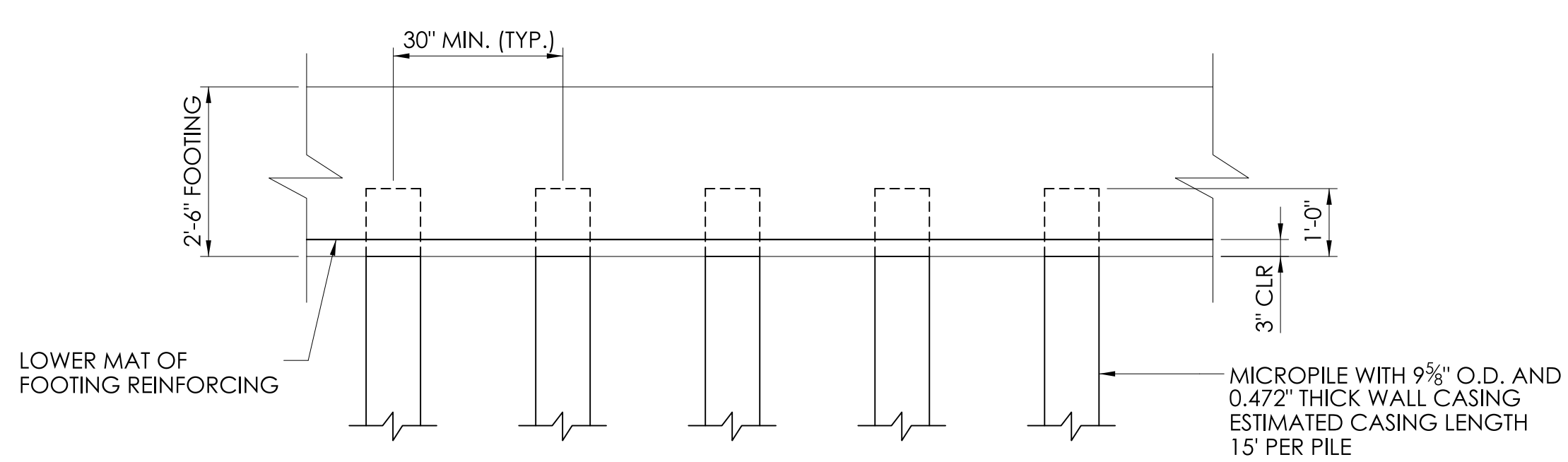
ABUTMENT 1 (SOUTH) PLAN
SCALE: 1/4" = 1'-0"



ABUTMENT 1 (SOUTH) ELEVATION
SCALE: 1/4" = 1'-0"

SUBFOOTING NOTES:

SUBFOOTING SHALL BE PAID UNDER THE ITEM "FLOWABLE FILL" - SEE SPECIAL PROVISION
SUBFOOTING SHALL BE PCC 03340 MIN. 28-DAY STRENGTH OF 3,300 PSI



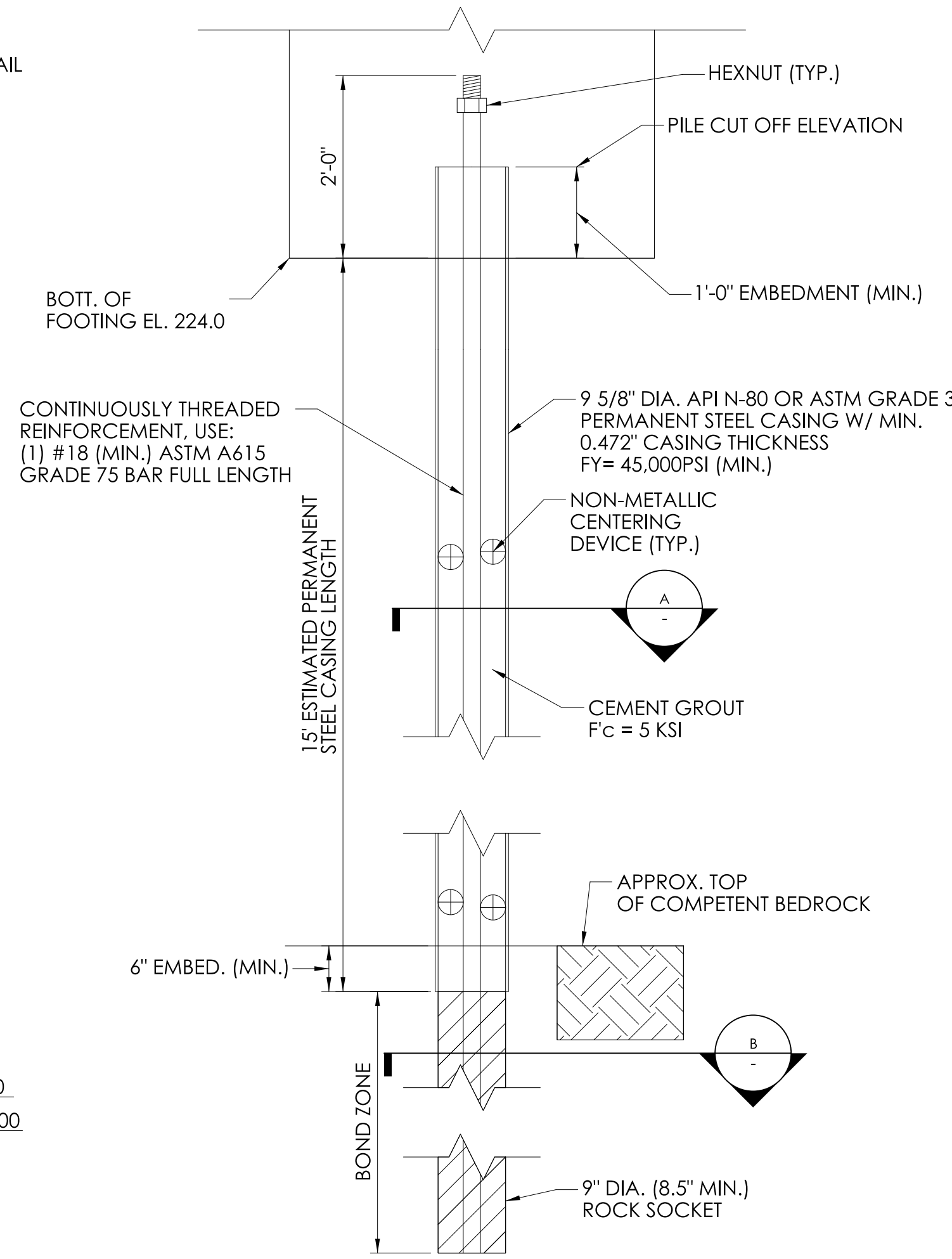
TYPICAL MICROPILE SPACING DETAIL
SCALE: 1/2" = 1'-0"

MICROPILE DESIGN TABLE

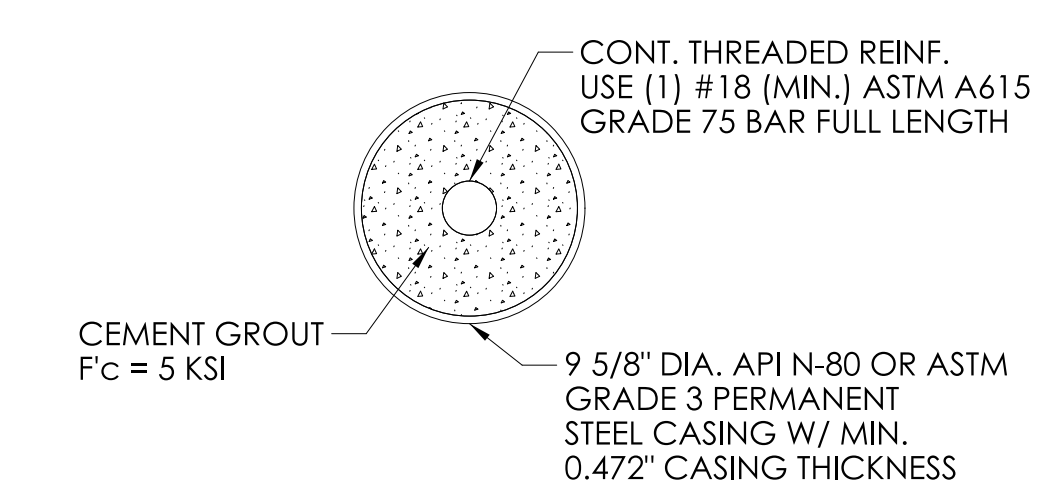
| | |
|------------------------------|-----------------------|
| SERVICE LIMIT STATE - SRV-I | 100 KIPS |
| STRENGTH LIMIT STATE - STR-I | 140 KIPS (0.7 FACTOR) |
| ULTIMATE PILE CAPACITY | 200 KIPS |

MICROPILE NOTES:

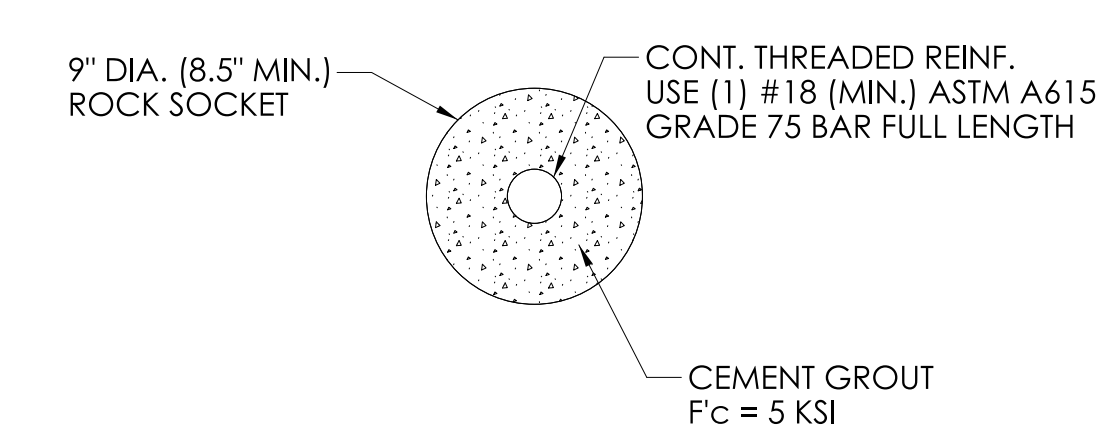
1. A MICROPILE SHALL BE INSTALLED AT ALL AREAS OF SUBSTRUCTURE WHERE BEDROCK IS BELOW ELEVATION 216.0.
2. THE MICROPILE DESIGN PROVIDED ON THESE DRAWINGS IS BASED ON THE LOADS AND RESISTANCES SHOWN IN THE TABLE ABOVE.
3. THE MICROPILES SHALL BE DESIGNED USING PERMANENT STEEL CASINGS. THE CASINGS SHALL EXTEND INTO BEDROCK.
4. THE 9 5/8" CASING SHALL BE INSTALLED WITH NO CASING JOINTS WITHIN 10 FEET BELOW THE BOTTOM OF THE SUBFOOTING.
5. MICROPILES WILL BE DRILLED THRU ALL MATERIALS ENCOUNTERED.
6. THE CONTRACTOR IS ADVISED TO THE POSSIBLE PRESENCE OF BOULDERS, RUBBLE AND OTHER MISCELLANEOUS MATERIALS. REFER TO SOIL BORING LOGS SHOWN ON DWG. NO. STR-3 THROUGH DWG. NO. STR-7.
7. THE CONTRACTOR SHALL PROPERLY DISPOSE OF ALL MATERIALS (CUTTING, ETC.) RECOVERED FROM THE DRILLING OPERATIONS.
8. MICROPILE DETAILS SHOWN ARE BASED ON ASSUMPTIONS THAT WERE USED DURING DESIGN. THE CONTRACTOR SHALL DESIGN THE MICROPILES IN ACCORDANCE WITH THE SPECIFICATIONS FOR ITEM "MICROPILES."



MICO-PILE ELEVATION
SCALE: 3/4" = 1'-0"

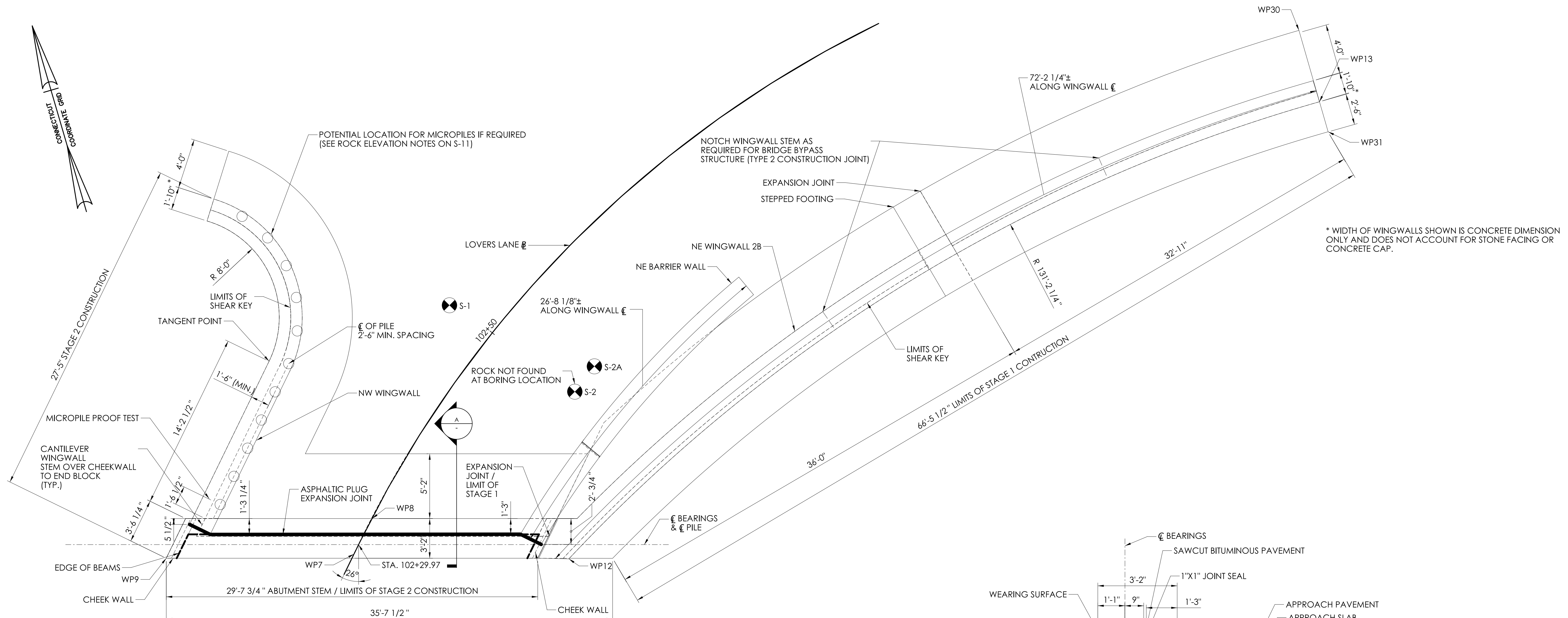


MICO-PILE SECTION A
SCALE: 1 1/2" = 1'-0"



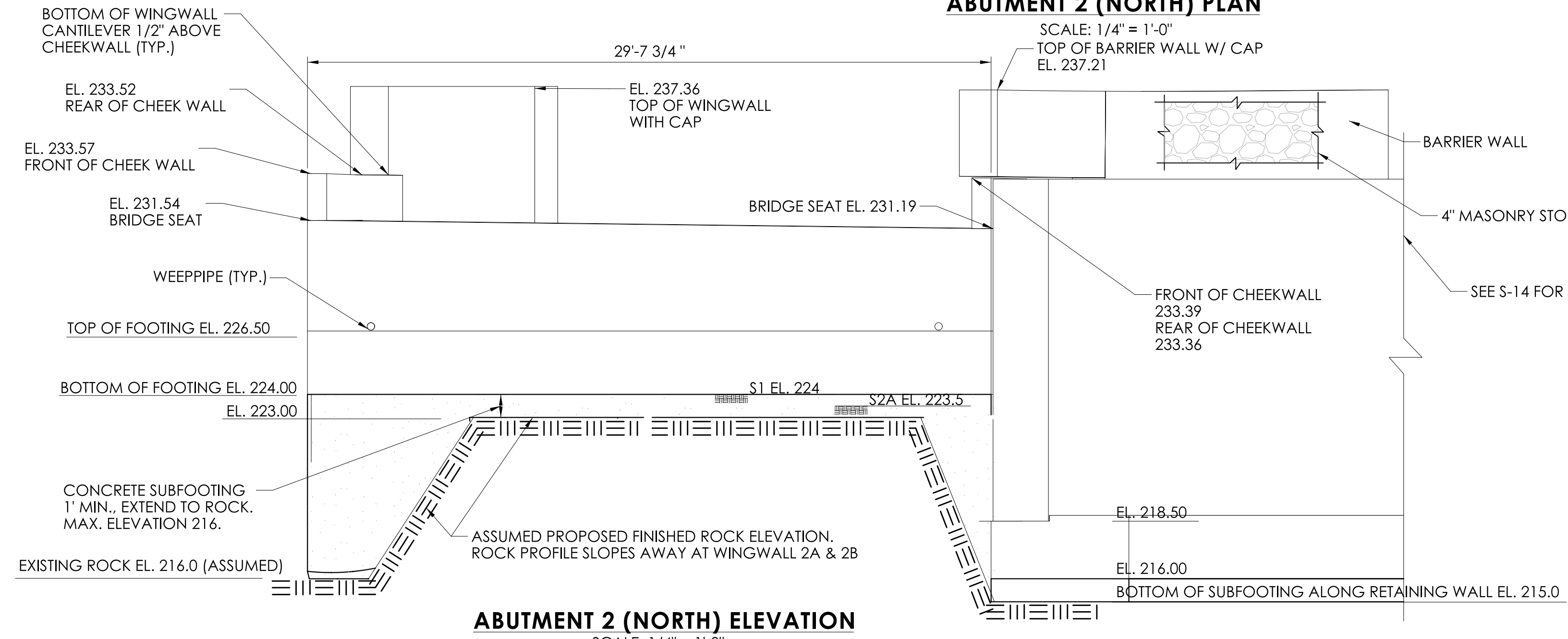
MICO-PILE SECTION B
SCALE: 1 1/2" = 1'-0"

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |

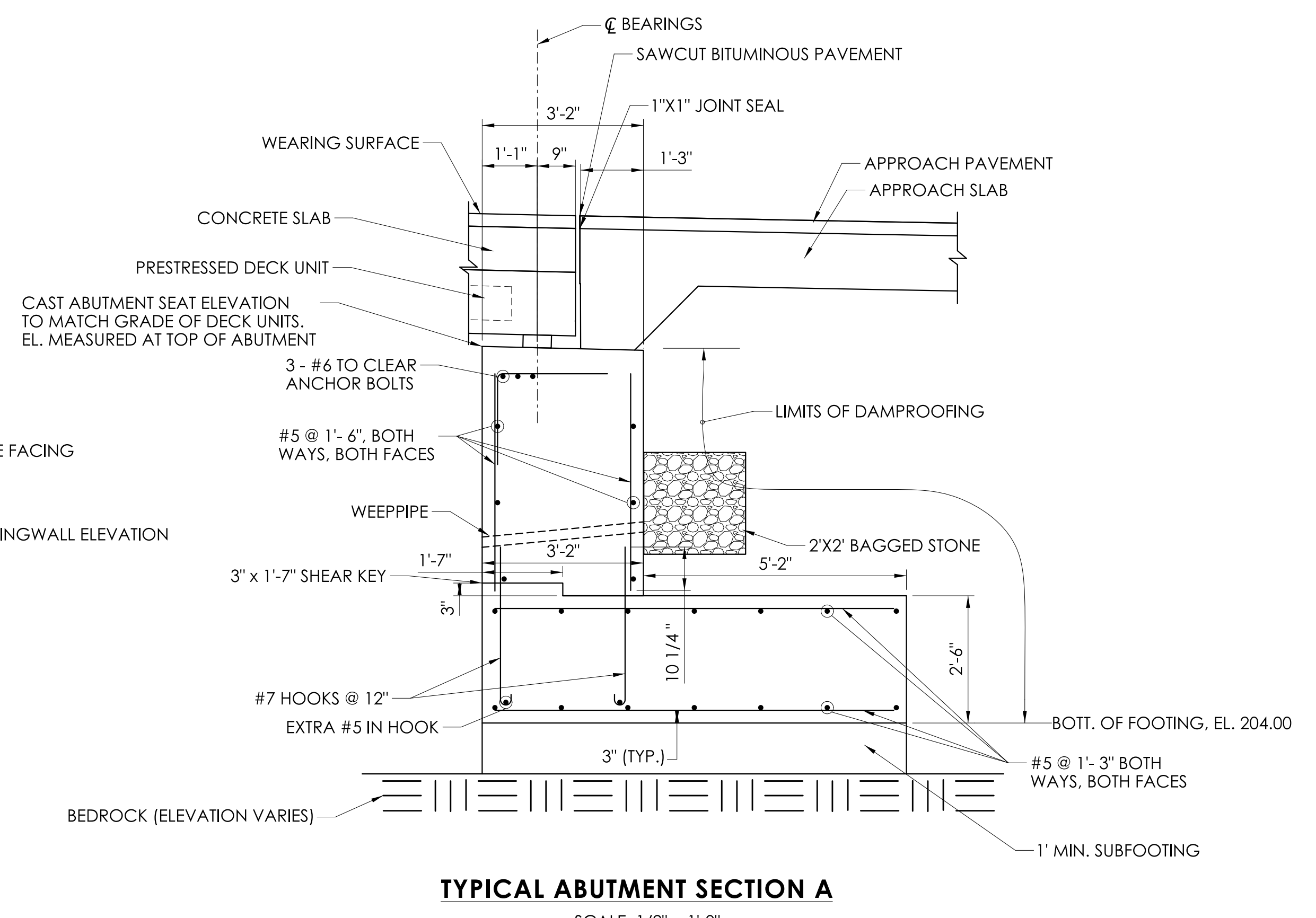


* WIDTH OF WINGWALLS SHOWN IS CONCRETE DIMENSION ONLY AND DOES NOT ACCOUNT FOR STONE FACING OR CONCRETE CAP.

ABUTMENT 2 (NORTH) PLAN
SCALE: 1/4" = 1'-0"

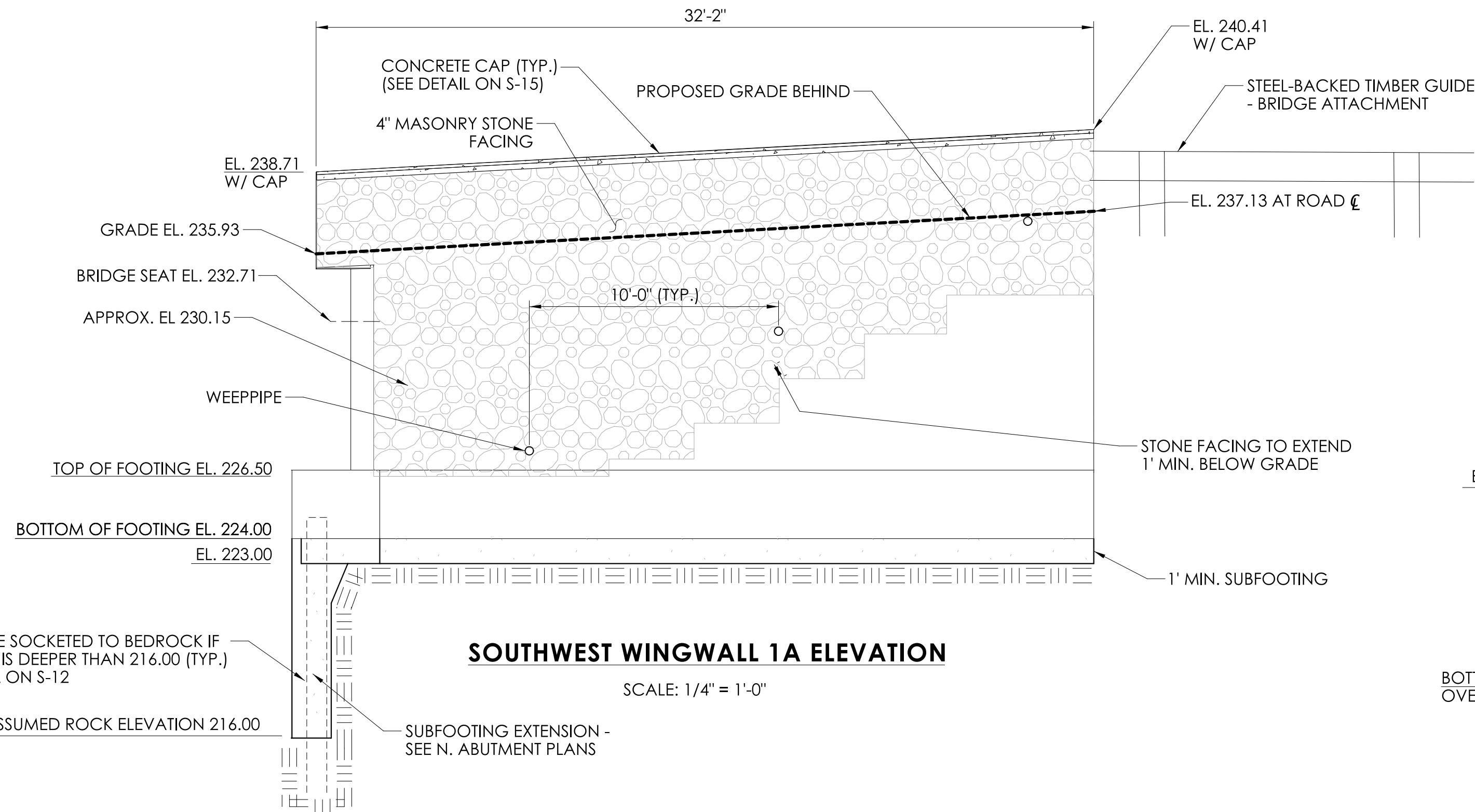


ABUTMENT 2 (NORTH) ELEVATION
SCALE: 1/4" = 1'-0"



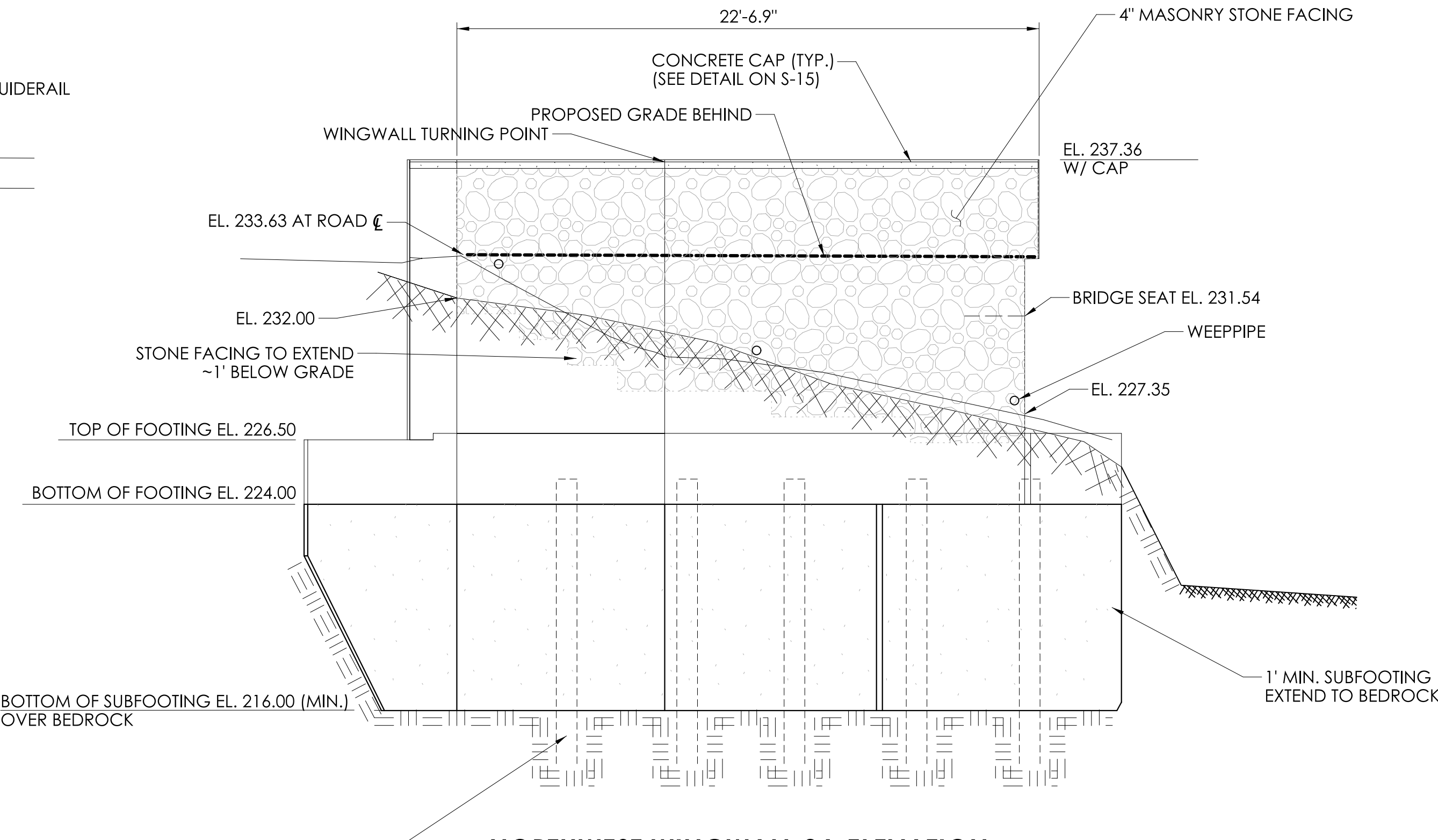
TYPICAL ABUTMENT SECTION A
SCALE: 1/2" = 1'-0"

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |



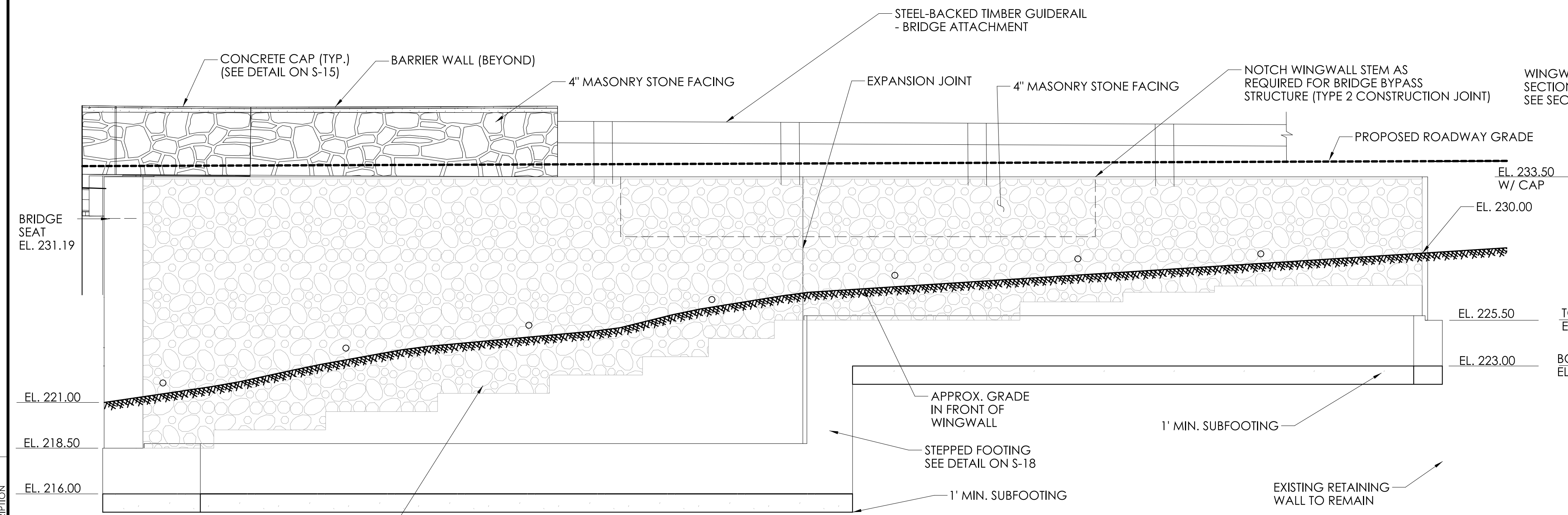
SOUTHWEST WINGWALL 1A ELEVATION

SCALE: 1/4" = 1'-0"



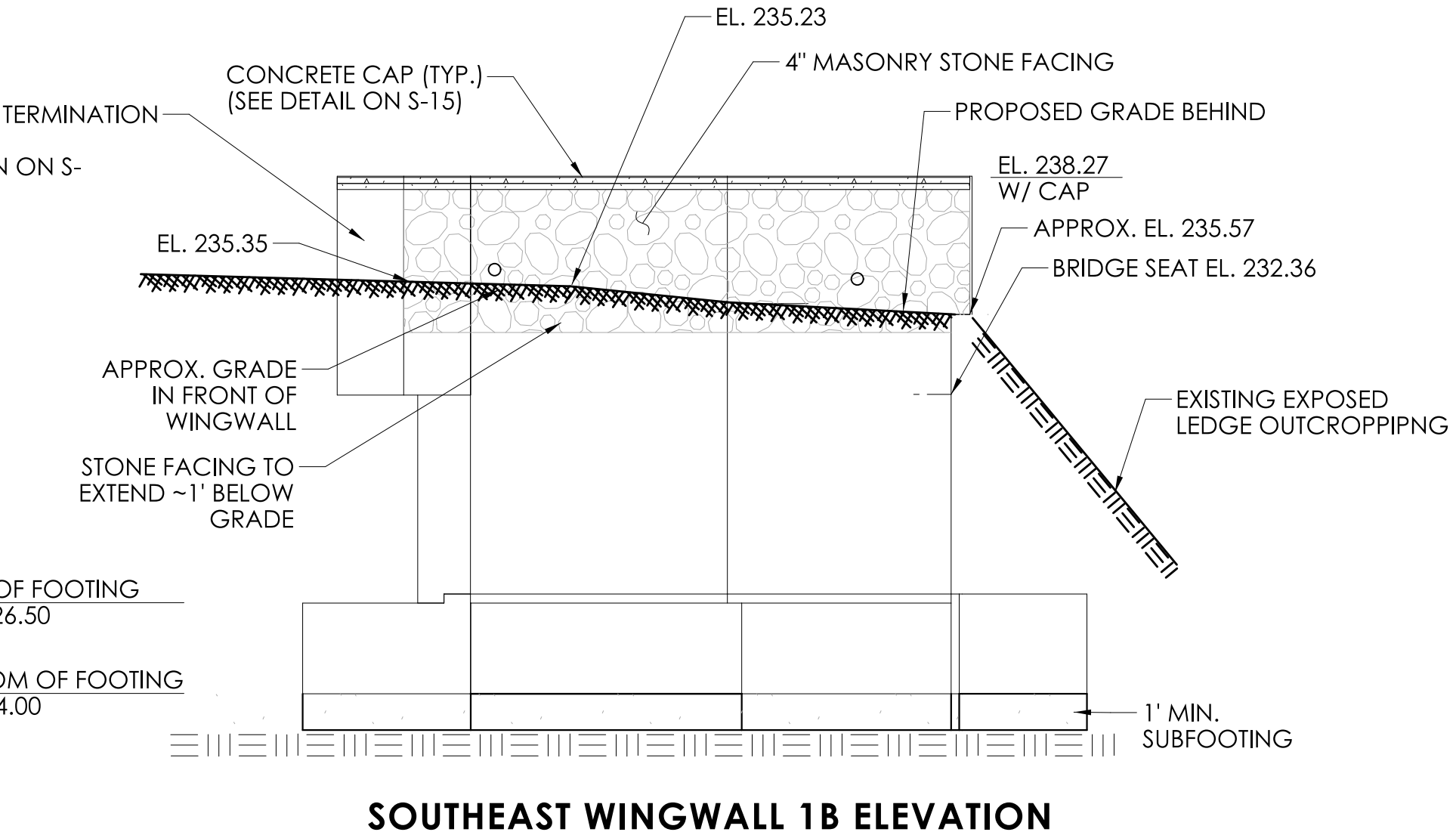
NORTHWEST WINGWALL 2A ELEVATION

SCALE: 1/4" = 1'-0"



NORTHEAST RETAINING WALL 2B ELEVATION

SCALE: 1/4" = 1'-0"

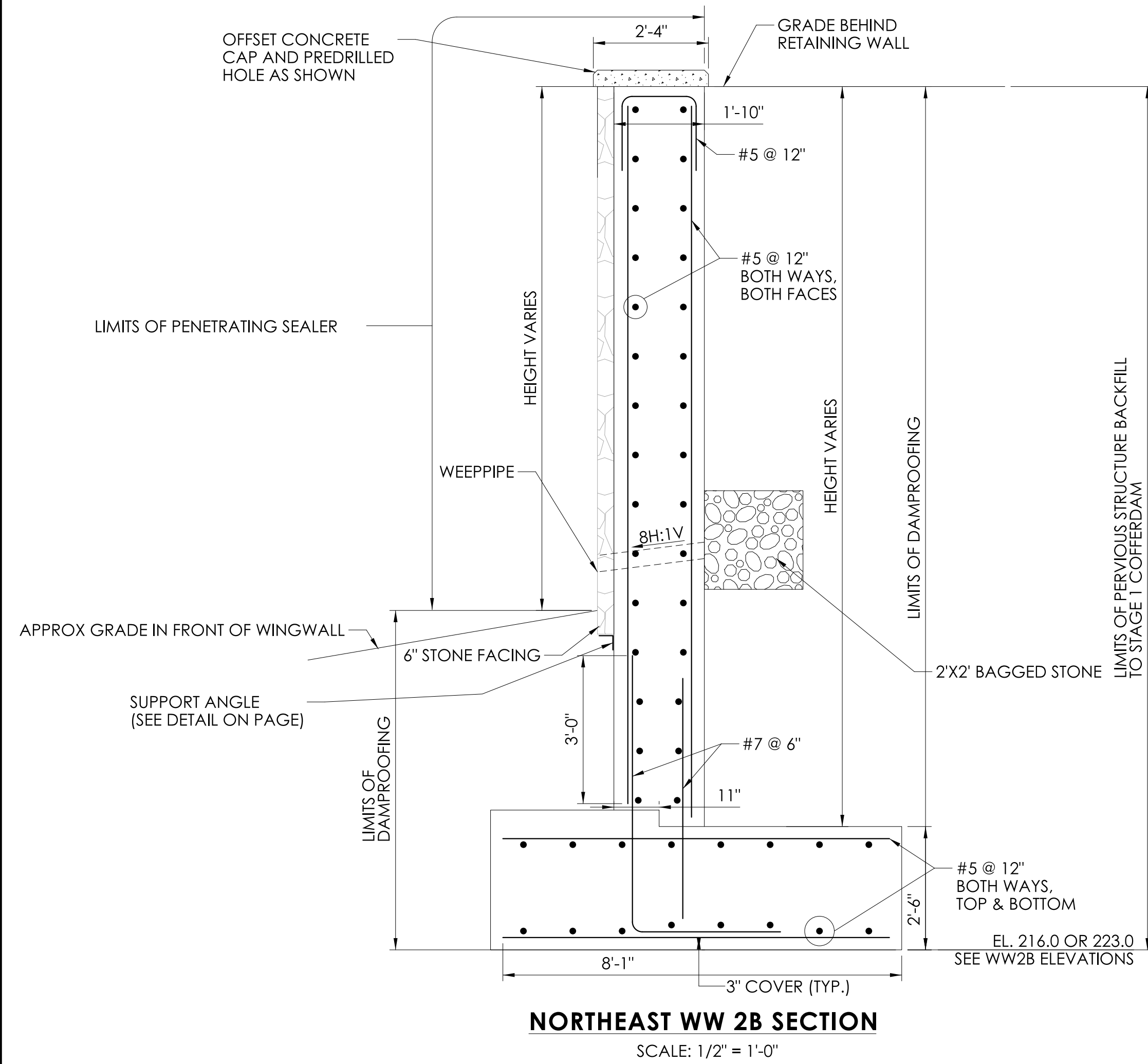


SOUTHEAST WINGWALL 1B ELEVATION

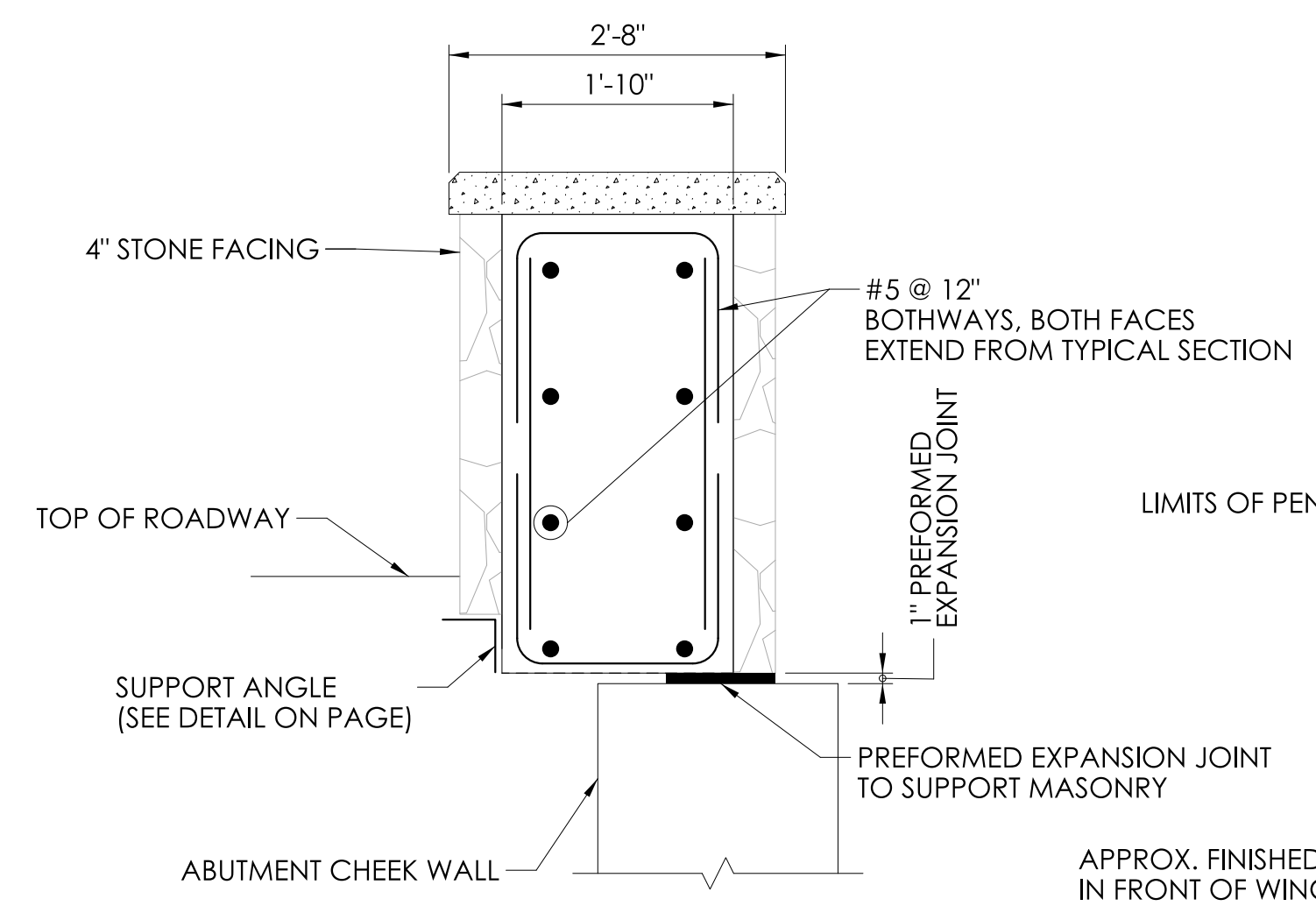
SCALE: 1/4" = 1'-0"

| LIMIT STATE | MAX. DESIGN FOUNDATION PRESSURES | | |
|-------------|----------------------------------|-----------------------------|----------------------|
| | NOMINAL BEARING RESISTANCE | FACTORED BEARING RESISTANCE | MAX BEARING PRESSURE |
| SERVICE | 5 TSF | 5 TSF | 3.3 TSF STRENGTH I |
| STRENGTH | 13.5 TSF | 7.425 TSF | 4.35 TSF SERVICE I |

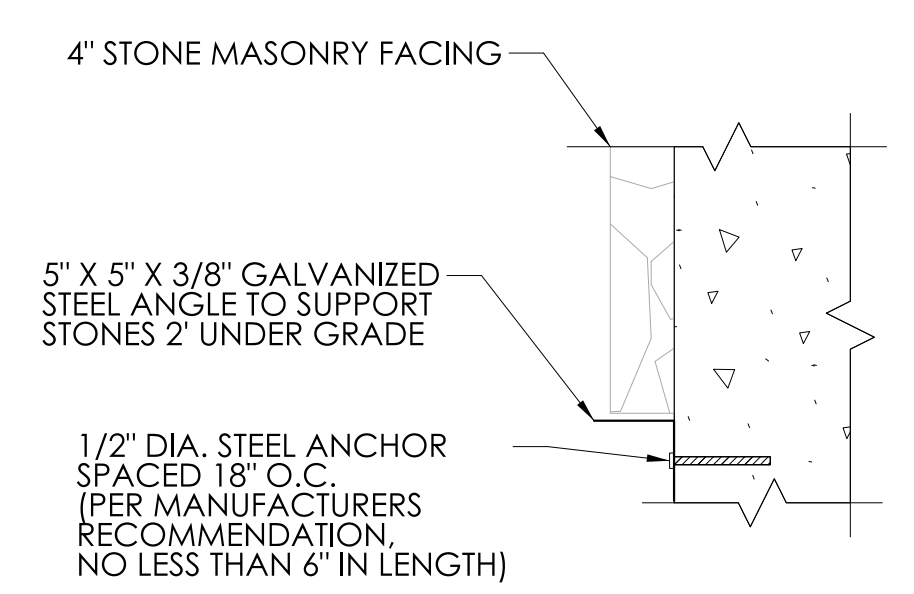
| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |



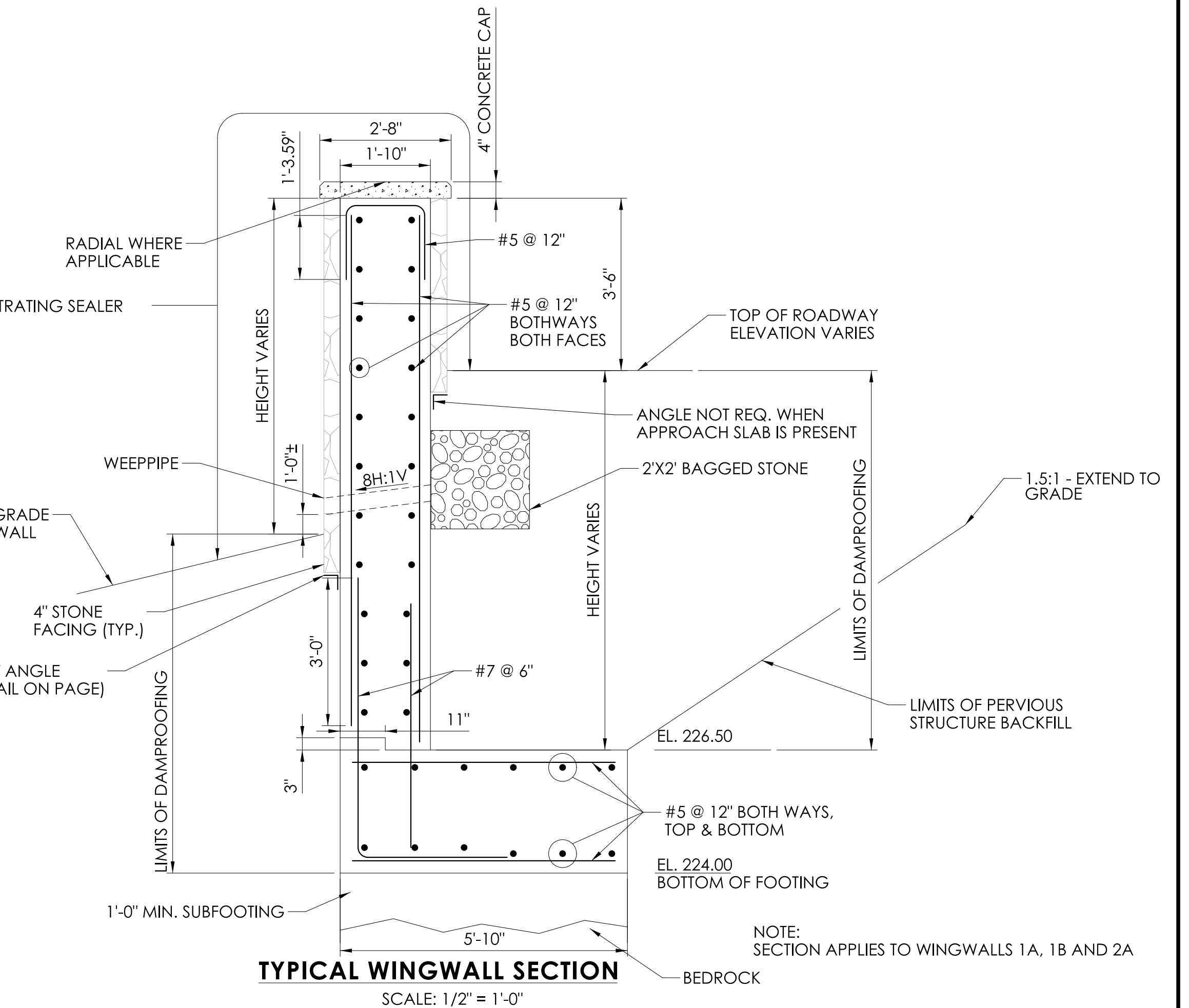
NORTHEAST WW 2B SECTION
SCALE: 1/2" = 1'-0"



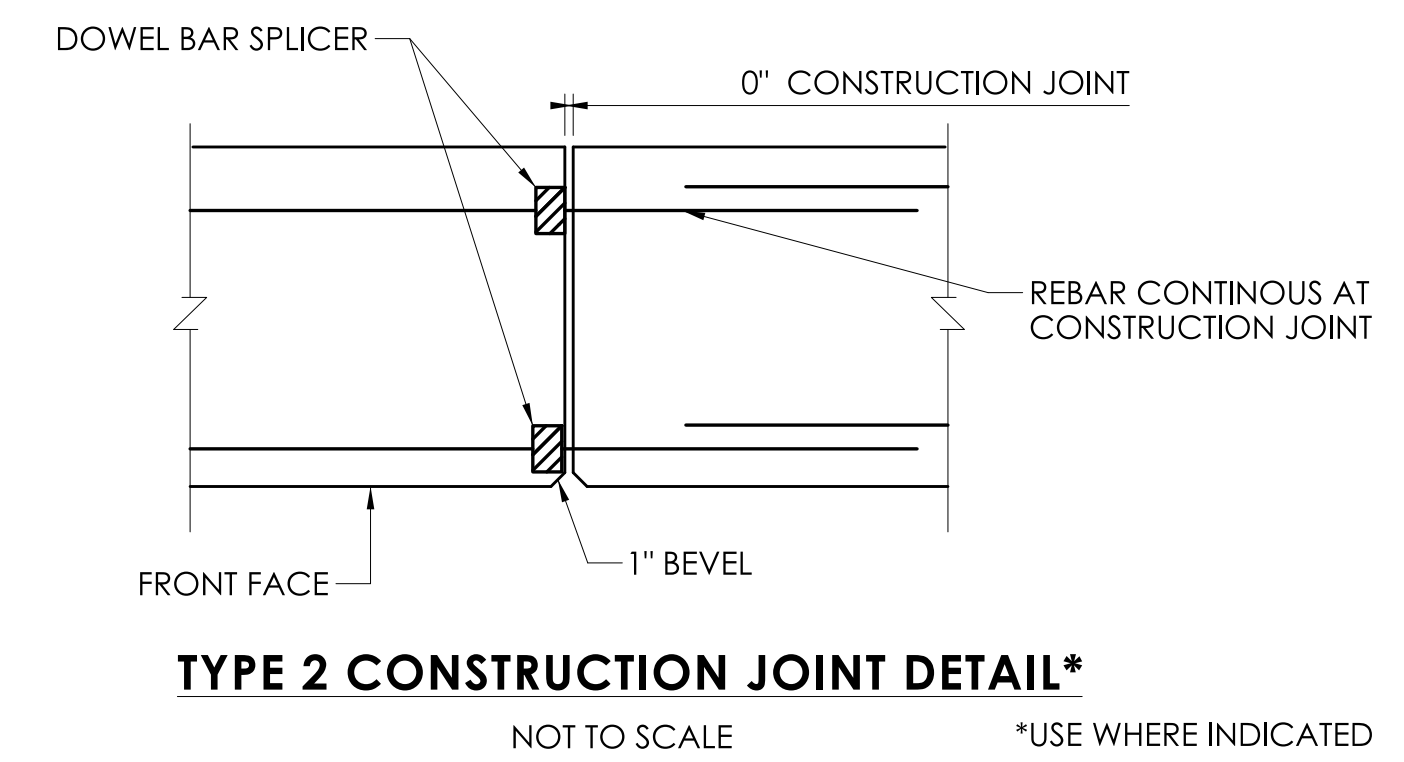
TYPICAL WINGWALL CANTILEVERED END
SCALE: 3/4" = 1'-0"



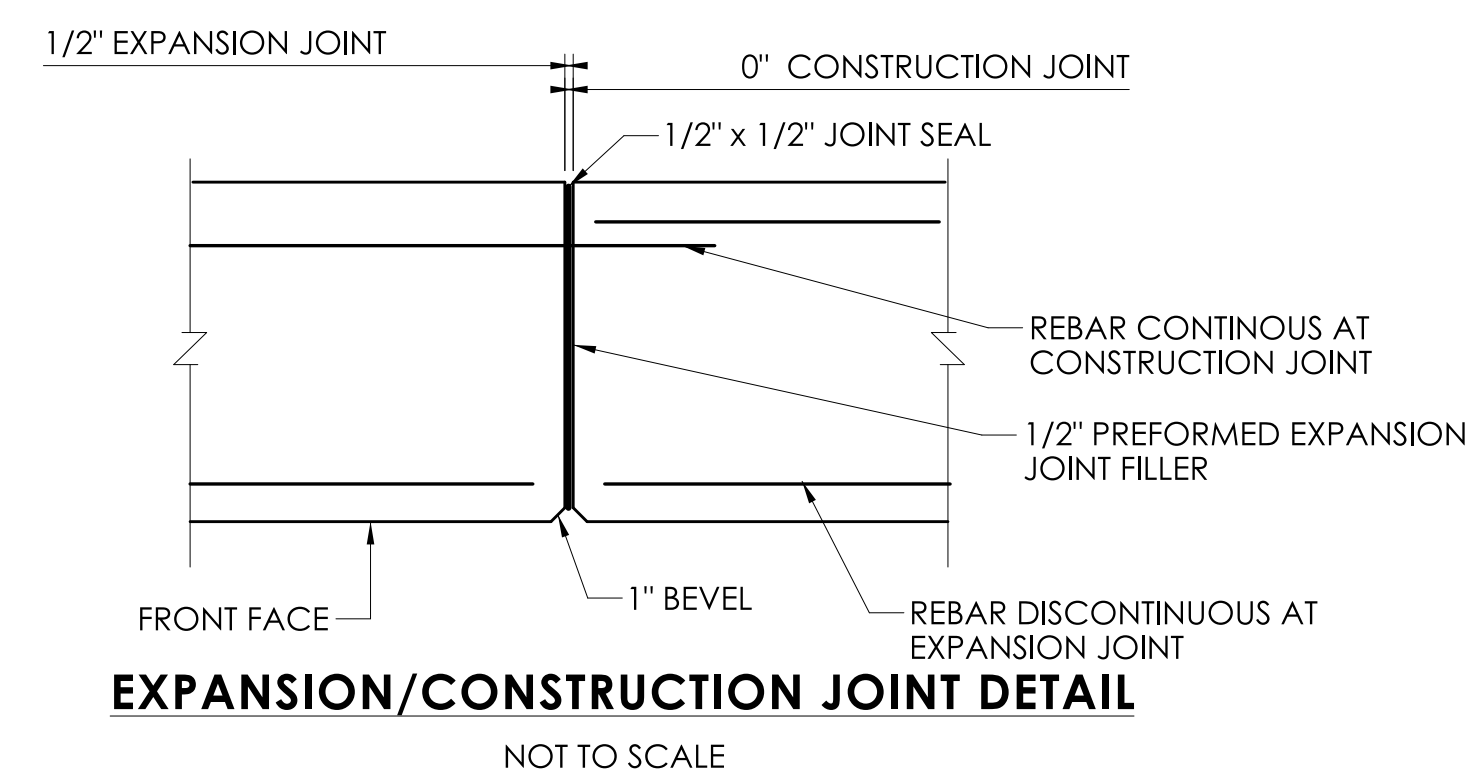
MASONRY FACING SUPPORT ANGLE DETAIL
SCALE: 1" = 1'-0"



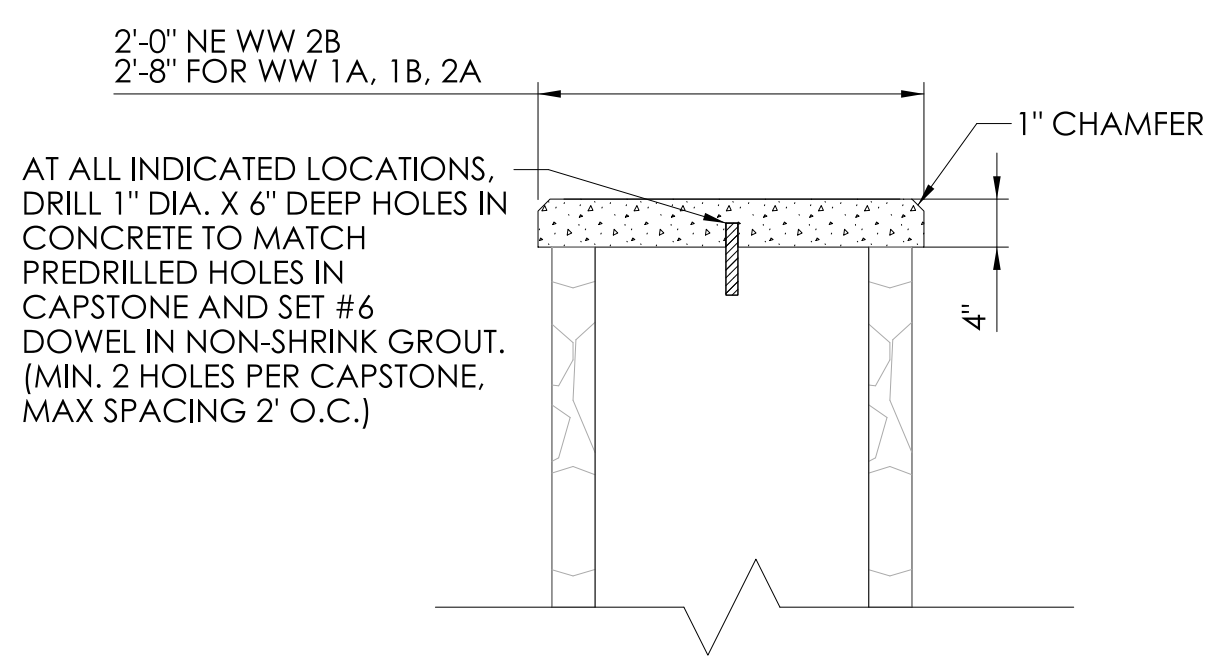
TYPICAL WINGWALL SECTION
SCALE: 1/2" = 1'-0"



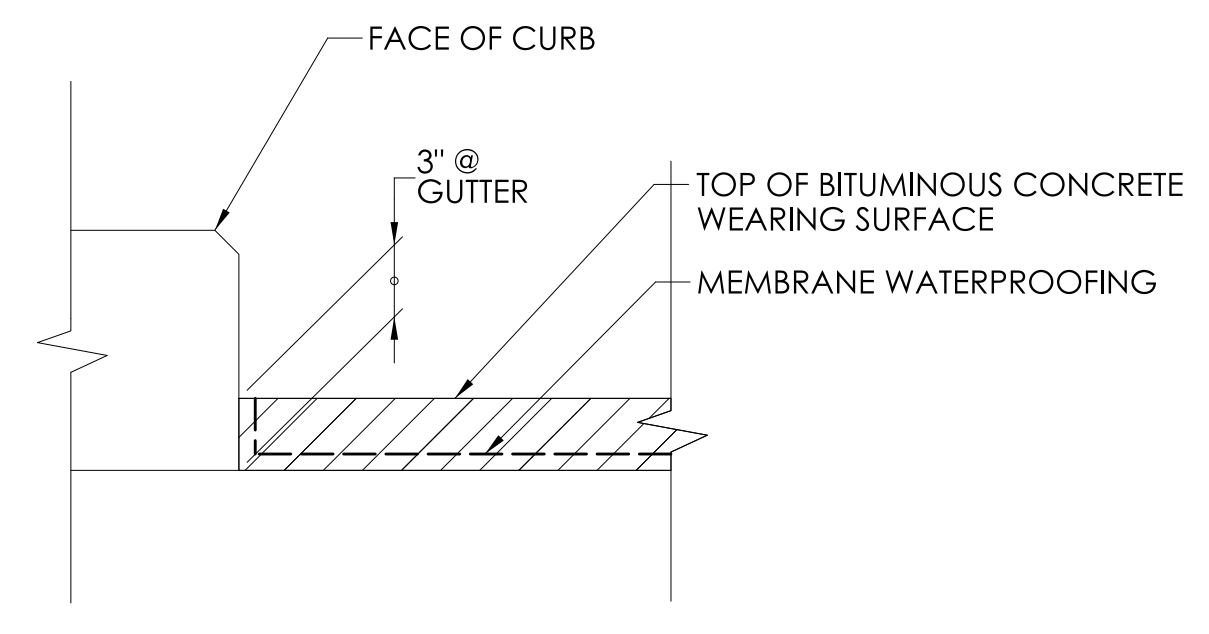
TYPE 2 CONSTRUCTION JOINT DETAIL*
NOT TO SCALE *USE WHERE INDICATED



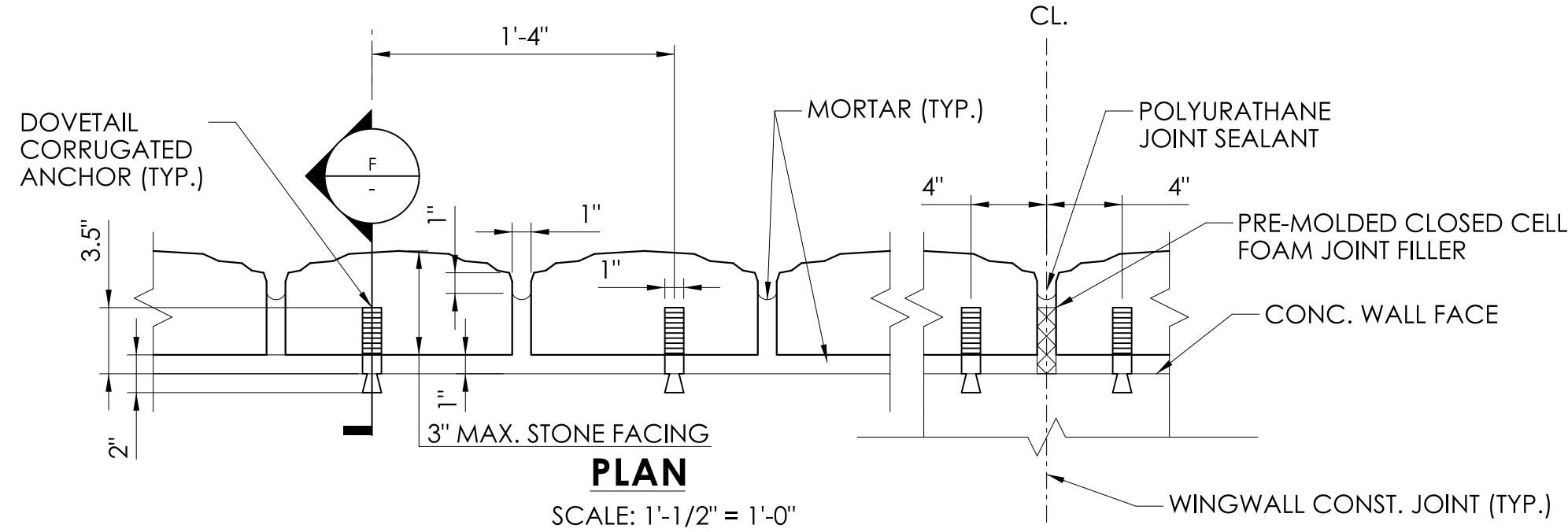
EXPANSION/CONSTRUCTION JOINT DETAIL
NOT TO SCALE



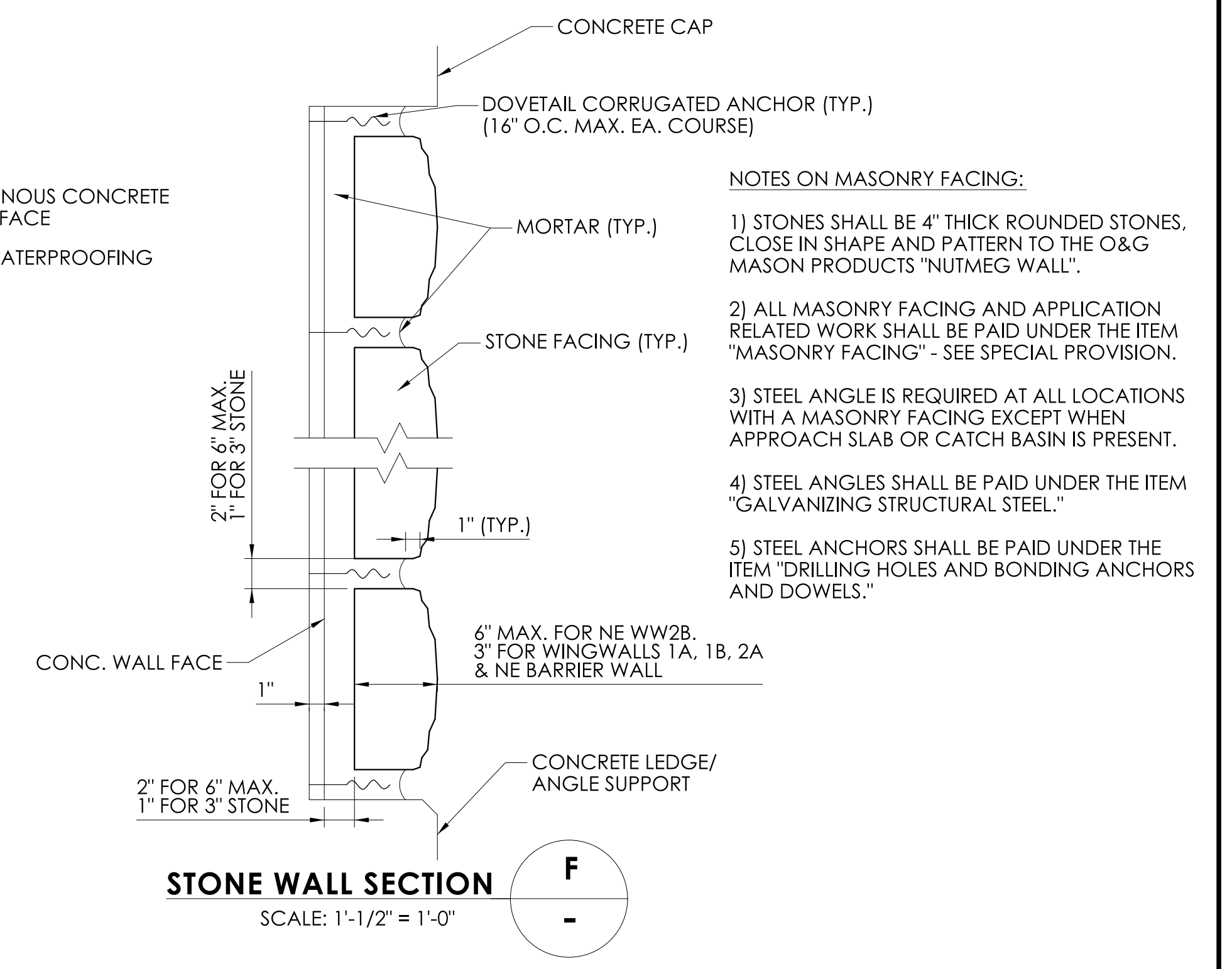
TYPICAL CONCRETE CAP DETAIL
SCALE: 3/4" = 1'-0"



TREATMENT OF MEMBRANE WATERPROOFING AT GUTTER
SCALE: 1 1/2" = 1'-0"



PLAN
SCALE: 1'-1/2" = 1'-0"

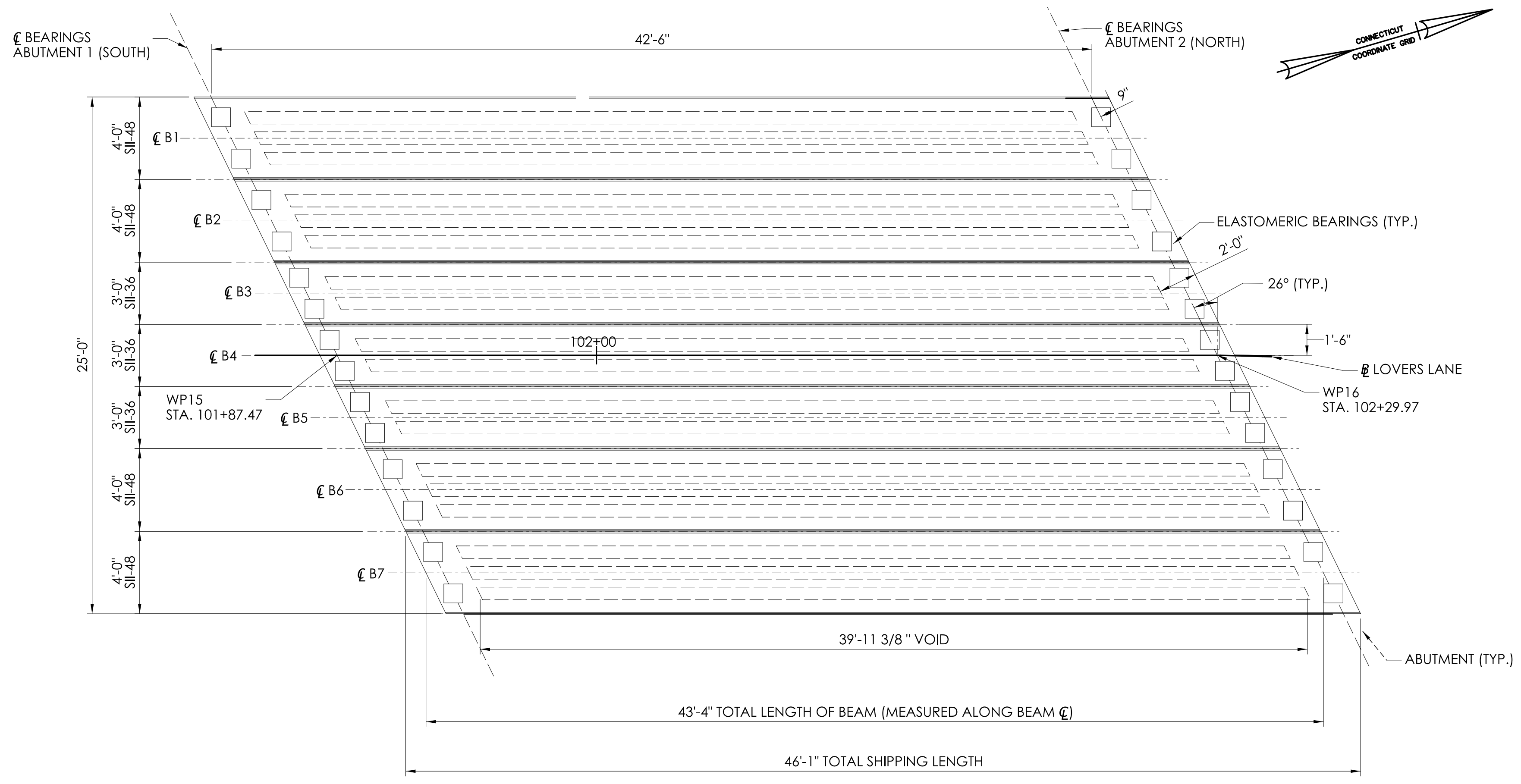


STONE WALL SECTION
SCALE: 1'-1/2" = 1'-0"

- NOTES ON MASONRY FACING:**
- 1) STONES SHALL BE 4" THICK ROUNDED STONES, CLOSE IN SHAPE AND PATTERN TO THE O&G MASON PRODUCTS "NUTMEG WALL".
 - 2) ALL MASONRY FACING AND APPLICATION RELATED WORK SHALL BE PAID UNDER THE ITEM "MASONRY FACING" - SEE SPECIAL PROVISION.
 - 3) STEEL ANGLE IS REQUIRED AT ALL LOCATIONS WITH A MASONRY FACING EXCEPT WHEN APPROACH SLAB OR CATCH BASIN IS PRESENT.
 - 4) STEEL ANGLES SHALL BE PAID UNDER THE ITEM "GALVANIZING STRUCTURAL STEEL."
 - 5) STEEL ANCHORS SHALL BE PAID UNDER THE ITEM "DRILLING HOLES AND BONDING ANCHORS AND DOWELS."

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |
| | | |

DESIGNER/DRAFTER: JT CHECKED BY: DW
 SIGNATURE/BLOCK: _____
 STATE OF CONNECTICUT TOWN OF WILTON
 PROJECT NUMBER: 0161-0142
 PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVERS LANE OVER COMSTOCK BROOK
 TOWN(S): WILTON
 DRAWING TITLE: SUBSTRUCTURE DETAILS



FRAMING PLAN
SCALE: 1/4" = 1'-0"

PRESTRESSED DECK UNIT NOTES:

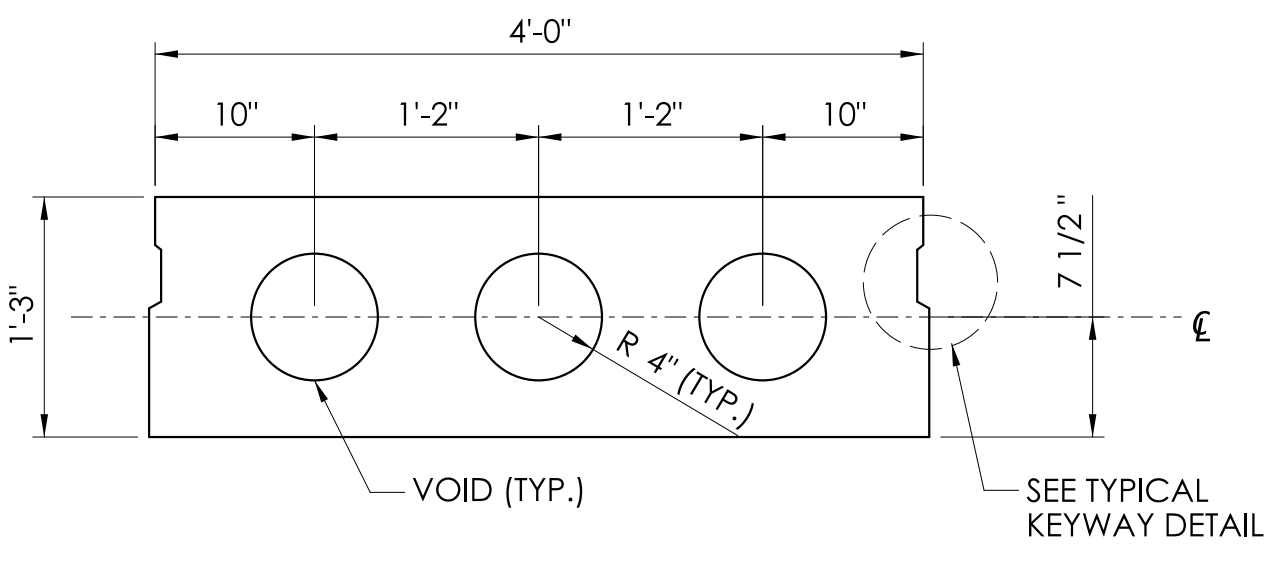
- PRESTRESSED DECK UNITS SHALL CONFORM TO THE FOLLOWING REQUIREMENTS:
f_{ci} = 6 KSI
f_c = 8 KSI
- PRESTRESSED STRANDS SHALL CONFORM TO THE FOLLOWING REQUIREMENTS. 0.6" DIAMETER, UNLOCATED, 7 WIRE, LOW RELAXATION STRANDS CONFORMING TO THE REQUIREMENTS OF AASHTO M203, GRADE 270:
ULTIMATE STRENGTH (f_s) = 270,000 PSI
JACKING TENSION (f_j) = 43,940 LBS. PER STRAND
- PRESTRESSED STRANDS SHALL BE PLACED 2" MINIMUM ON CENTER AND SHALL HAVE A MINIMUM COVER OF 2".
- ENDS OF THE DECK UNITS SHALL BE VERTICAL AFTER APPLICATION OF FULL DEAD LOAD.
- THE DRILLING OF HOLES IN PRESTRESSED DECK UNITS, OR THE USE OF POWDER ACTUATED TOOLS ON PRESTRESSED DECK UNITS WILL NOT BE PERMITTED.
- NO ADDITIONAL DEAD LOADS OR LIVE LOADS SHALL BE APPLIED TO THE PRESTRESSED DECK UNITS UNTIL GROUT KEYWAYS ARE FULLY FILLED AND GROUT IN THE LONGITUDINAL SHEAR KEYS HAVE REACHED A SEVEN-DAY COMPRESSIVE STRENGTH OF 14KSI.
- THE DECK UNITS SHALL BE PLACED AT THE NOMINAL SPACING SHOWN ON THE PLANS WITH A 1/2" WIDE GAP BETWEEN THE BOTTOM OF THE UNITS. THE WIDTH OF THIS GAP CAN VARY DUE TO SWEEP OF THE BEAMS.
- SHEAR KEY SHALL BE OMITTED ON OUTSIDE FACE OF FASCIA BEAMS.
- MILD REINFORCING STEEL SHALL BE GALVANIZED ASTM A615 GRADE 60.
- THE CONTRACTOR SHALL MANUFACTURE AND CONSTRUCT DECK UNITS IN ACCORDANCE WITH SPECIFICATIONS FOR "PRESTRESSED DECK UNITS".
- ALL INSERTS OF HOLES CAST INTO THE DECK UNITS FOR THE PURPOSE OF HANDLING AND SETTING THE UNITS SHALL BE SEALED WITH GROUT TO A SMOOTH FINISH UPON COMPLETION OF THE WORK.
- THE COST OF FURNISHING INSERTS SHALL BE INCLUDED IN THE ITEMS #0514202, AND #0514222 "PRESTRESSED DECK UNITS".
- LIFTING HOOKS SHALL BE PLACED IN LINE WITH THE CENTER OF THE BEARINGS AND SHALL BE INSTALLED PER THE FABRICATOR'S STANDARD DETAILS. THE FABRICATOR IS FULLY RESPONSIBLE FOR THE DESIGN OF THE LIFTING DEVICES WHICH SHALL BE ADEQUATE FOR THE FACTORY OF SAFETY REQUIRED BY THE ERECTION PROCEDURE.

| STRANDS SUMMARY | | | |
|-------------------------|---------------|---------------------|------------------------|
| DECK UNIT TYPE | STRAND NUMBER | STRAND CG AT ENDS * | STRAND CG AT MIDSPAN * |
| SII-48 (B1, B2, B6, B7) | 17 | 3.41 IN | 2.00 IN |
| SII-36 (B3, B4, B5) | 13 | 3.23 IN | 2.00 IN |

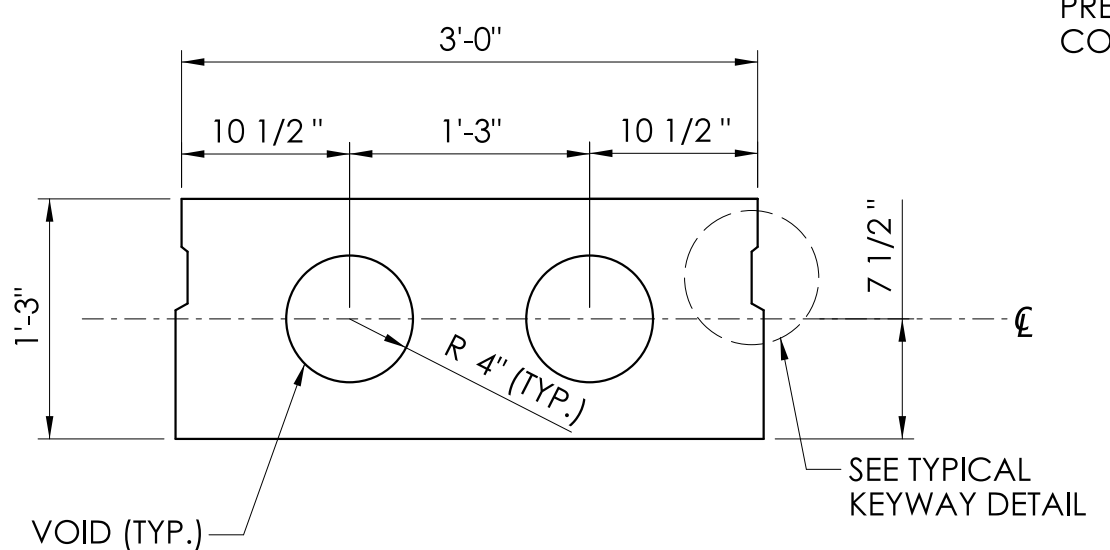
*CGS MEASURED FROM BOTTOM OF DECK UNIT AND BASED ON THE GROSS NON-COMPOSITE SECTION.

| CAMBER TABLE | | | |
|-------------------------|---|---|---|
| MEMBER NUMBER | AT TRANSFER | AT ERECTION | FINAL |
| | CAMBER DUE TO PRETENSIONING FORCE AT TRANSFER MINUS THE DEFLECTION DUE TO THE DEAD LOAD OF THE MEMBER | CAMBER (DUE TO PRETENSIONING FORCE AT TRANSFER MINUS THE DEFLECTION DUE TO THE DEAD LOAD OF THE MEMBER) APPROXIMATELY 30 DAYS AFTER TRANSFER. | CAMBER AFTER ALL DEAD LOADS ARE APPLIED TO THE STRUCTURE. |
| SII-48 (B1, B2, B6, B7) | 1.224 IN | 1.636 IN | 1.192 IN |
| SII-36 (B3, B4, B5) | 1.249 IN | 1.679 IN | 1.236 IN |

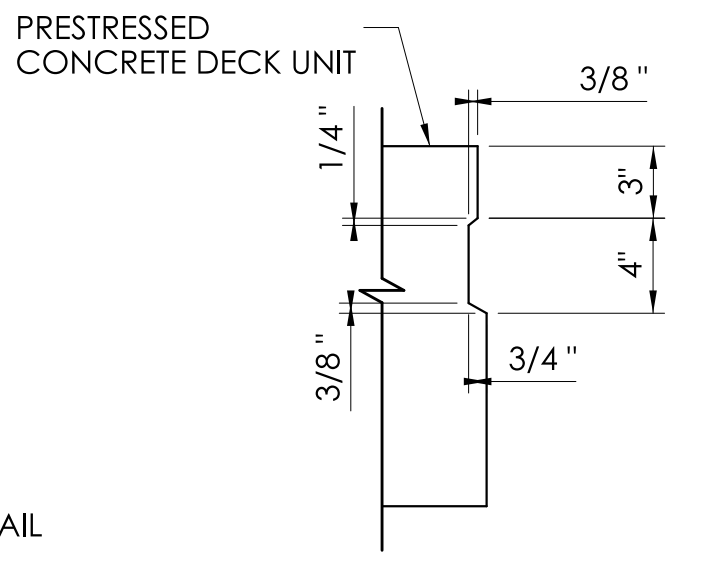
*POSITIVE VALUES IN THE CAMBER TABLE INDICATE UPWARD CAMBER.



SII-48
SCALE: 1" = 1'-0"



SII-36
SCALE: 1" = 1'-0"



TYPICAL KEYWAY DETAIL
SCALE: 1 1/2" = 1'-0"

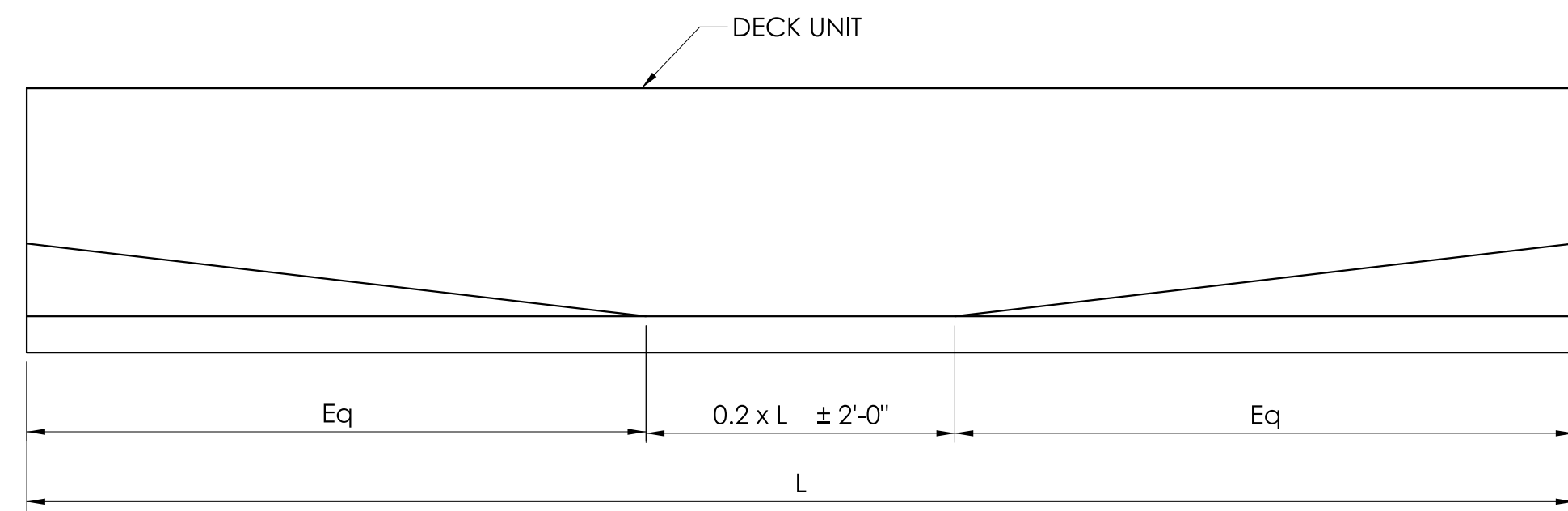
PRESTRESSED CONCRETE DECK UNITS

BEAM DEAD LOAD DEFLECTION TABLE NOTES:

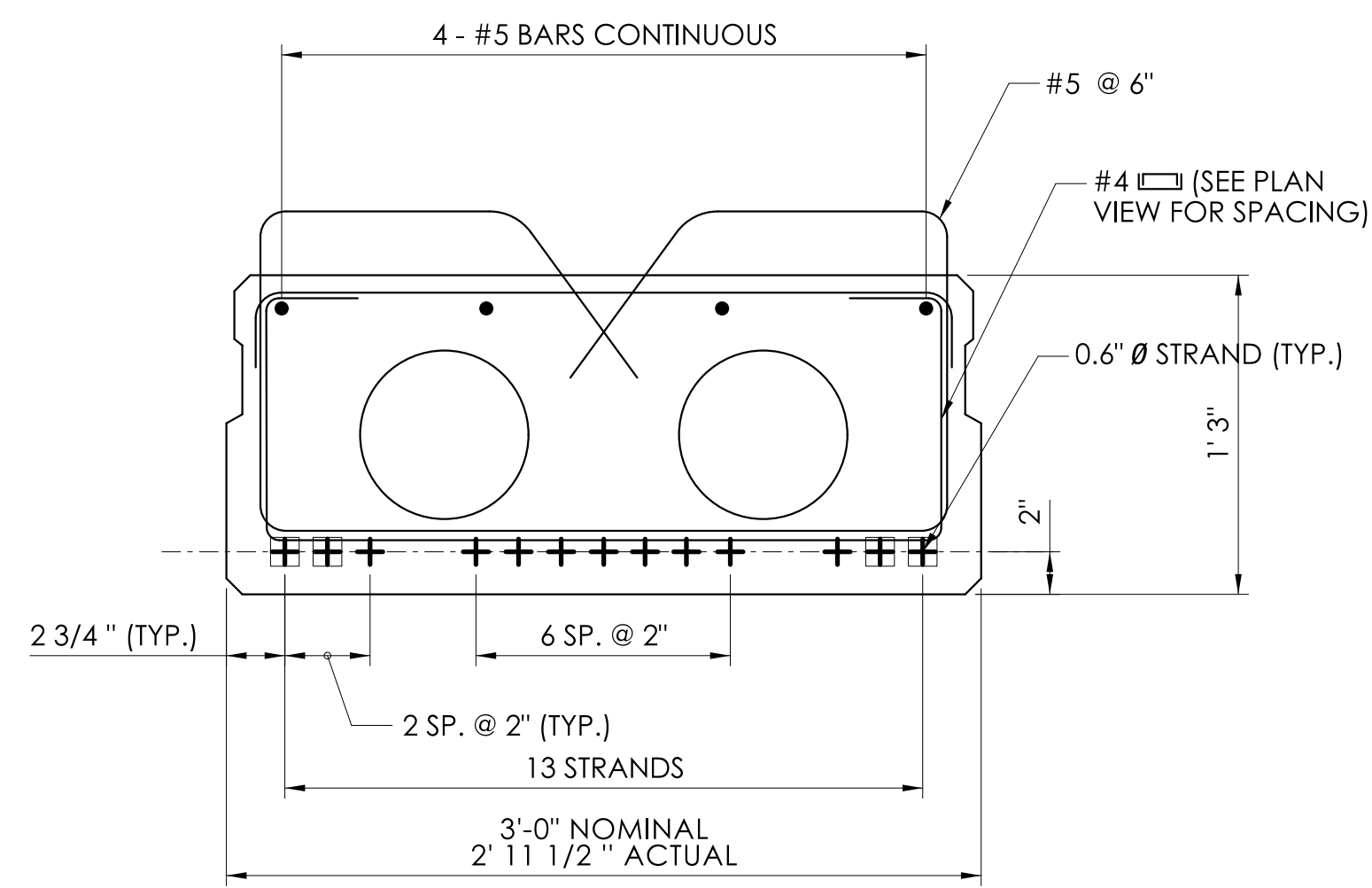
- "SEL" IS THE DEFLECTION DUE TO THE GIRDER SELF-WEIGHT.
- "PDL" IS PRE-COMPOSITE DEAD LOAD DEFLECTION DUE TO THE WEIGHT OF THE CONCRETE DECK.
- "SDL" IS THE DEFLECTION DUE TO THE SUPERIMPOSED DEAD LOAD (CONCRETE CURB, RAILING, AND ASPHALT OVERLAY).
- ALL DEFLECTIONS ARE IN INCHES AND ARE COMPUTED AT THE MIDSPAN OF THE GIRDER AT 30 DAYS.
- POSITIVE DEFLECTION VALUES INDICATE DOWNWARD DEFLECTION.

| BEAM NO. | DEAD LOAD DEFLECTIONS AT MIDSPAN (INCHES) | | |
|-----------------|---|------|------|
| | SELF | PDL | SDL |
| B1, B2, B6 & B7 | 1.56 | 0.44 | 0.09 |
| B3, B4 & B5 | 1.60 | 0.45 | 0.08 |

| | | | | | |
|----------------------|----------------|------------------|--|---|----------------------------------|
| DESIGNER/DRAFTER: JT | CHECKED BY: DW | SIGNATURE/BLOCK: | STATE OF CONNECTICUT TOWN OF WILTON | PROJECT NUMBER: 0161-0142 PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVERS LANE OVER COMSTOCK BROOK TOWN(S): WILTON DRAWING TITLE: FRAMING PLAN | DRAWING NO. S-16 SHEET NO. |
|----------------------|----------------|------------------|--|---|----------------------------------|

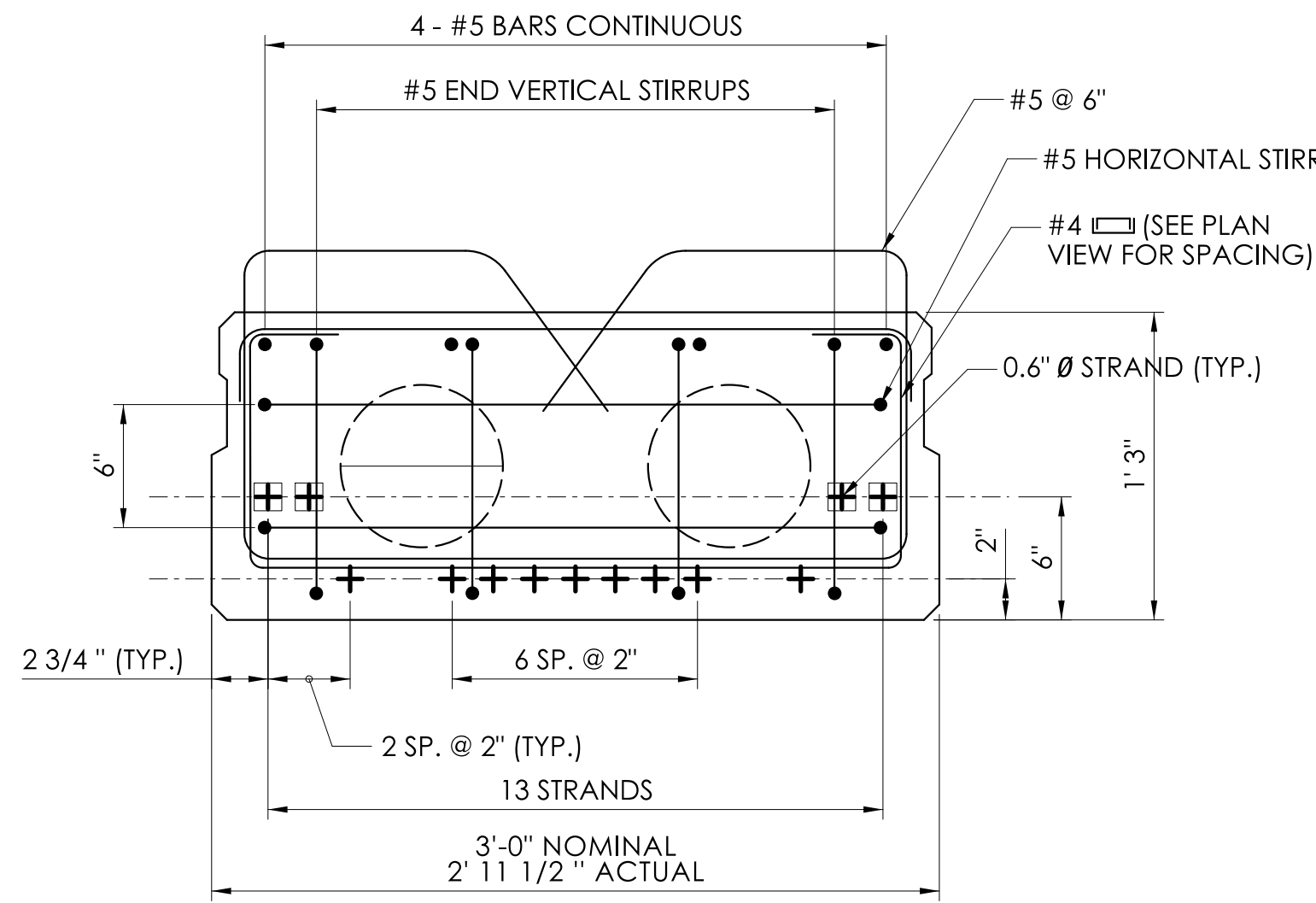


HOLD DOWN POINTS FOR DRAPED STRANDS
SCALE: N.T.S.



NOTE:
STRANDS MARKED THUS: ⊕ INDICATE DRAPED STRANDS

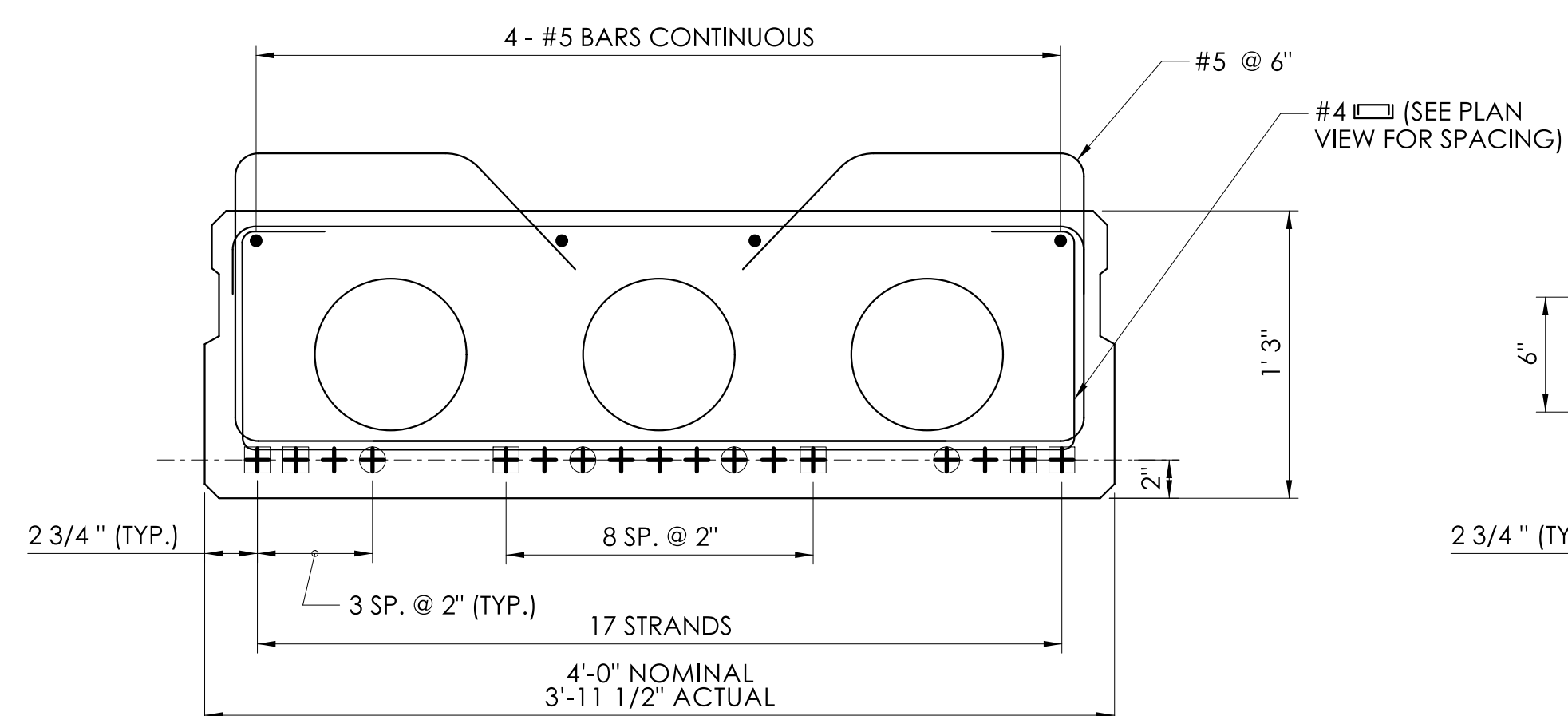
SECTION THRU MIDSPAN
SCALE: 1 1/2" = 1'-0"



NOTE:
STRANDS MARKED THUS: ⊕ INDICATE DRAPED STRANDS

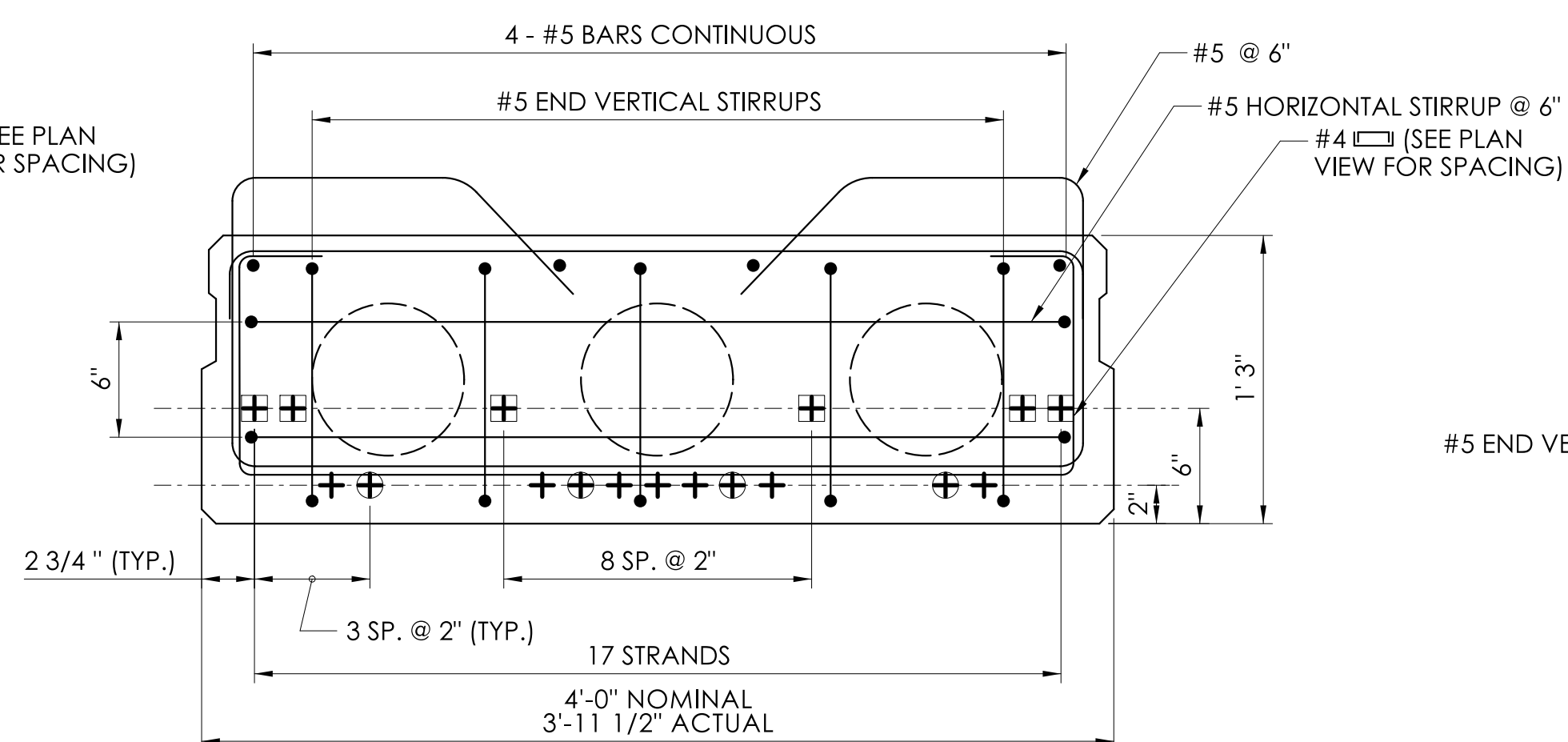
SECTION AT BEAM END
SCALE: 1 1/2" = 1'-0"

**PRESTRESSED DECK UNIT (3'-0" X 1'-3")
BEAMS B3, B4, B5**



NOTE:
STRANDS MARKED THUS: ⊕ INDICATE DRAPED STRANDS
STRANDS MARKED THUS: ⊕ INDICATE STRANDS DEBONDED FOR 6 FT FROM ENDS OF BEAMS

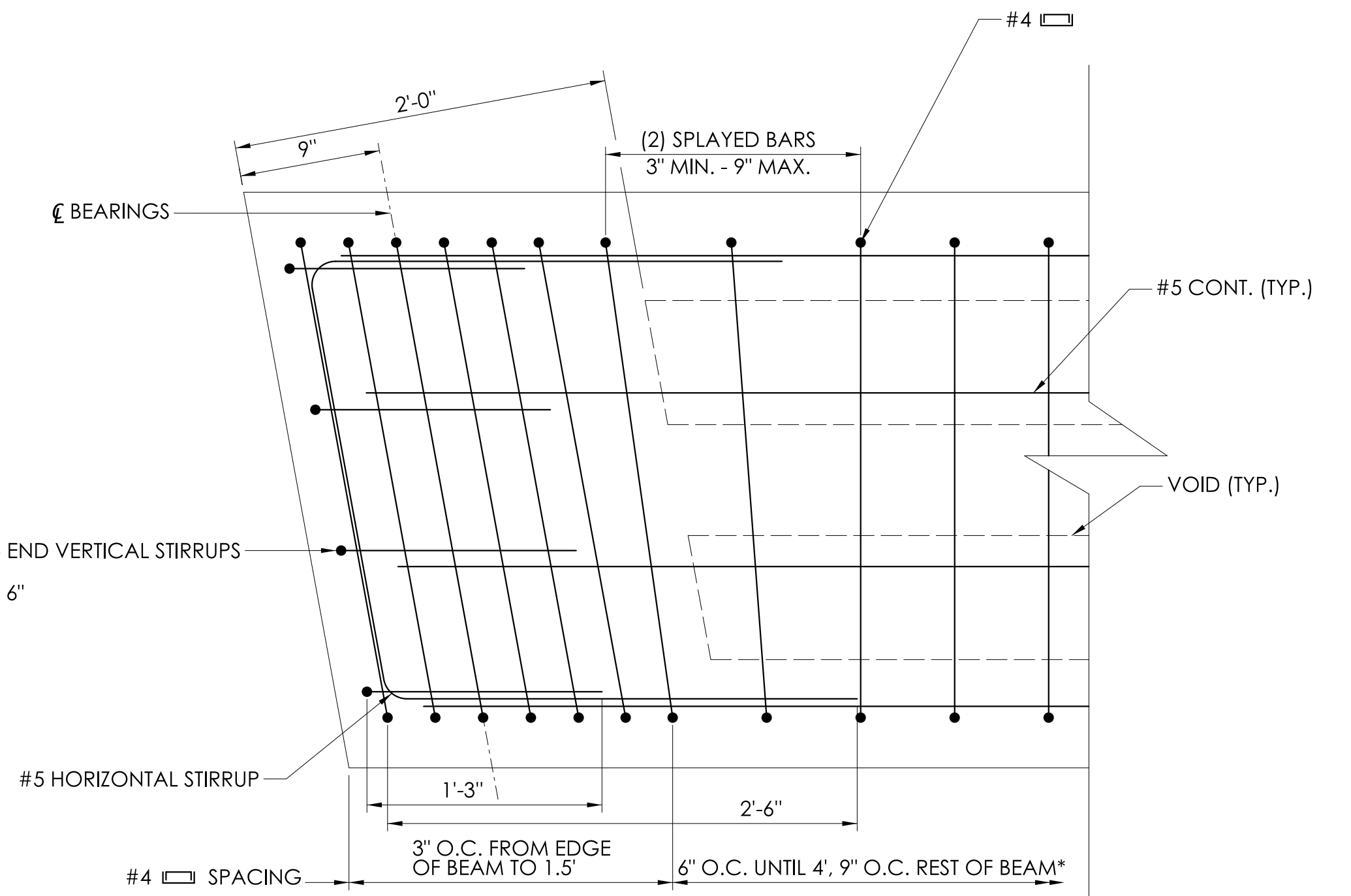
SECTION THRU MIDSPAN
SCALE: 1 1/2" = 1'-0"



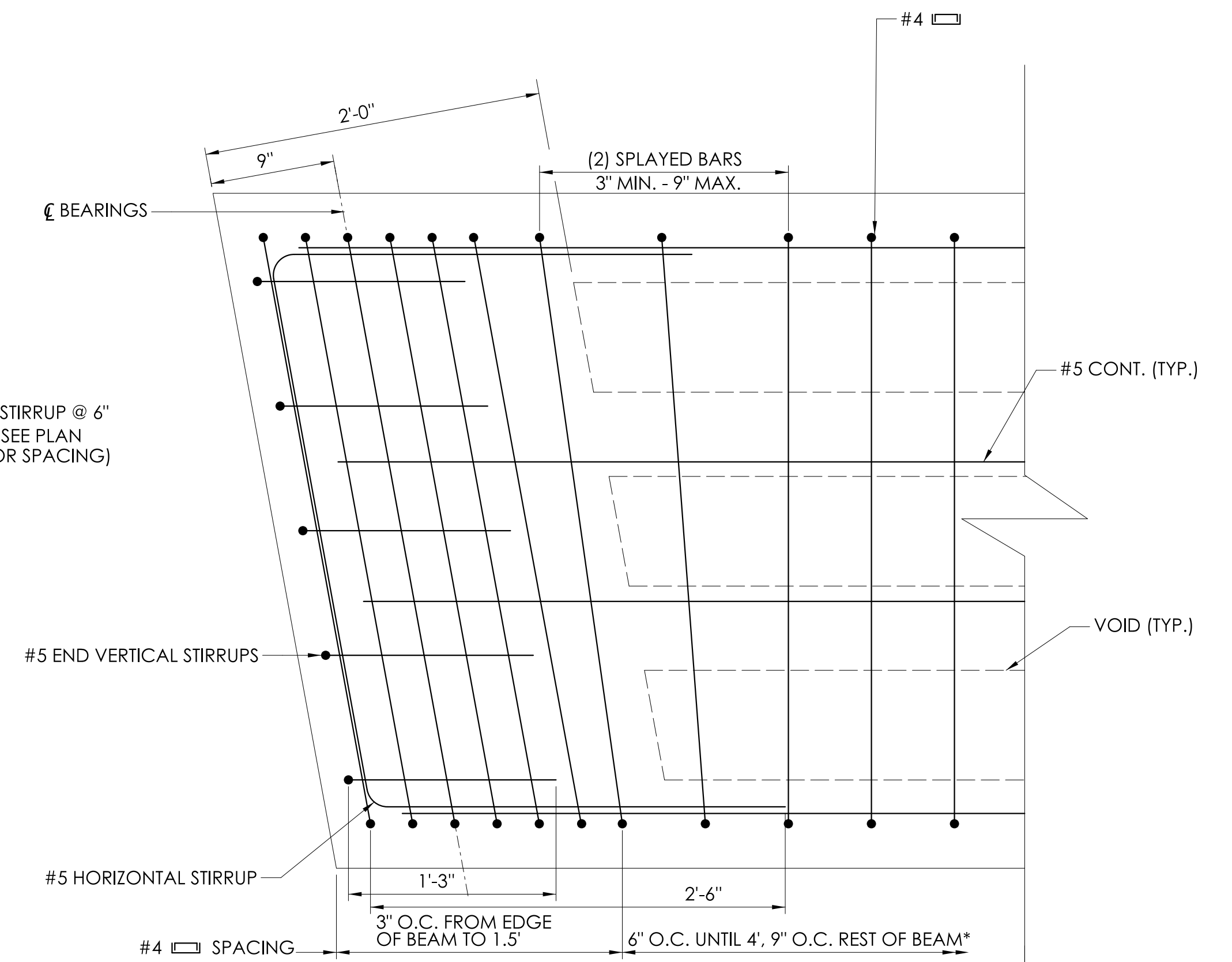
NOTE:
STRANDS MARKED THUS: ⊕ INDICATE DRAPED STRANDS
STRANDS MARKED THUS: ⊕ INDICATE STRANDS DEBONDED FOR 6 FT FROM ENDS OF BEAMS

SECTION AT BEAM END
SCALE: 1 1/2" = 1'-0"

**PRESTRESSED DECK UNIT (4'-0" X 1'-3")
BEAMS B1, B2, B6, B7**



PLAN VIEW
SCALE: 1 1/2" = 1'-0"

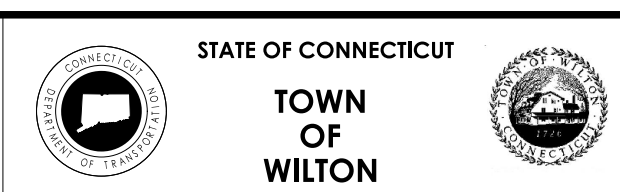


PLAN VIEW
SCALE: 1 1/2" = 1'-0"

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |

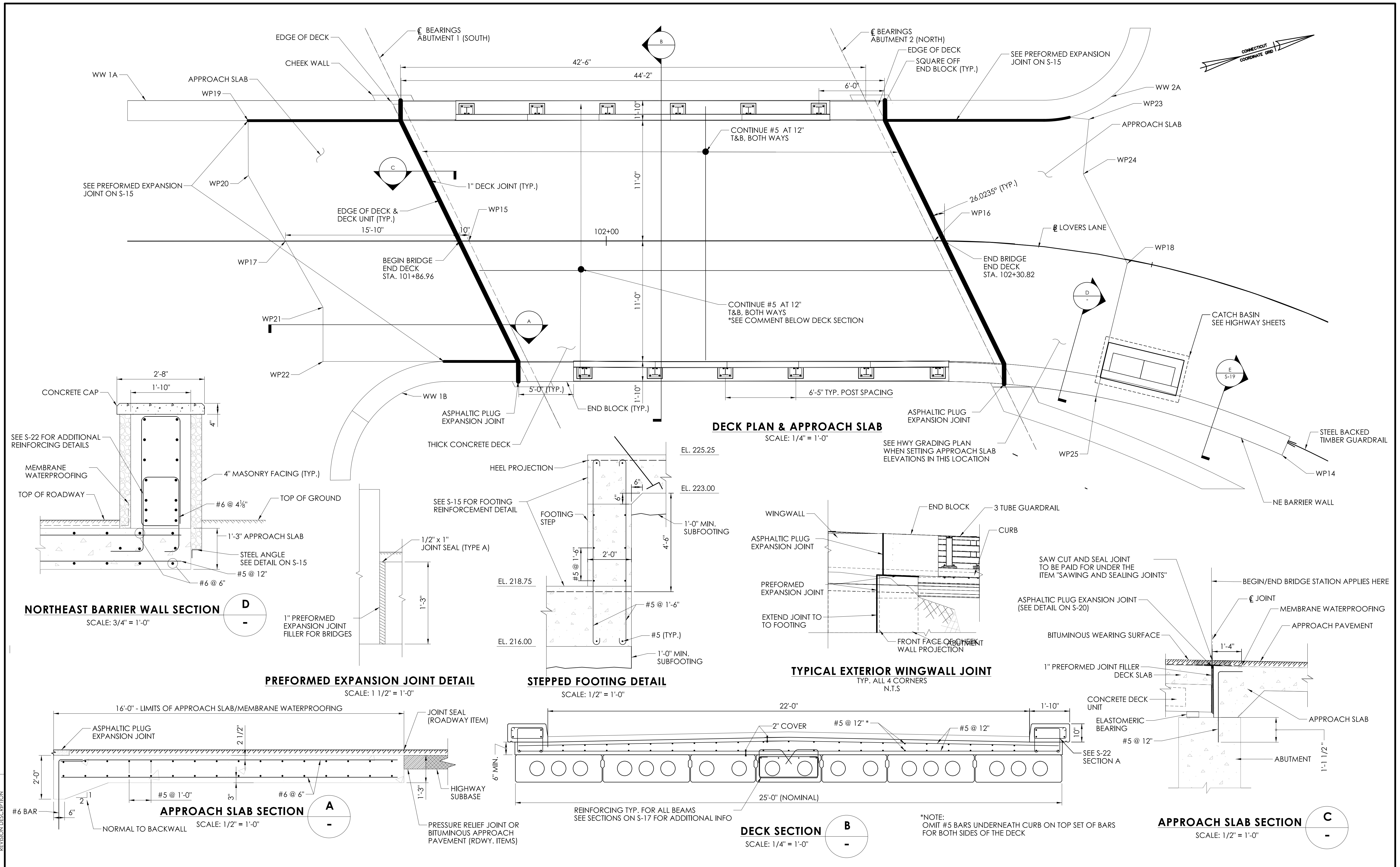
DESIGNER/DRAFTER: JT
CHECKED BY: DW

SIGNATURE/
BLOCK:



PROJECT NUMBER: 0161-0142
PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04875 LOVERS LANE OVER COMSTOCK BROOK
TOWN(S): WILTON
DRAWING TITLE: BEAM DETAILS

DRAWING NO.
S-17
SHEET NO.



NORTHEAST BARRIER WALL SECTION
SCALE: 3/4" = 1'-0"

PREFORMED EXPANSION JOINT DETAIL
SCALE: 1 1/2" = 1'-0"

STEPPED FOOTING DETAIL
SCALE: 1/2" = 1'-0"

TYPICAL EXTERIOR WINGWALL JOINT
TYP. ALL 4 CORNERS
N.T.S.

APPROACH SLAB SECTION
SCALE: 1/2" = 1'-0"

DECK SECTION
SCALE: 1/4" = 1'-0"

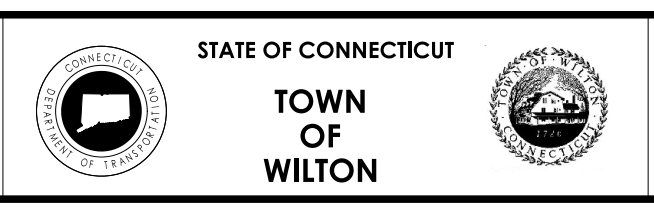
APPROACH SLAB SECTION
SCALE: 1/2" = 1'-0"

*NOTE:
OMIT #5 BARS UNDERNEATH CURB ON TOP SET OF BARS
FOR BOTH SIDES OF THE DECK

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |

DESIGNER/DRAFTER: JT
CHECKED BY: DW

SIGNATURE/
BLOCK:

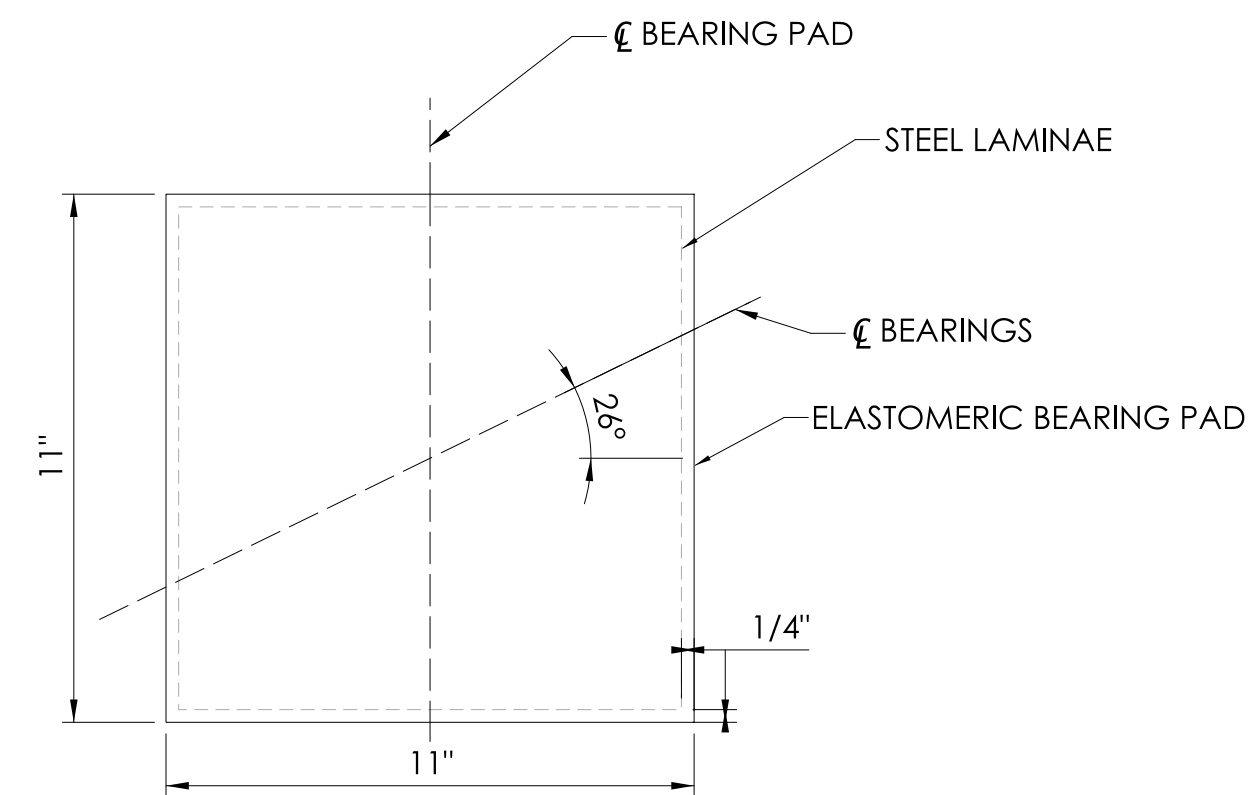


PROJECT NUMBER: 0161-0142
PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVERS LANE OVER COMSTOCK BROOK
TOWN(S): WILTON
DRAWING TITLE: DECK DETAILS

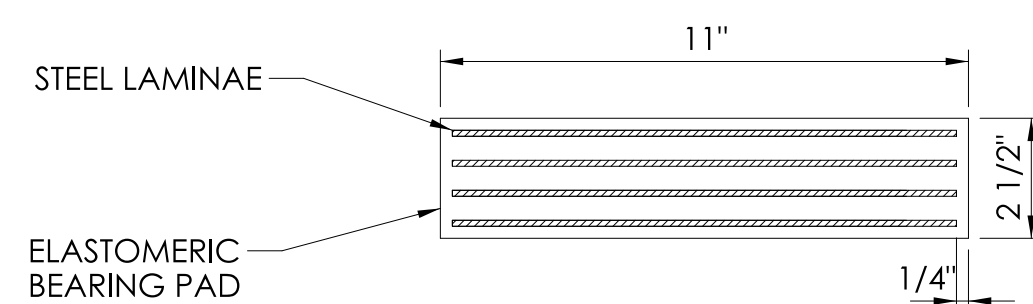
DRAWING NO.
S-18
SHEET NO.

ELASTOMERIC BEARING NOTES:

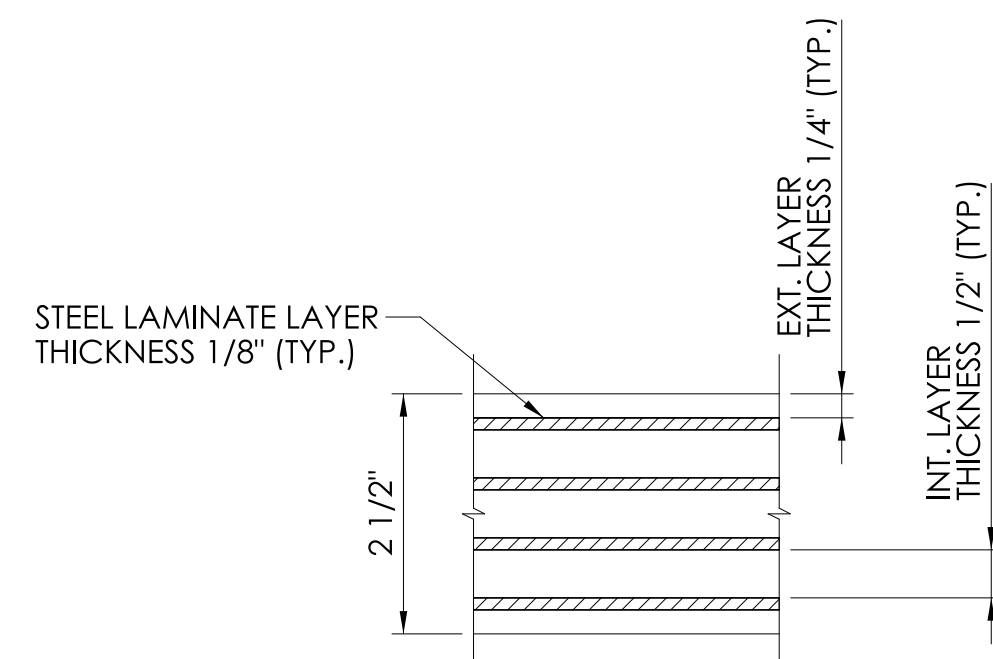
1. THE ELASTOMERIC BEARINGS ARE DESIGNED USING METHOD A OF THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS.
2. THE ELASTOMER SHALL BE TYPE CT, GRADE 3 AS DEFINED BY ASTM D4104 AND SHALL HAVE A SHORE S DUROMETER HARDNESS OF 50 +/- 5 POINTS AND A SHEAR MODULUS WITHIN LIMITS OF 200 TO 250 PSL. THE ELASTOMER SHALL CONTAIN ONLY VIRGIN POLYCHLOROPRENE (NEOPRENE) AS THE RAW POLYMER.
3. THE ELASTOMERIC BEARINGS SHALL BE INSTALLED WHEN THE AMBIENT TEMPERATURE IS BETWEEN 40°F AND 77°F AND HAS BEEN WITHIN THIS RANGE FOR MORE THAN TWO HOURS.
4. THE CONCRETE ABUTMENT SEATS SHALL BE CAREFULLY FINISHED SMOOTH TO AN EVEN, LEVEL SURFACE AND SHALL SHOW NO VARIATIONS FROM A TRUE PLANE GREATER THAN 1/16".
5. THE INTERNAL STEEL LAMINAE SHALL CONFORM TO ASTM A570, GRADE 36.
6. ELASTOMERIC BEARING PADS SHALL BE PAID UNDER THE ITEM "ELASTOMERIC BEARING PADS".



BEARING PLAN
SCALE: 3" = 1'-0"



BEARING ELEVATION
SCALE: 3" = 1'-0"



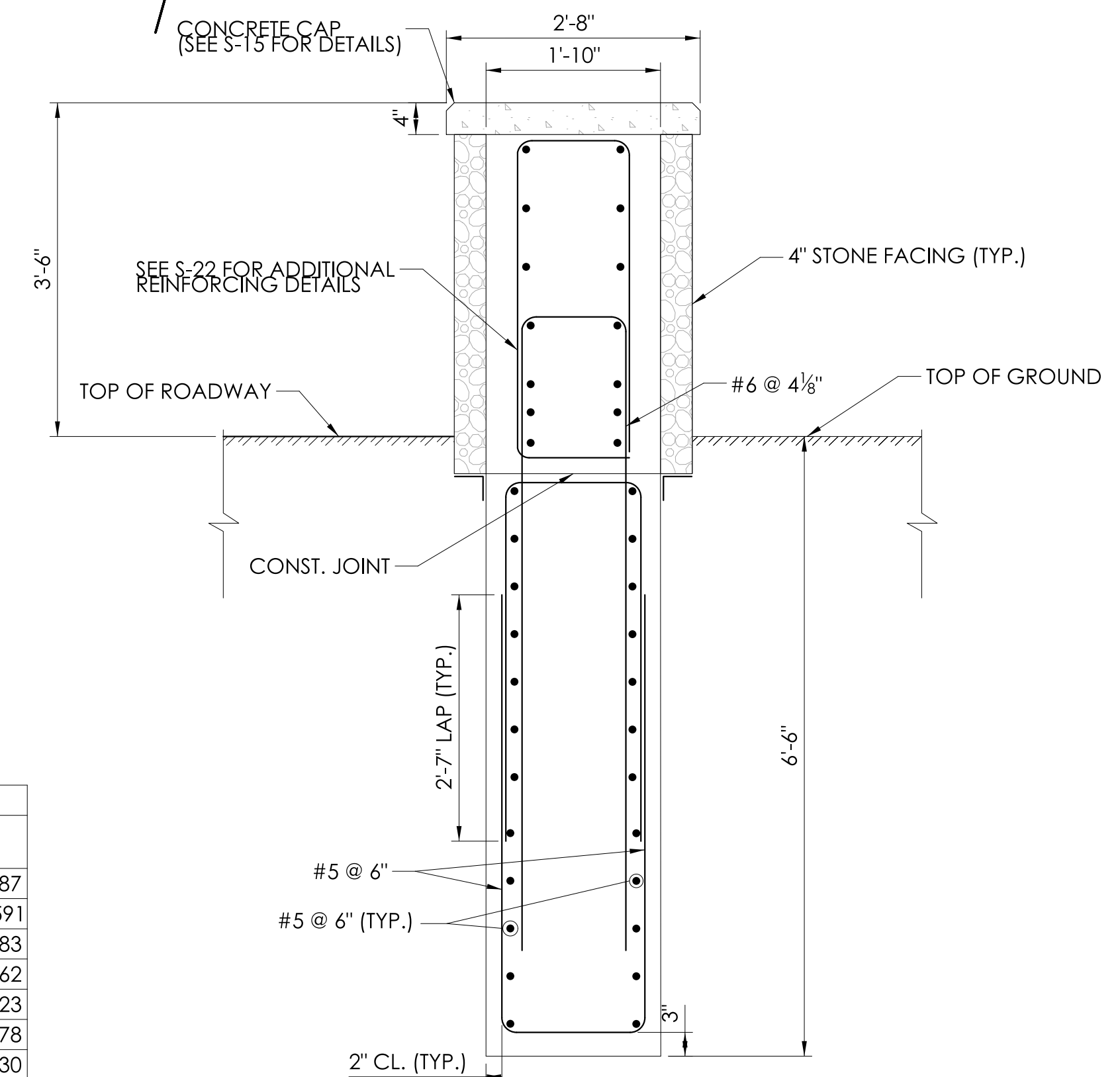
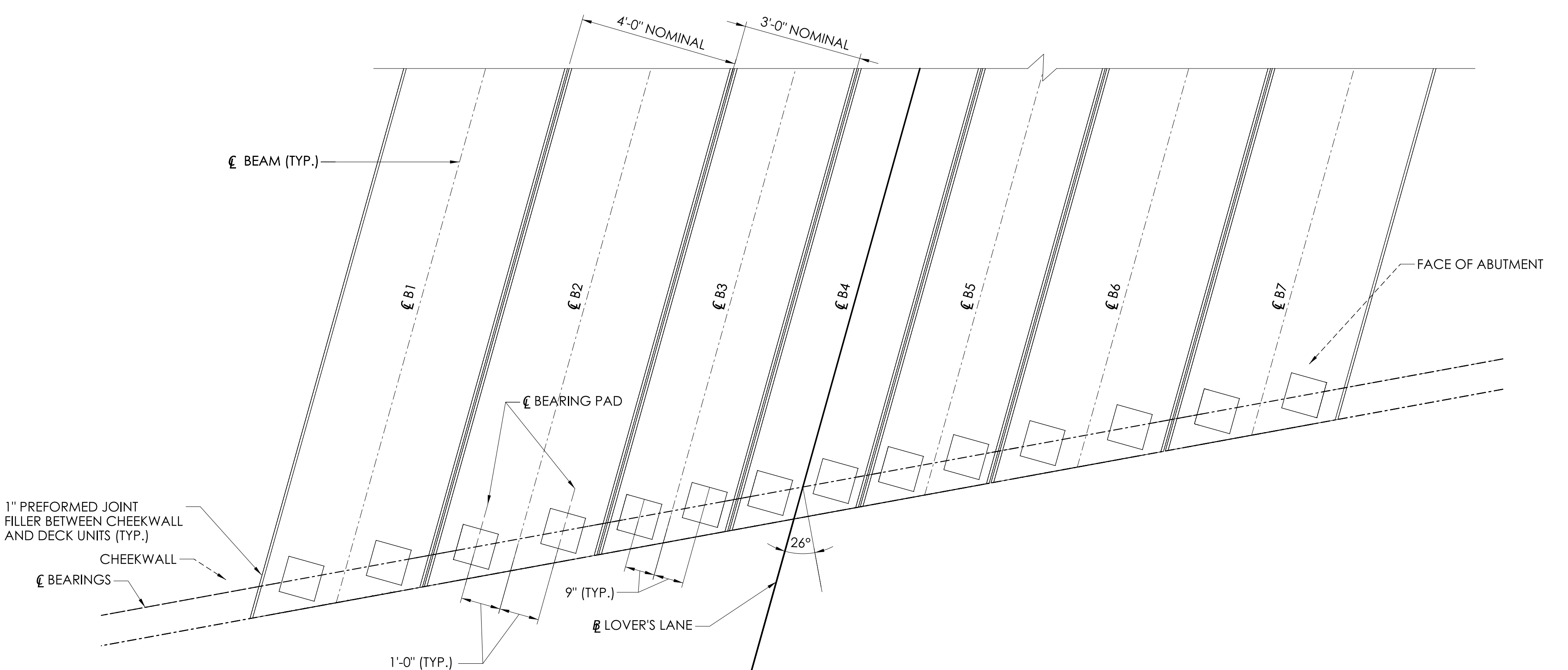
BEARING SECTION
SCALE: 6" = 1'-0"

ELASTOMERIC BEARING DETAIL

| *BEARING DESIGN LOAD TABLE | | |
|----------------------------|----------------|----------------|
| | SOUTH ABUTMENT | NORTH ABUTMENT |
| SERVICE DEAD LOADS (KIPS) | 25.55 | 25.55 |
| SERVICE LIVE LOADS (KIPS) | 34.9 | 34.9 |

*LOADS ARE PER BEARING, 2 BEARINGS PER BEAM

| € OF BEAM | FINISHED DECK ELEVATIONS L=42'-6" | | | | |
|-----------|-----------------------------------|---------|---------|---------|-------------------------|
| | 0/4L (€ NORTH ABUTMENT) | 1/4L | 2/4L | 3/4L | 4/4L (€ SOUTH ABUTMENT) |
| B7 | 234.504 | 234.143 | 233.854 | 233.637 | 233.487 |
| B6 | 234.658 | 234.284 | 233.982 | 233.751 | 233.591 |
| B5 | 234.795 | 234.409 | 234.096 | 233.853 | 233.683 |
| B4 | 234.913 | 234.518 | 234.195 | 233.943 | 233.762 |
| B3 | 234.913 | 234.508 | 234.175 | 233.913 | 233.723 |
| B2 | 234.915 | 234.499 | 234.154 | 233.880 | 233.678 |
| B1 | 234.919 | 234.490 | 234.132 | 233.845 | 233.630 |



BARRIER WALL/WINGWALL 1B TERMINATION SECTION

SCALE: 1/2" = 1'-0"

E
S-18

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |

BITUMINOUS CONCRETE PLACEMENT AT ASPHALTIC PLUG JOINTS (APJ)

- THE REQUIREMENTS OF SPECIAL PROVISION SECTION 4.06 SHALL BE MET EXCEPT IN LIEU OF DENSITY TESTING, THE METHODS DESCRIBED BELOW SHALL BE FOLLOWED TO ASSURE PROPER COMPACTION.
- TOP LIFT MUST BE UNIFORM THICKNESS; INTERMEDIATE LIFTS CAN BE PLACED AT 1 1/4" TO 2 1/2" COMPACTED.
- REQUIREMENTS FOR PROPER COMPACTION:
 - MINIMUM 265° F DELIVERY TEMPERATURE OF MATERIAL. PLACE AND SPREAD MATERIAL BEFORE IT COOLS TO 260° F. MATERIAL BELOW TEMPERATURE REQUIREMENT WILL BE REJECTED.
 - COMPACT NON-SURFACE LIFTS WITH VIBRATORY PLATE COMPACTOR MEETING THE FOLLOWING REQUIREMENTS:
 - DESIGNED TO COMPACT ASPHALT
 - EQUIPPED WITH A WATER TANK
 - CENTRIFUGAL FORCE 3200 LBS TO 6000 LBS
 - WEIGHS MINIMUM 160 LBS (WITHOUT WATER)
 - MINIMUM 4400 VIBRATIONS PER MINUTE
 - COMPACT TOP LIFT WITH 3 1/2 TO 4 1/2 TON DOUBLE DRUM ROLLER, DESIGNED TO COMPACT BITUMINOUS CONCRETE.
 - PROVIDE NUMBER OF PASSES BASED ON LIFT THICKNESS AS FOLLOWS:

| LIFT THICKNESS (INCHES) | NUMBER OF PASSES |
|-------------------------|------------------|
| 1 1/4 TO 1 1/2 | 8 |
| 1 1/2 TO 2 | 10 |
| 2 TO 2 1/2 | 12 |
- ADDITIONAL COMPACTING EQUIPMENT MAY BE REQUIRED TO COMPLETE LIFT COMPACTION BEFORE MATERIAL COOLS TO 180° F.
- AT CORNERS OR OTHER AREAS INACCESSIBLE TO PLATE TAMPER, HAND TAMP 20 TIMES MINIMUM BEFORE MATERIAL COOLS TO 180° F.
- ALTERNATE EQUIPMENT MAY BE REQUESTED AS A SUPPLEMENT TO CONTRACTOR'S QC PLAN, THE EQUIPMENT AND PROCEDURES MUST BE APPROVED BY THE ENGINEER PRIOR TO USE.
- IF THESE METHODS ARE NOT PERFORMED TO THE SATISFACTION OF THE ENGINEER, DENSITY VERIFICATION MAY BE REQUIRED WHEREIN THE CONTRACTOR SHALL PROVIDE DENSITY TESTING WITH A QC NUCLEAR DENSITY GAUGE OR COLLECT CORE SAMPLES AS SPECIFIED IN SECTION 4.06.

ASPHALTIC PLUG EXPANSION JOINT SYSTEM NOTES

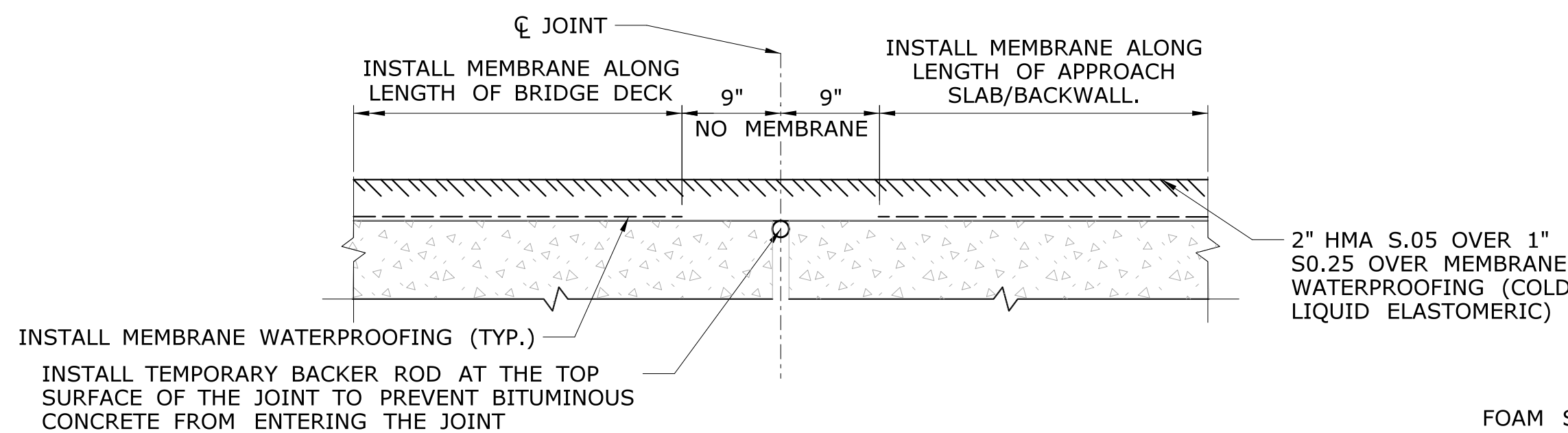
- A BRIDGING PLATE SHALL BE USED TO SPAN THE GAP BETWEEN TWO DECK ENDS OR THE JOINT BETWEEN A DECK END AND A CONCRETE APPROACH SLAB.
- DISCONTINUE THE INSTALLATION OF THE BRIDGING PLATE WHERE THE APPROACH SLAB IS DISCONTINUED (TYPICALLY IN THE ROADWAY SHOULDERS). SEE "ASPHALTIC PLUG EXPANSION JOINT SYSTEM" SPECIAL PROVISION.
- NEW STEEL BRIDGING PLATES SHALL BE A MINIMUM OF 1/4" THICK BY 8" WIDE. FOR JOINT OPENINGS WHICH EXCEED 3", A 3/8" THICK BY 12" WIDE PLATE WILL BE REQUIRED.
- NO BRIDGING PLATE SHALL BE USED AT THE FOLLOWING LOCATIONS:
 - JOINT BETWEEN A DECK END AND A CONCRETE APPROACH PAVEMENT
 - WHERE A BRIDGE DECK END MEETS A BITUMINOUS APPROACH PAVEMENT
- THE REMOVAL OF ALL EXISTING JOINT SYSTEMS, BITUMINOUS CONCRETE OVERLAY, MEMBRANE WATERPROOFING AND BOND BREAKER WITHIN THE LIMITS SHOWN TO BE INCLUDED FOR PAYMENT UNDER THE ITEM "REMOVAL OF EXISTING WEARING SURFACE".
- TEMPORARY CLOSED CELL BACKER ROD DIAMETER SHALL BE DETERMINED AFTER MEASURING THE JOINT OPENING, THE ROD SHALL BE 25% LARGER THAN THE JOINT OPENING.
- INSTALLATION OF MEMBRANE WITHIN THE LIMITS SHOWN TO BE PAID UNDER THE ITEM, "MEMBRANE WATERPROOFING (COLD LIQUID ELASTOMERIC)".
- THE FURNISHING AND PLACING OF HMA S0.5 AND HMA S0.25 TO BE INCLUDED FOR PAYMENT UNDER THE ITEM "HMA S0.5 AND HMA S0.25" RESPECTIVELY.
- SAW-CUTTING AND REMOVAL OF PAVEMENT FOR JOINT INSTALLATION TO BE INCLUDED FOR PAYMENT UNDER THE ITEM "ASPHALTIC PLUG EXPANSION JOINT SYSTEM".
- INSTALLATION OF FOAM SUPPORTED SILICONE GLAND TO BE PAID UNDER THE ITEM "PREFORMED JOINT SEAL".
- ASPHALTIC PLUG EXPANSION JOINT SYSTEMS MAY BE INSTALLED ONLY WITHIN THE TEMPERATURE RANGE SPECIFIED IN THE SPECIAL PROVISION "ASPHALTIC PLUG EXPANSION JOINT SYSTEM". REFERENCE THE RANGE OF THERMAL MOVEMENT FOR THE SELECTED JOINT PRODUCT IN THE TABLE FOR "INSTALLATION RESTRICTIONS" IN THE SPECIAL PROVISION.
- EXPLORATION OF PAVEMENT THICKNESS AND JOINT LOCATION TO BE INCLUDED IN THE GENERAL COST OF THE ITEM "REMOVAL OF EXISTING WEARING SURFACE".
- CONTRACTOR SHALL NOTIFY THE DEPARTMENT IF THE EXISTING PAVEMENT IS DETERMINED TO BE LESS THAN 2" OR GREATER THAN 6" WITHIN THE BRIDGE LIMITS.
- FOAM SUPPORTED SILICONE GLAND SHALL BE INCLUDED FOR PAYMENT UNDER ITEM "PREFORMED JOINT SEAL."

JOINT WORK FOR BRIDGES

- ALL WORK TO REMOVE BITUMINOUS CONCRETE OVERLAY, MEMBRANE WATERPROOFING, EXISTING JOINT COMPONENTS AND SEALING ELEMENTS SHALL BE INCLUDED IN THE COST OF "REMOVAL OF EXISTING WEARING SURFACE".
- WHERE EXISTING BRIDGE DECK JOINTS ARE CONCEALED BENEATH BITUMINOUS CONCRETE OVERLAY THE CONTRACTOR SHALL VERIFY THE BRIDGE DECK JOINT LOCATION AND SUBMIT THE LIMITS OF SAW-CUTTING FOR THE ENGINEERS APPROVAL.
- THE FURNISHING AND PLACING OF TEMPORARY PAVEMENT IN THE JOINT CUT-OUT SHALL CONFORM TO "BITUMINOUS CONCRETE PLACEMENT AT ASPHALTIC PLUG JOINTS (APJ)" AND SHALL BE INCLUDED FOR PAYMENT UNDER THE ITEM "HMA S0.5."
- MEMBRANE WATERPROOFING SHALL BE "MEMBRANE WATERPROOFING (COLD LIQUID ELASTOMERIC)" AND SHALL BE PLACED PRIOR TO PLACEMENT OF PAVEMENT OVERLAY.
- ROUGH OR DAMAGED CONCRETE DECK SURFACES SHALL BE REPAIRED WITH A CONCRETE LEVELING MATERIAL INCLUDED FOR PAYMENT UNDER THE ITEM "ASPHALTIC PLUG EXPANSION JOINT SYSTEM".
- THE DEPTH OF PROPOSED ASPHALTIC PLUG JOINT IS ESTIMATED TO BE 4" AVERAGE.

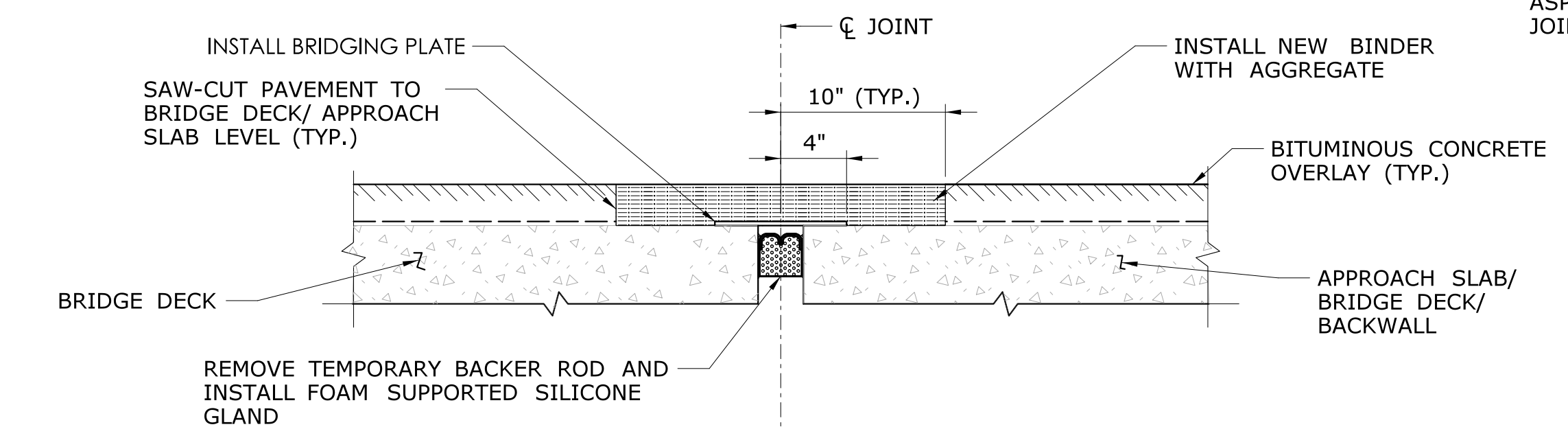
SUGGESTED SEQUENCE OF WORK

- STEP 1: CONTRACTOR SHALL PERFORM AN EXPLORATION AT THE GUTTERLINE (AT THE FOUR CORNERS OF THE BRIDGE) AND THE CROWN (AT THE BEGINNING AND END OF THE BRIDGE). A MINIMUM OF SIX REPRESENTATIVE DEPTH MEASUREMENTS SHALL BE TAKEN PER BRIDGE AT THESE LOCATIONS TO DETERMINE THE DEPTH OF PAVEMENT AND THE LOCATION OF THE DECK ENDS (CENTERLINE OF PROPOSED JOINT). ADDITIONAL MEASUREMENTS SHALL BE TAKEN IF NEEDED IN ACCORDANCE WITH SPECIAL PROVISION FOR "REMOVAL OF EXISTING WEARING SURFACE." CONTRACTOR SHALL ALSO MEASURE THE DECK JOINT GAP OPENING FOR SIZING OF THE FOAM SUPPORTED SILICONE GLAND.
- STEP 2: REMOVE EXISTING PAVEMENT MATERIAL AND JOINT MATERIAL TO BRIDGE DECK LEVEL ALONG ENTIRE LENGTH OF BRIDGE. REMOVE BRIDGING PLATES PRIOR TO MILLING THE BRIDGE DECK.
- STEP 3: INSTALL TEMPORARY BACKER ROD FLUSH WITH THE BRIDGE DECK AND APPROACH SLAB OR BACKWALL.
- STEP 4: INSTALL MEMBRANE WATERPROOFING TO THE TOP OF DECK AND APPROACH SLAB WITHIN THE LIMITS SHOWN.
- STEP 5: PLACE BITUMINOUS CONCRETE OVERLAY AS INDICATED ON THE PLANS.
- STEP 6: SAW-CUT PAVEMENT FULL DEPTH AT 10" EACH SIDE OF CENTERLINE OF JOINT, AND REMOVE ALL PAVEMENT MATERIAL BETWEEN SAW-CUTS. TO BE PAID FOR UNDER THE ITEM "ASPHALTIC PLUG EXPANSION JOINT SYSTEM".
- STEP 7: INSTALL PROPOSED ASPHALTIC PLUG EXPANSION JOINT SYSTEM WITH FOAM SUPPORTED SILICONE GLAND AND BRIDGING PLATE. LOCATING PINS SHALL NOT BE USED TO SECURE THE BRIDGING PLATE.
- STEP 8: INSTALL CRACK SEAL AT CURB LINE ALONG THE LENGTH OF THE BRIDGE, BOTH SIDES. CRACK SEALING SHALL BE INCLUDED FOR PAYMENT UNDER ITEM "JOINT AND CRACK SEALING OF BITUMINOUS CONCRETE PAVEMENT."



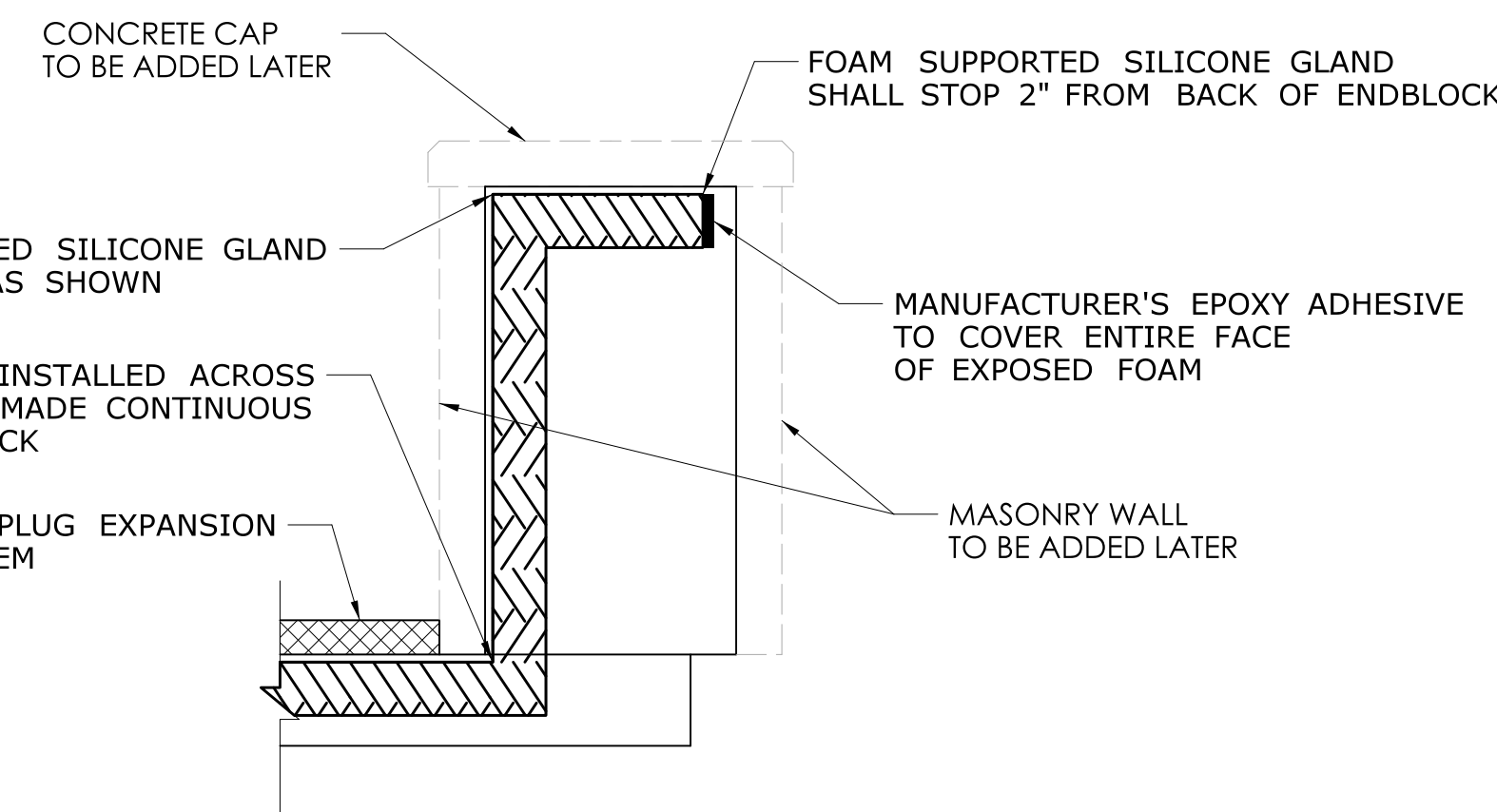
PLACEMENT OF PAVEMENT ALONG THE BRIDGE

N.T.S.



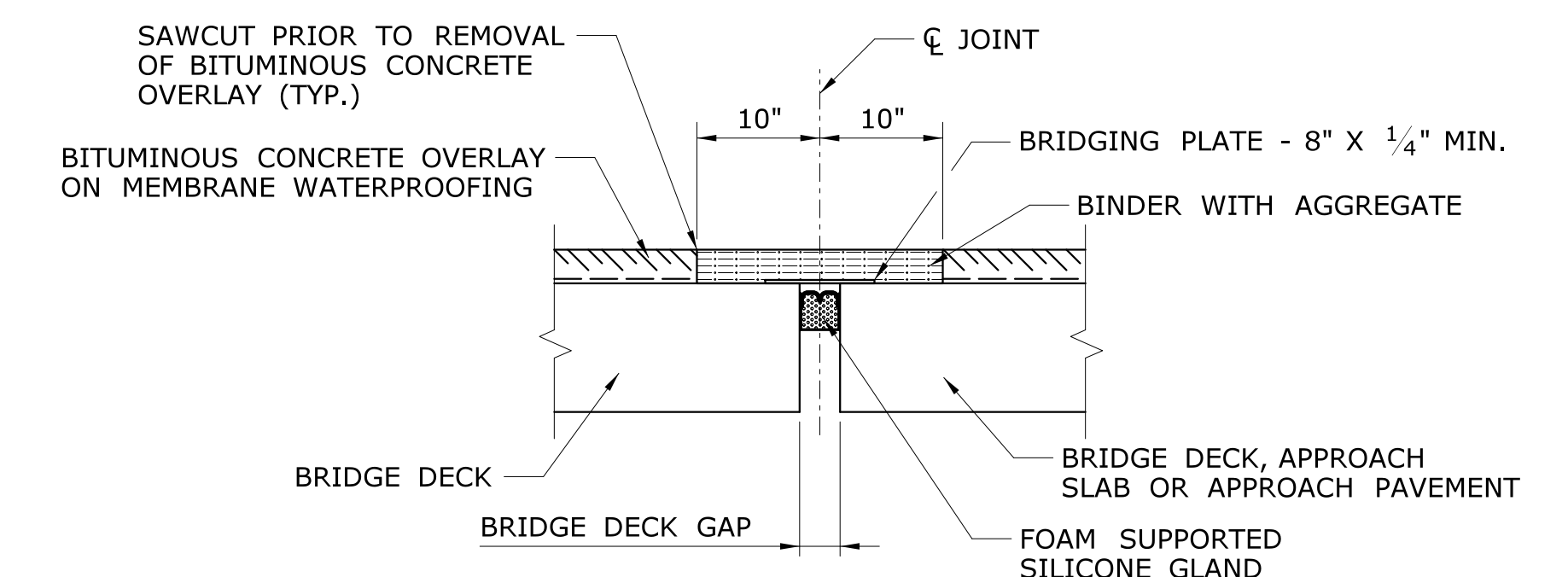
INSTALL ASPHALTIC PLUG EXPANSION JOINT

N.T.S.



SCHEMATIC OF ASPHALTIC PLUG JOINT AT ENDBLOCK

NOT TO SCALE



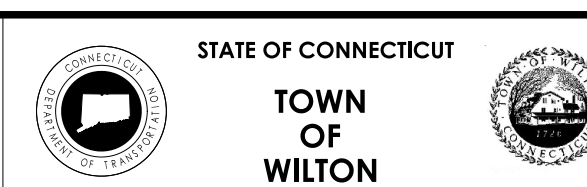
TYPICAL SECTION ASPHALTIC PLUG EXPANSION JOINT SYSTEM

NOT TO SCALE

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |
| | | |

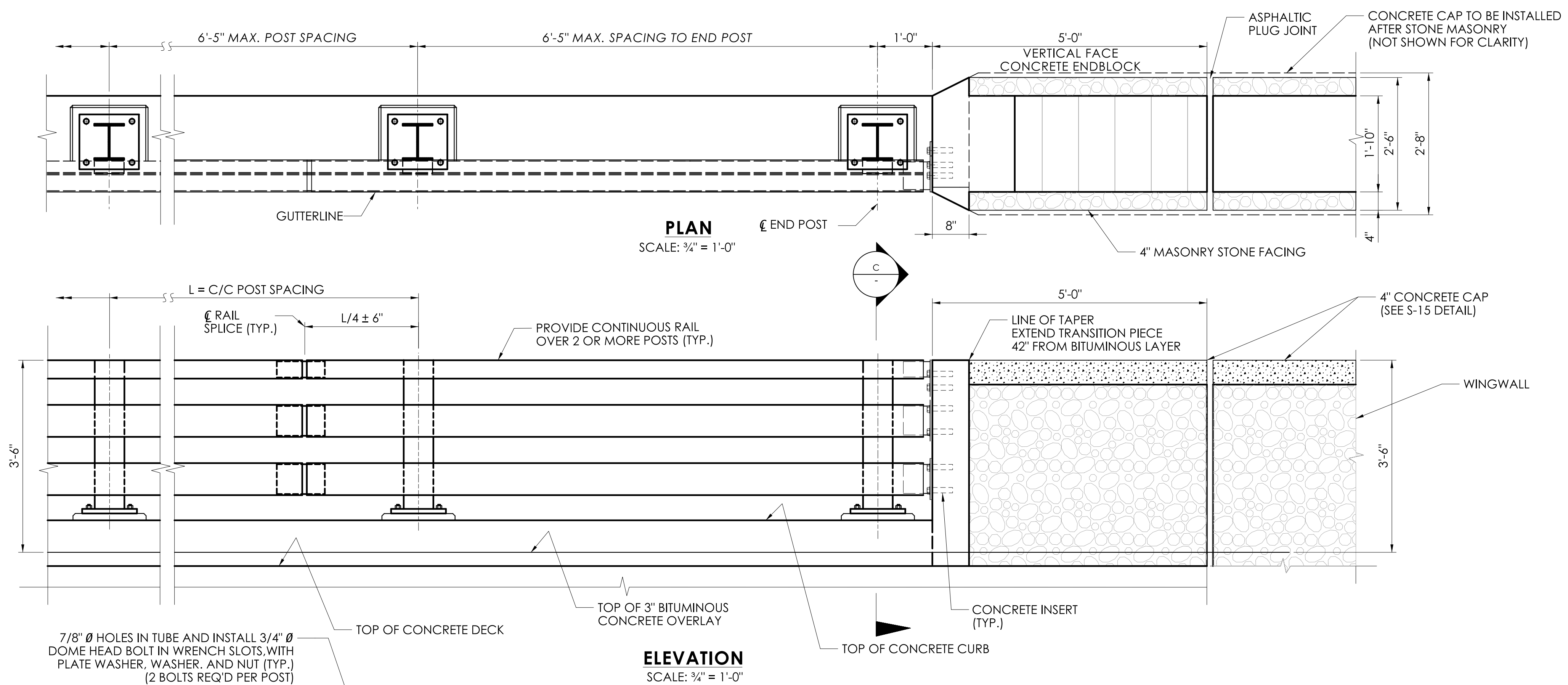
| | | |
|----------------------|----------------|------------------|
| DESIGNER/DRAFTER: JT | CHECKED BY: DW | SIGNATURE/BLOCK: |
|----------------------|----------------|------------------|

LASTED SAVED BY: Jfor FILE NAME: J:\DWG\2016\0182\10\0161-0142\Bridg\Contract_Plans\20_s8_MSH_BR04975_0161_0142_APEJ.dgn
 PLOTTED DATE: 10/21/2022



PROJECT NUMBER: 0161-0142
 PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVERS LANE OVER COMSTOCK BROOK
 TOWN(S): WILTON
 DRAWING TITLE: ASPHALTIC PLUG EXPANSION JOINT DETAILS

DRAWING NO. S-20
 SHEET NO.



PLAN
SCALE: 3/4" = 1'-0"

ELEVATION
SCALE: 3/4" = 1'-0"

BRIDGE RAIL NOTES

THE 3-TUBE CURB MOUNTED BRIDGE RAIL HAS BEEN EVALUATED AT TEST LEVEL 4 (TL-4) AND COMPLIES WITH MASH 2016.

CONCRETE FOR THE CURB AND ENDBLOCK SHALL BE CLASS PCC04462. THE COMPRESSIVE STRENGTH OF THE CONCRETE, BASED ON TEST CYLINDERS, SHALL BE NO LESS THAN 4,000 PSI PRIOR INSTALLING THE EPOXY GROUT BELOW THE BASEPLATES. PRIOR TO ALLOWING THE RAIL, CURB AND ENDBLOCK TO BE PLACED IN SERVICE FOR THE PROTECTION OF VEHICULAR TRAFFIC, THE COMPRESSIVE STRENGTH OF THE GROUT, BASED ON STRENGTH GAIN OVER TIME LISTED IN THE GROUT MANUFACTURER'S DATA SHEET, SHALL BE NO LESS THAN 5,000 PSI.

THE REINFORCEMENT SHALL CONFORM TO ASTM A615, GRADE 60 AND BE HOT-DIP GALVANIZED.

HOLLOW STRUCTURAL SHAPES SHALL CONFORM TO ASTM A500 GRADE C OR ASTM A501, GRADE B.

ALL OTHER STEEL SHALL CONFORM TO ASTM A572, GRADE 50 UNLESS NOTED OTHERWISE.

THE SILICON CONTENT OF THE STEEL USED FOR THE EXPOSED MEMBERS AND PLATE COMPONENTS SHALL FALL WITHIN THE RANGE OF 0 TO 0.4% OR 0.15% TO 0.25%.

ALL STEEL SHAPES, PLATES AND HOLLOW STRUCTURAL SECTIONS SHALL BE SHOP METALLIZED IN ACCORDANCE WITH THE SPECIAL PROVISION "METALLIZING STRUCTURAL STEEL (SITE NO. 1)". THE COLOR OF THE TOP COAT MATERIAL ON THE STEEL SHALL CONFORM TO FEDERAL STANDARD NO. 17038.

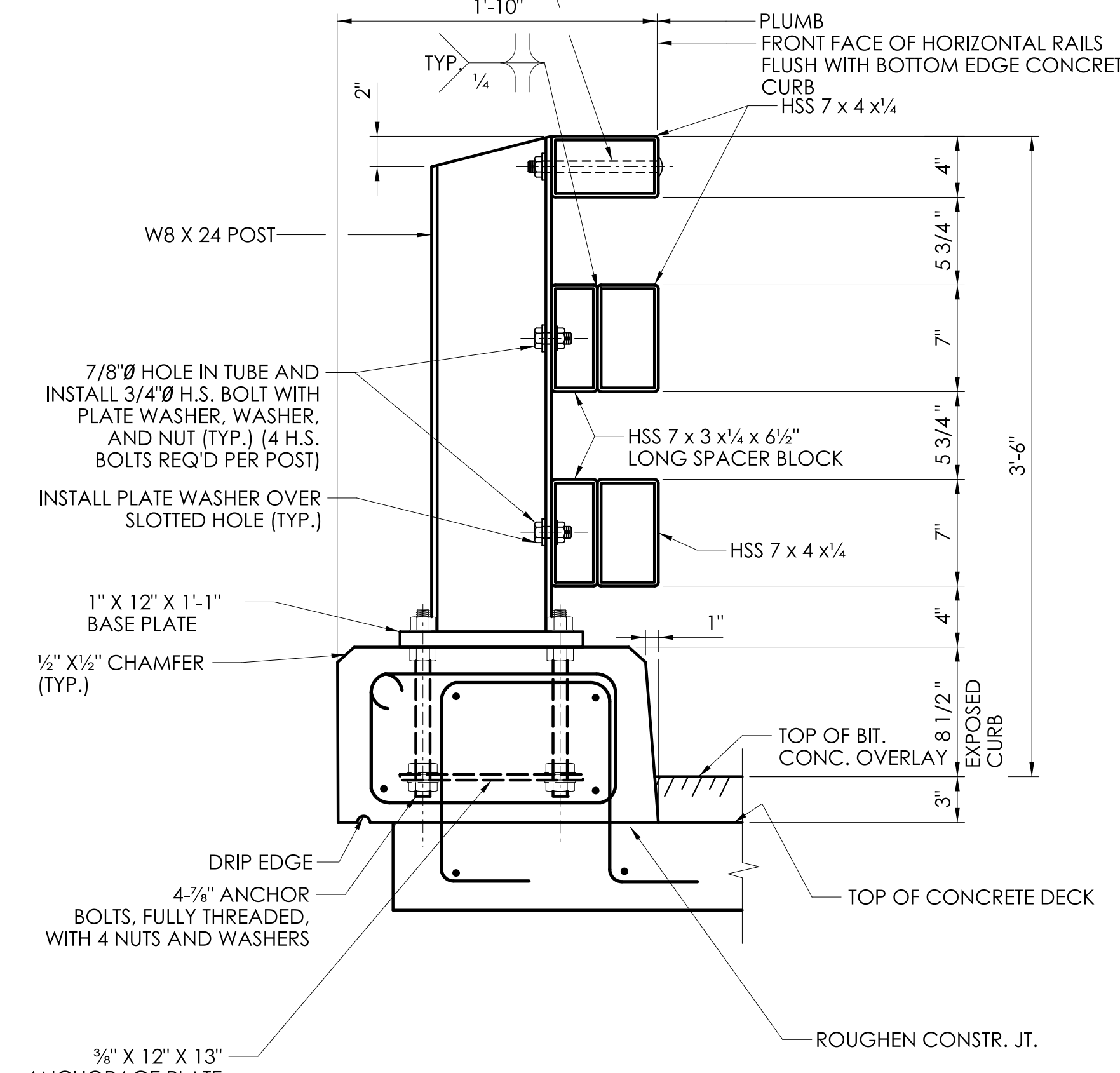
THE ANCHOR BOLTS SHALL CONFORM TO ASTM F1554, GRADE 105. THE NUTS SHALL CONFORM TO ASTM A563, GRADE DH. THE WASHERS SHALL CONFORM TO ASTM F436. THE BOLTS, NUTS AND WASHERS SHALL BE GALVANIZED IN ACCORDANCE WITH ASTM F2329.

ALL HIGH STRENGTH BOLTS SHALL CONFORM TO ASTM F3125 GRADE A325, TYPE 1. NUTS SHALL CONFORM TO ASTM A563, GRADE DH. CIRCULAR, FLAT, HARDENED STEEL WASHERS SHALL CONFORM TO ASTM F436. THE BOLTS, NUTS AND WASHERS SHALL BE GALVANIZED IN ACCORDANCE WITH ASTM F2329 OR ASTM B695, CLASS 55.

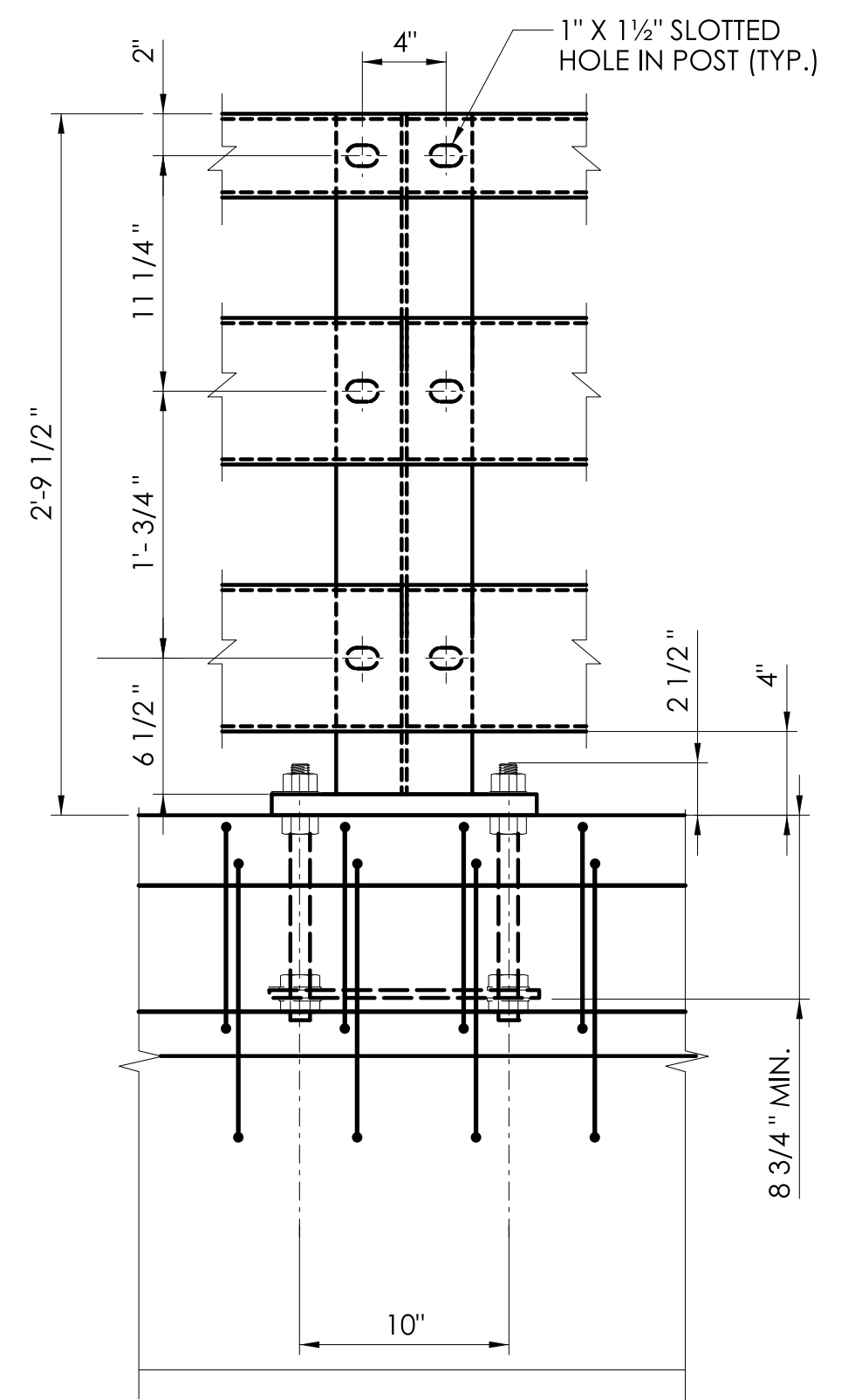
DOME HEAD BOLTS WITH WRENCH SLOTS USED FOR THE TOP RAIL SHALL CONFORM TO ASTM F3125 GRADE A325, TYPE 1 OR ASTM A449, GRADE 1. SUBSTITUTION OF DOME HEAD BOLTS WITH BOLTS MEETING DIFFERENT MATERIAL REQUIREMENTS IS NOT PERMITTED. NUTS SHALL CONFORM TO ASTM A563, GRADE DH. CIRCULAR, FLAT, HARDENED STEEL WASHERS SHALL CONFORM TO ASTM F436. THE BOLTS, NUTS AND WASHERS SHALL BE GALVANIZED IN ACCORDANCE WITH ASTM F2329 OR ASTM B695, CLASS 55.

RAIL ELEMENTS SHALL BE FABRICATED TO THE HORIZONTAL AND VERTICAL ALIGNMENT OF THE STRUCTURE. POSTS SHALL BE INSTALLED NORMAL TO GRADE IN THE LONGITUDINAL DIRECTION AND VERTICAL IN THE TRANSVERSE DIRECTION.

ALL BRIDGE RAIL MATERIALS, INCLUDING ANCHOR PLATES, ANCHOR BOLTS, CONCRETE INSERTS, HARDWARE AND EPOXY GROUT, SHALL BE PAID FOR UNDER THE ITEM "3-TUBE CURB MOUNTED BRIDGE RAIL".



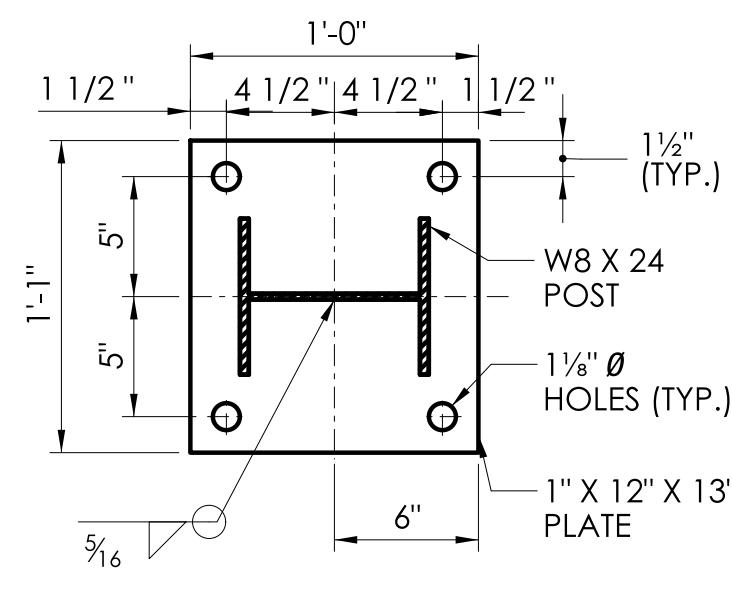
TYPICAL SECTION AT POST



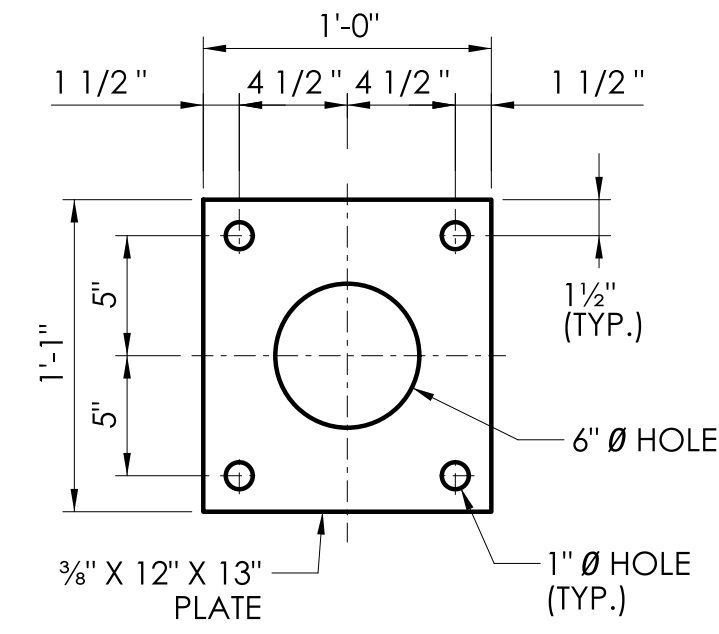
ELEVATION AT POST

CURB AND POST DETAILS
SCALE: 1 1/2" = 1'-0"

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |



BASE PLATE DETAIL
SCALE: 1 1/2" = 1'-0"



ANCHOR PLATE DETAIL
SCALE: 1 1/2" = 1'-0"

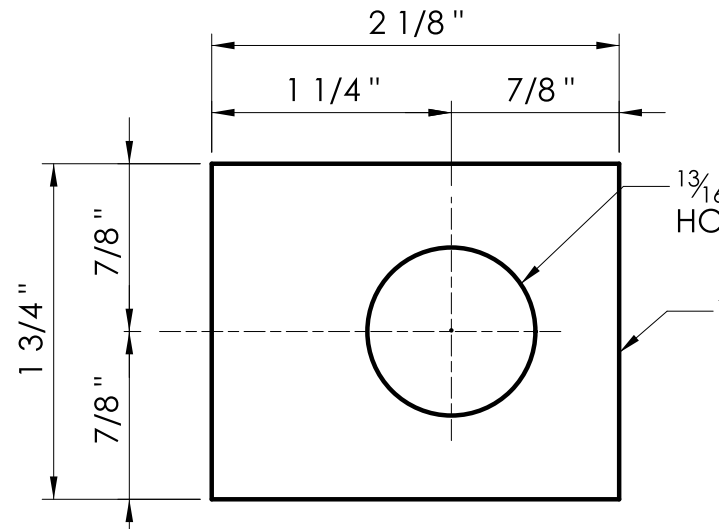
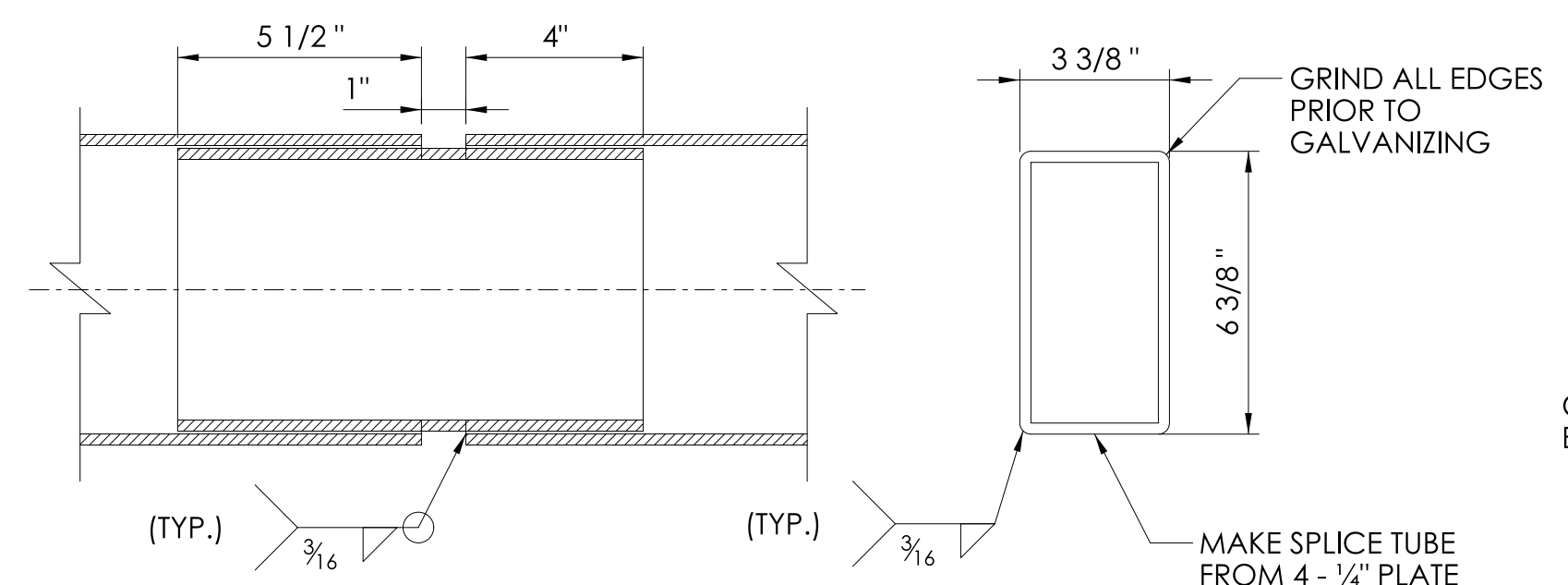
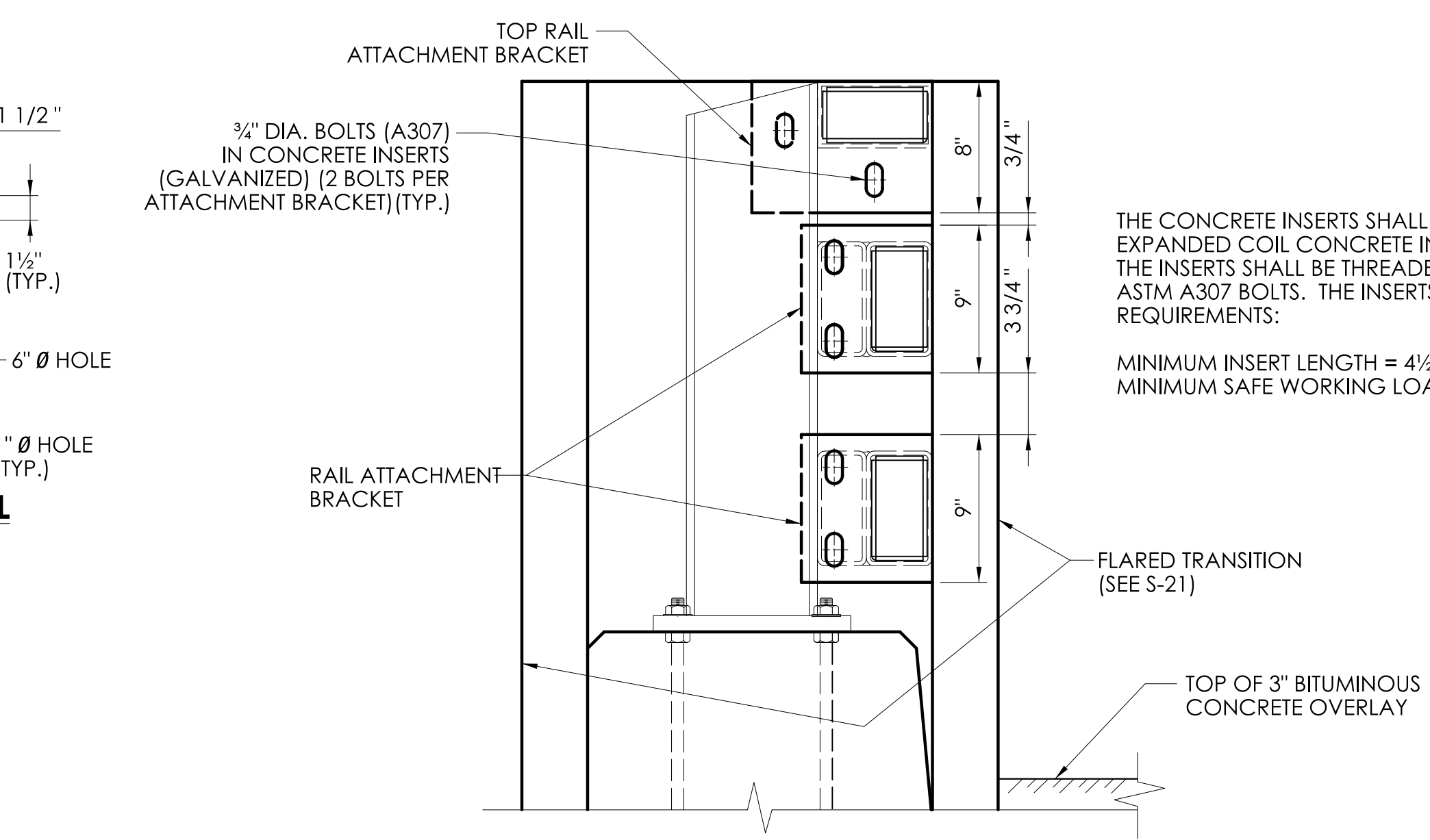


PLATE WASHER "C"
SCALE: N.T.S.

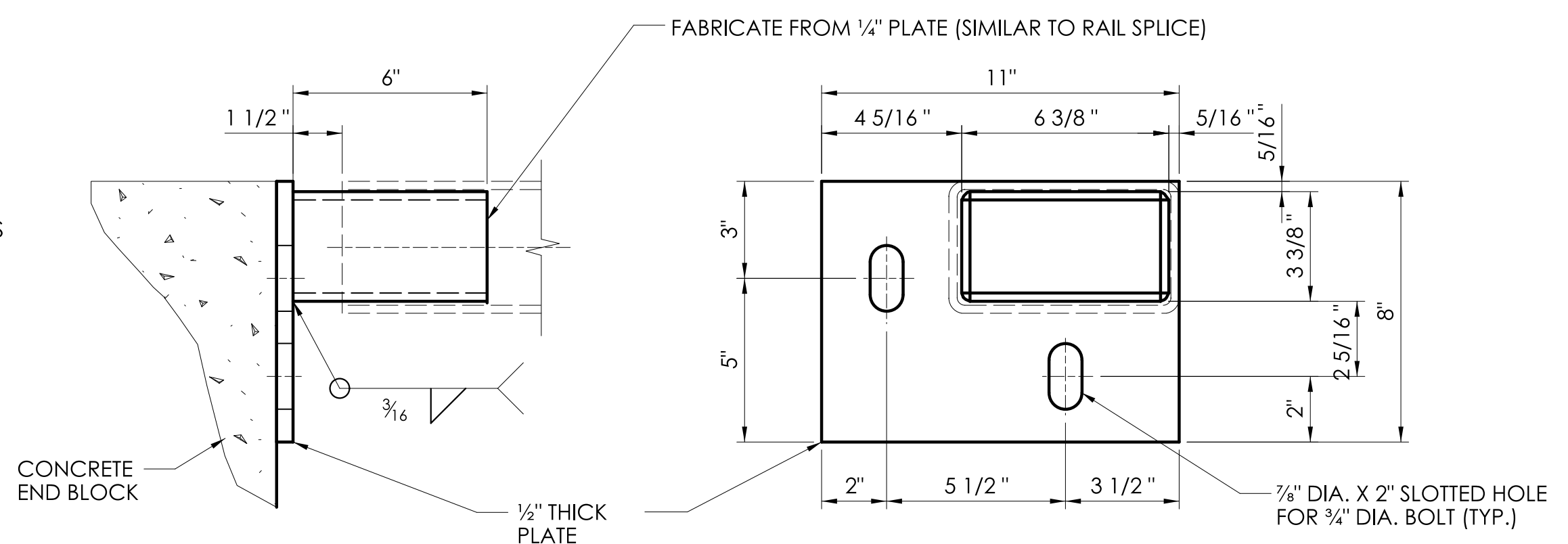


RAIL SPLICE DETAILS
SCALE: 3" = 1'-0"

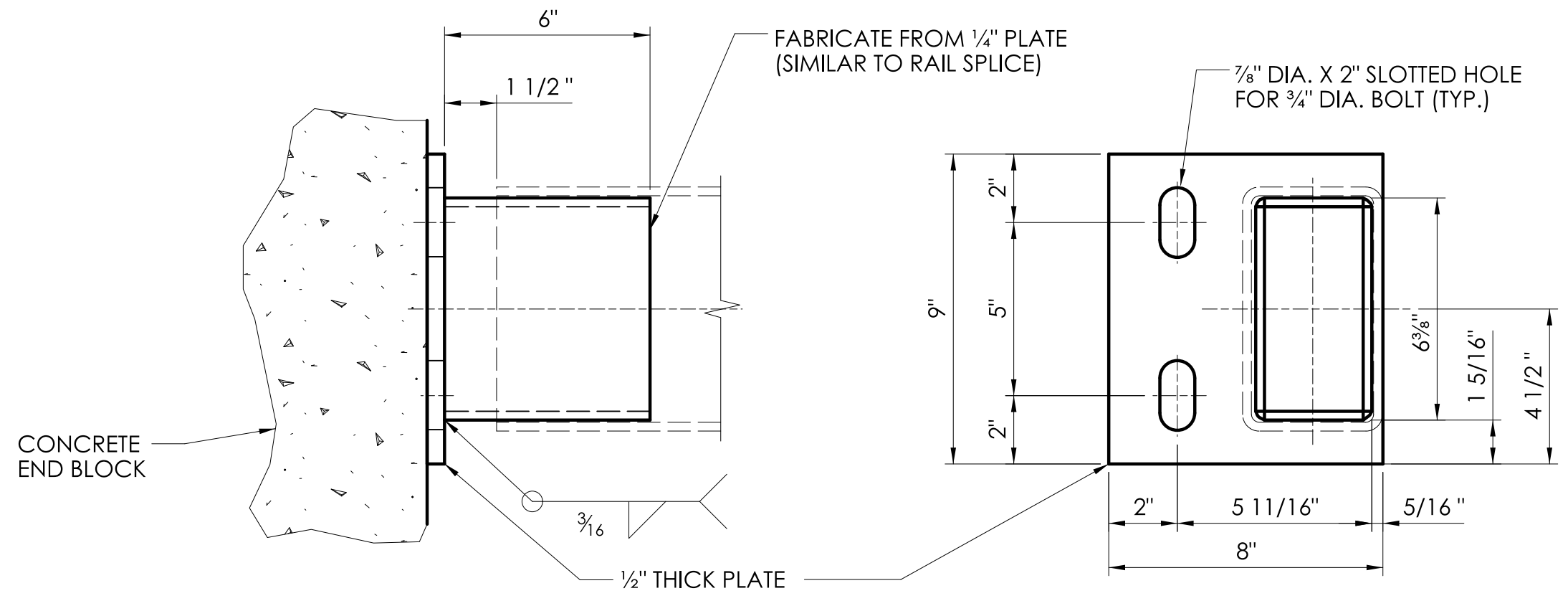


SECTION C
SCALE: 1 1/2" = 1'-0"

THE CONCRETE INSERTS SHALL BE HOT-DIP GALVANIZED, EXPANDED COIL CONCRETE INSERTS WITH A CLOSED-BACK. THE INSERTS SHALL BE THREADED TO RECEIVE 3/4" DIA. ASTM A307 BOLTS. THE INSERTS SHALL MEET THE FOLLOWING REQUIREMENTS:
MINIMUM INSERT LENGTH = 4 1/2"
MINIMUM SAFE WORKING LOAD IN TENSION = 4000 LBS.

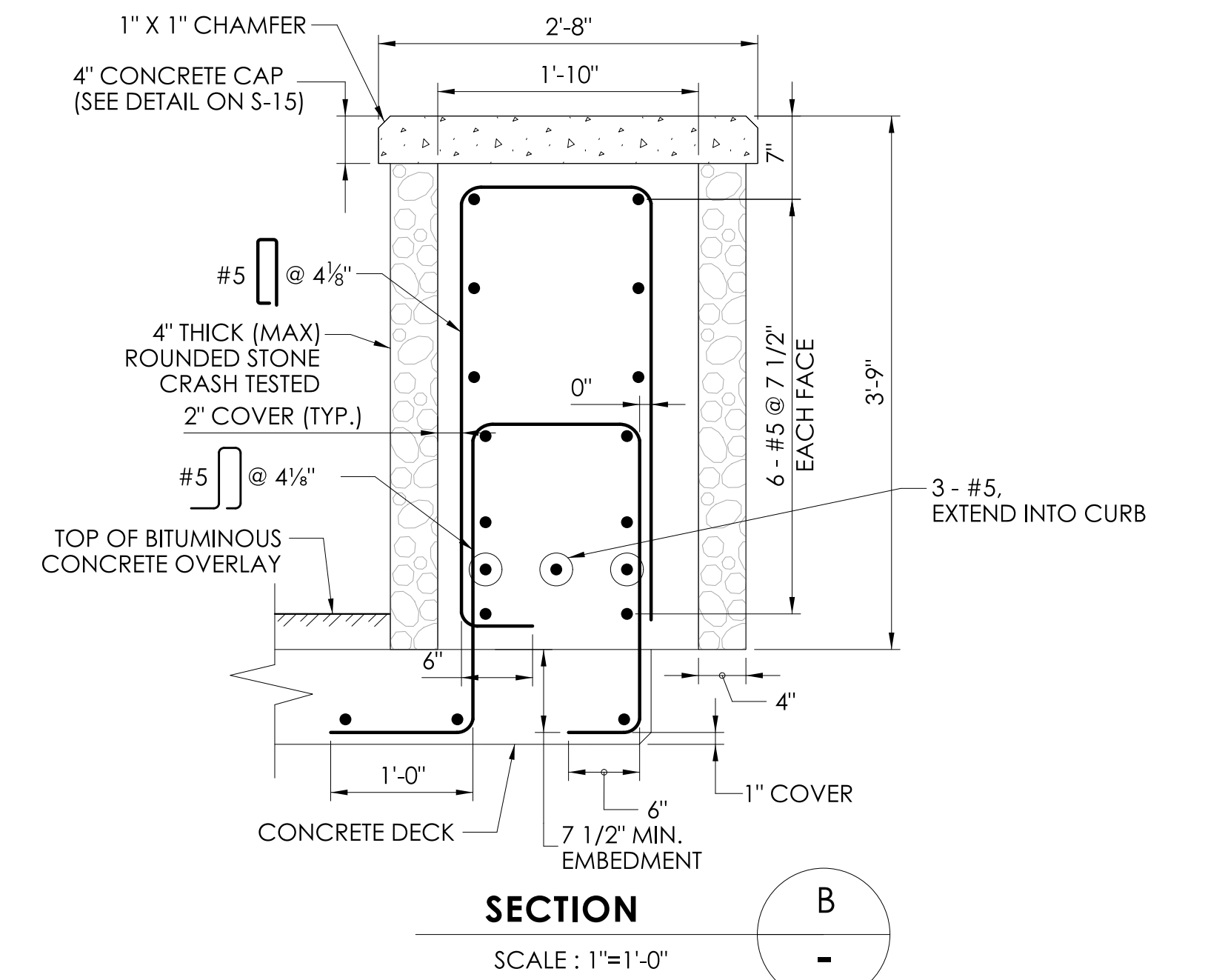
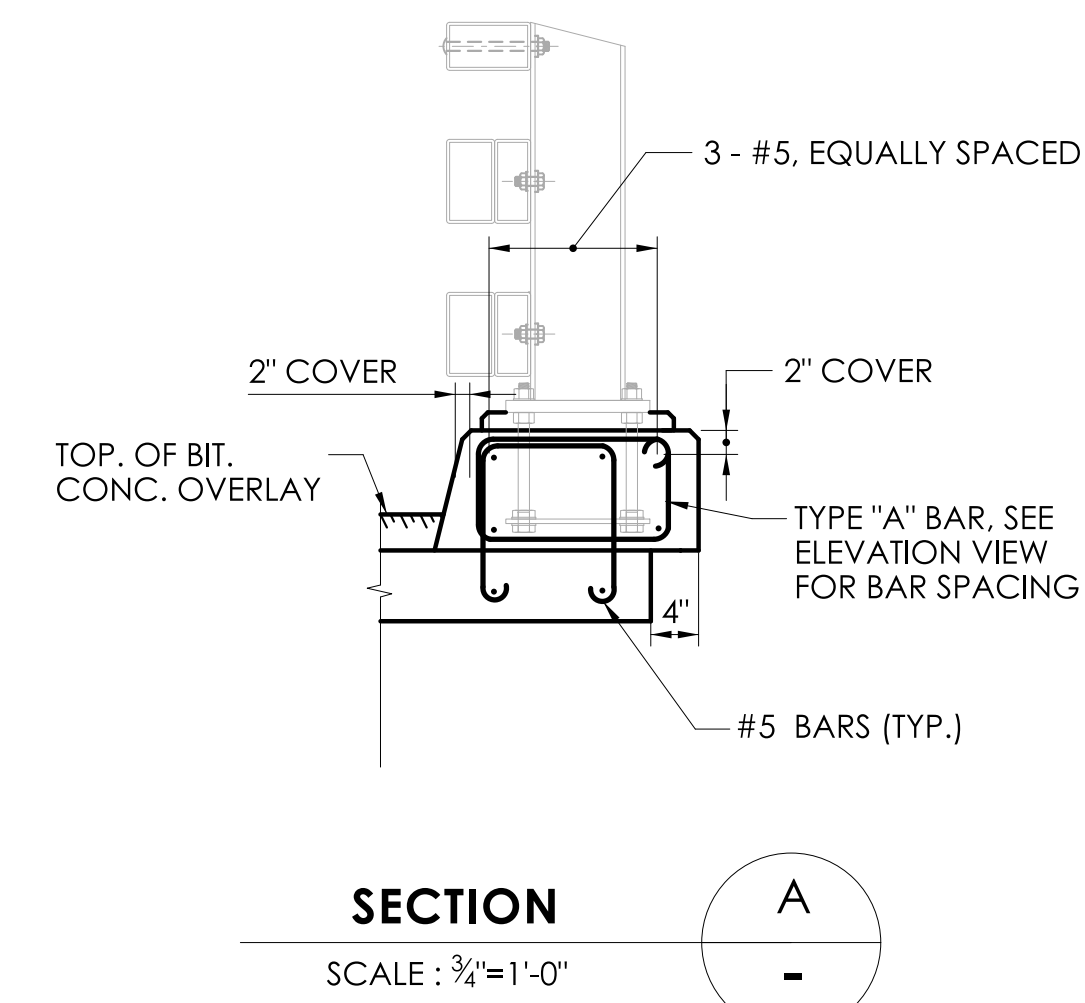
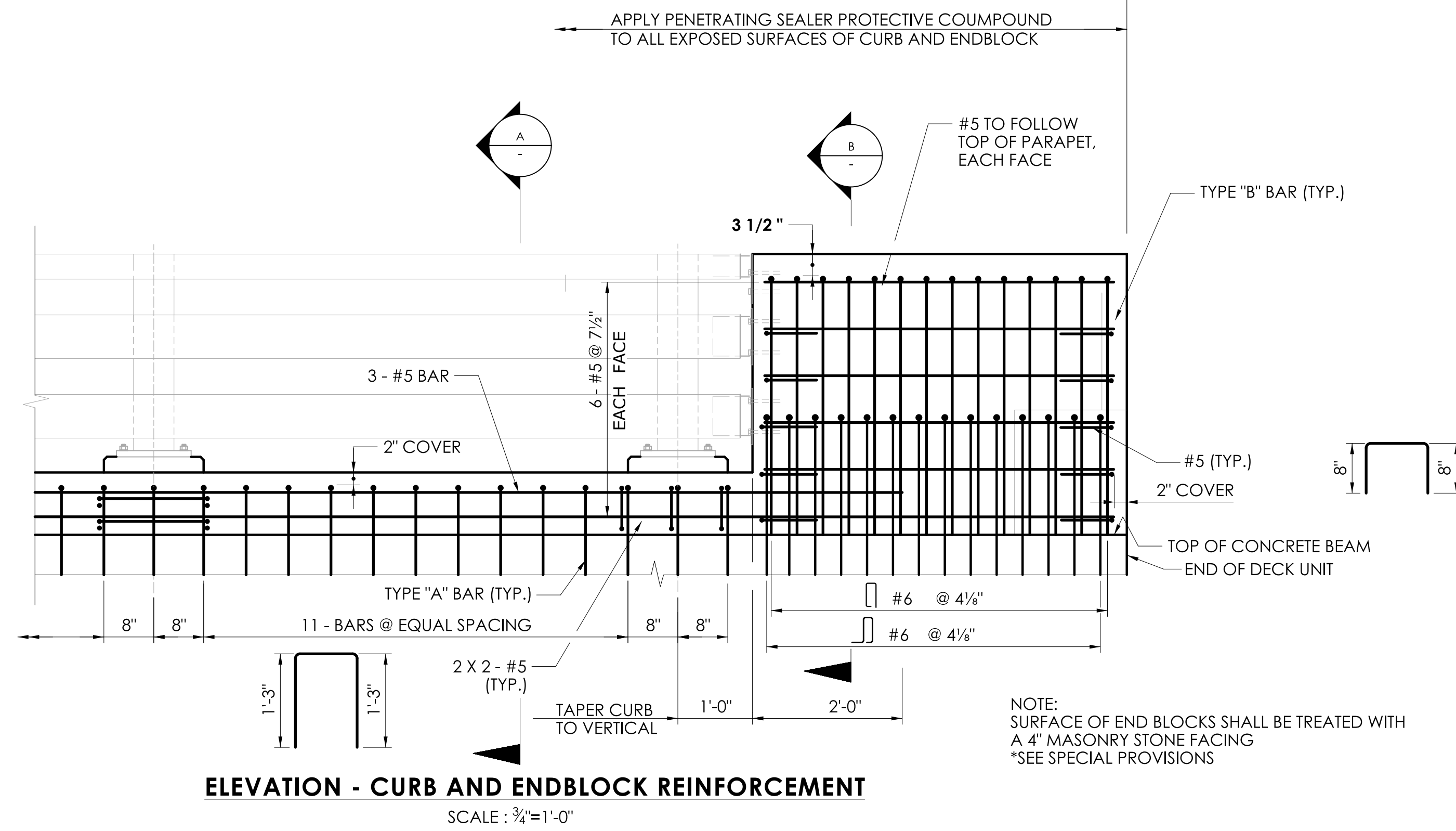


TOP RAIL ATTACHMENT BRACKET
SCALE: 3" = 1'-0"



RAIL ATTACHMENT BRACKET
SCALE: 3" = 1'-0"

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |

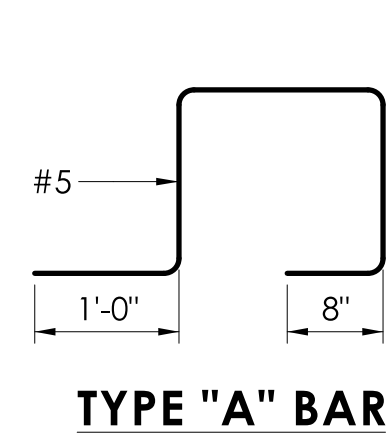


REINFORCEMENT SPLICE NOTES:

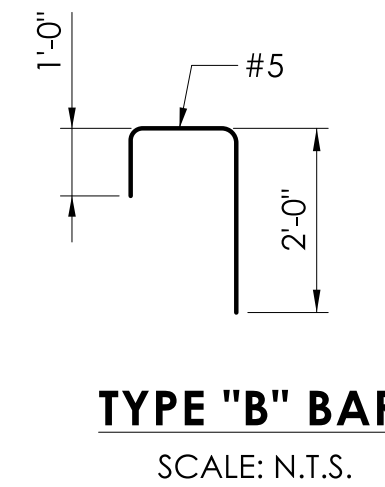
1. THE SPLICE LENGTH FOR THE LONGITUDINAL REINFORCEMENT IN THE CURB AND ENDBLOCK SHALL BE AS FOLLOWS UNLESS DIMENSIONED OTHERWISE:

| BAR SIZE | SPLICE LENGTH |
|----------|---------------|
| #5 | 2'-4" |

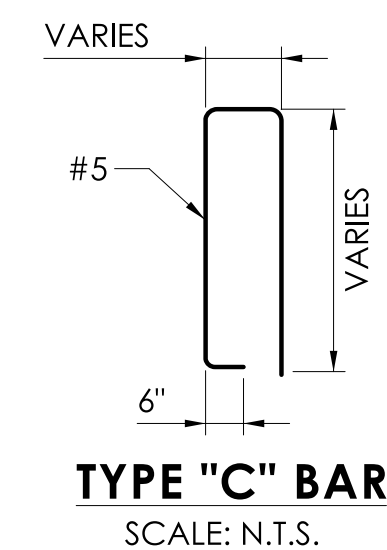
2. THE SPLICES SHALL BE ALTERNATED SO THAT 50% OR LESS OF THE LONGITUDINAL BARS ARE SPLICED AT THE SAME LOCATION.



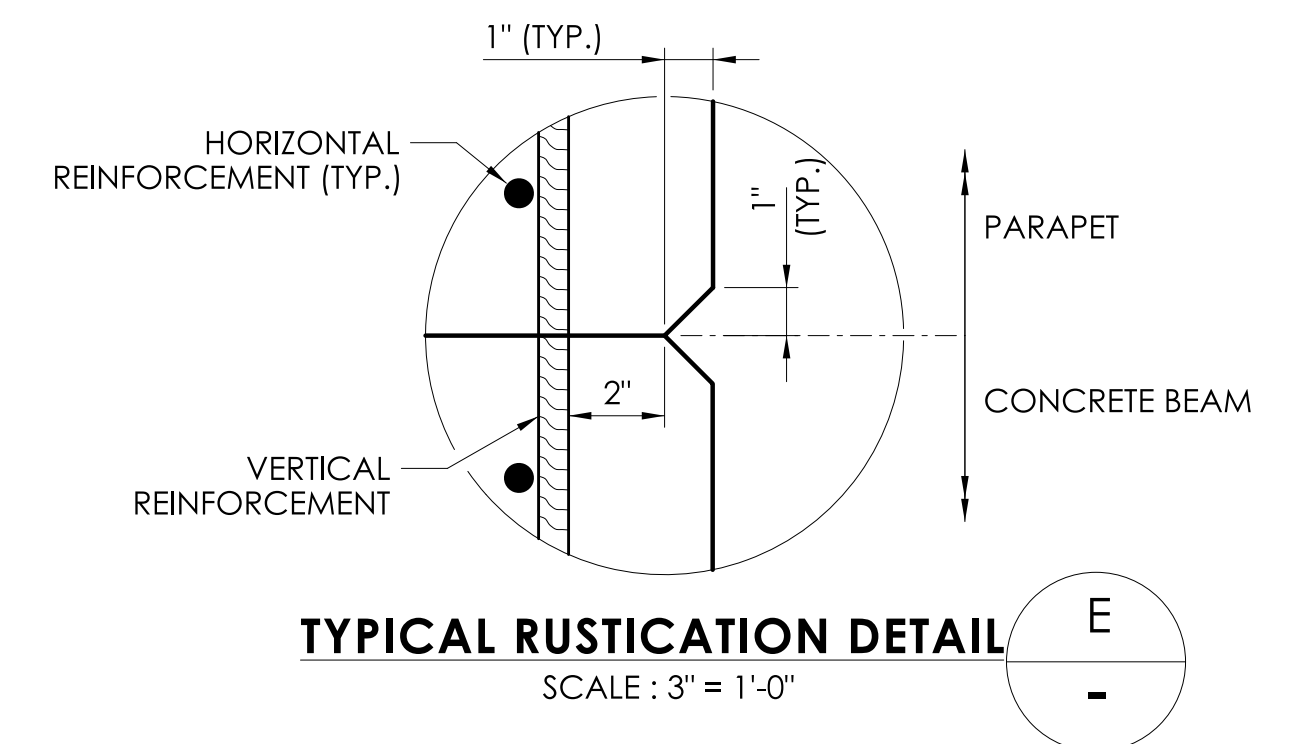
TYPE "A" BAR



TYPE "B" BAR
SCALE: N.T.S.

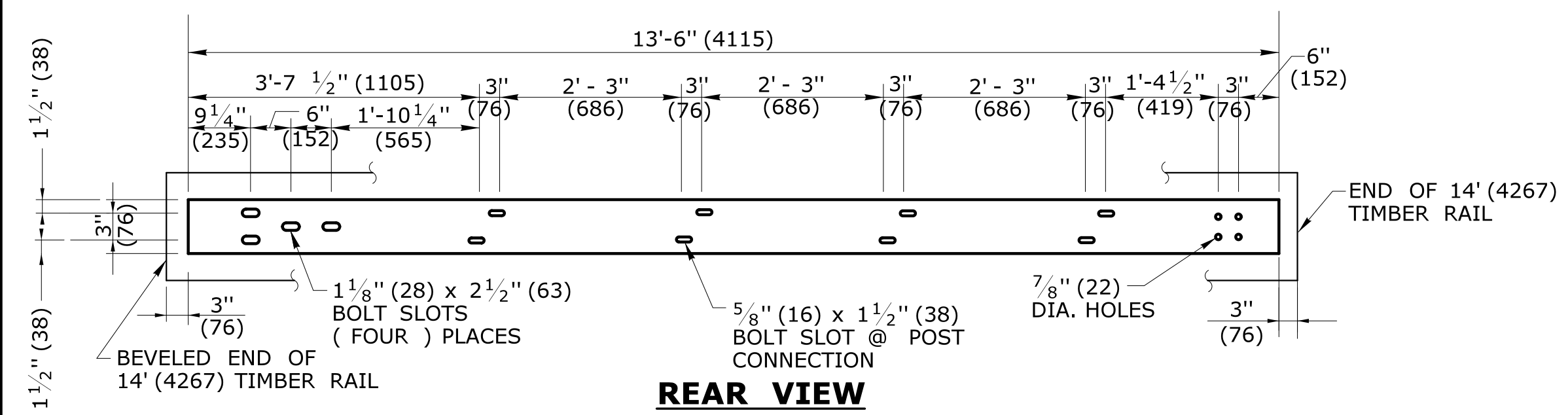


TYPE "C" BAR
SCALE: N.T.S.

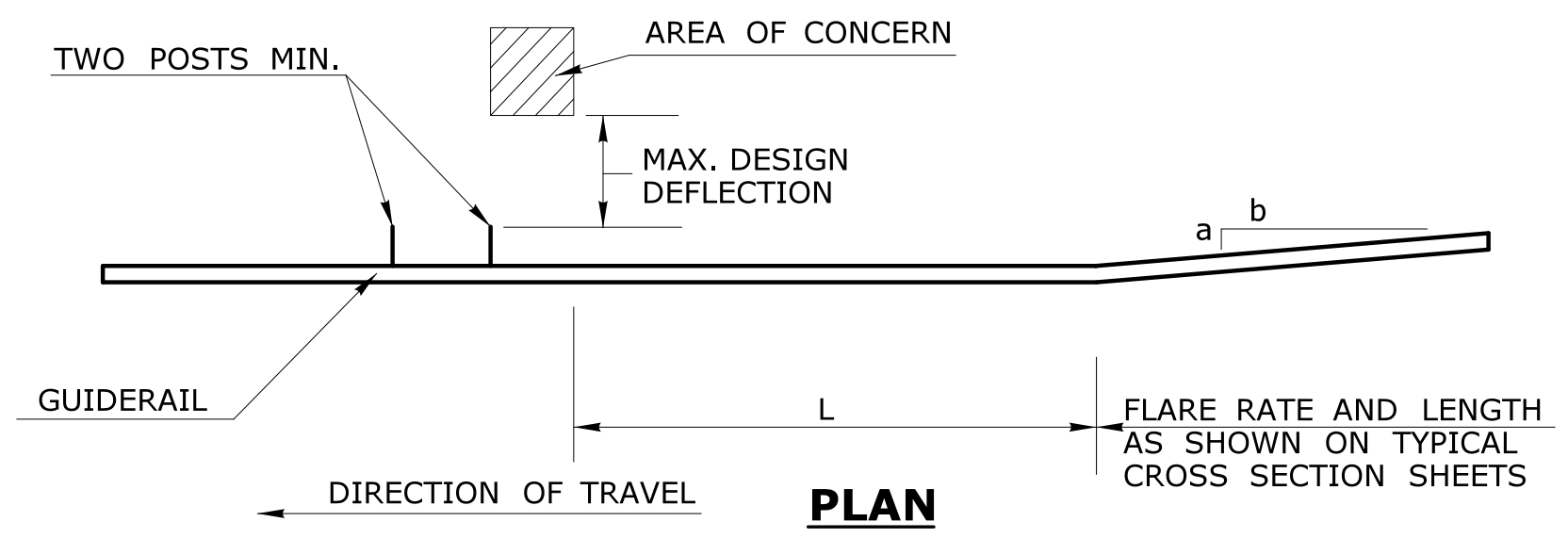


TYPICAL RUSTICATION DETAIL
SCALE: 3" = 1'-0"

| REV. | DATE | REVISION DESCRIPTION |
|------|------|----------------------|
| | | |
| | | |
| | | |
| | | |



STEEL TRANSITION RAIL
6" x 3/8" x 13'-6" (152x9x4115)
FOR LEADING END ATTACHMENT

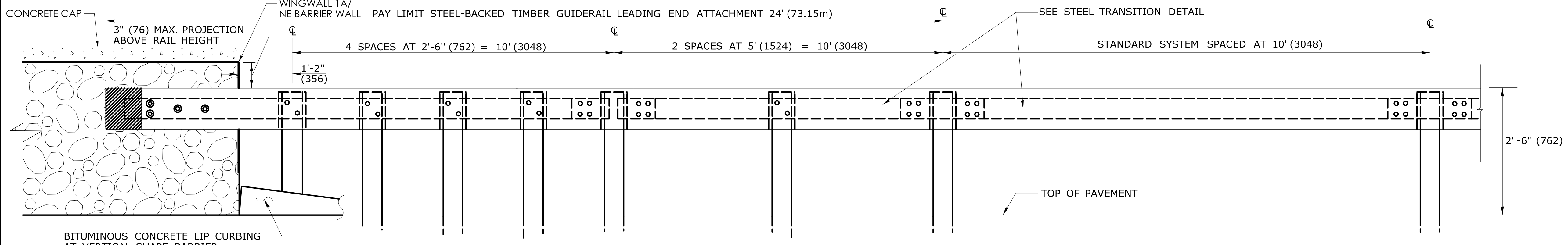
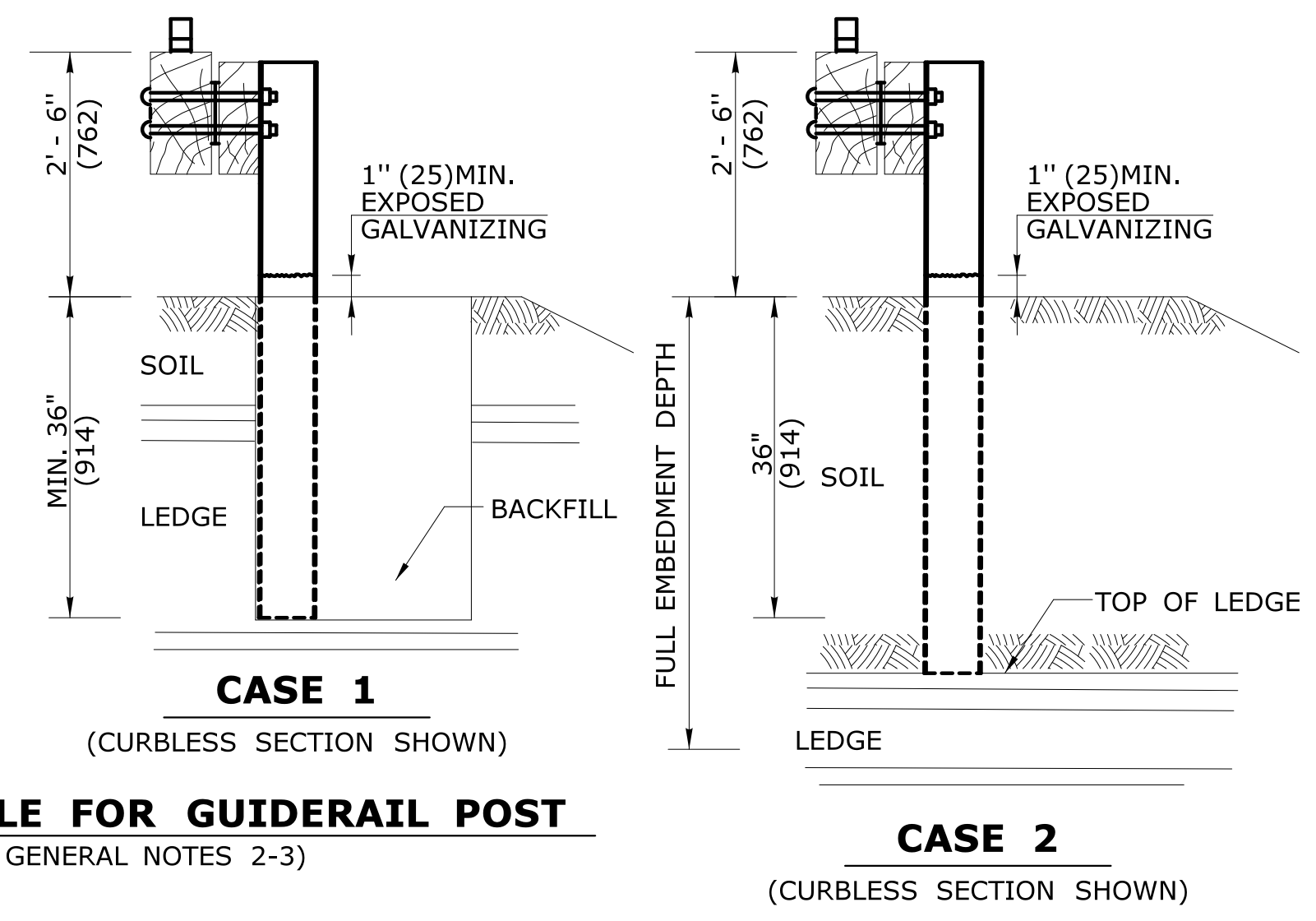
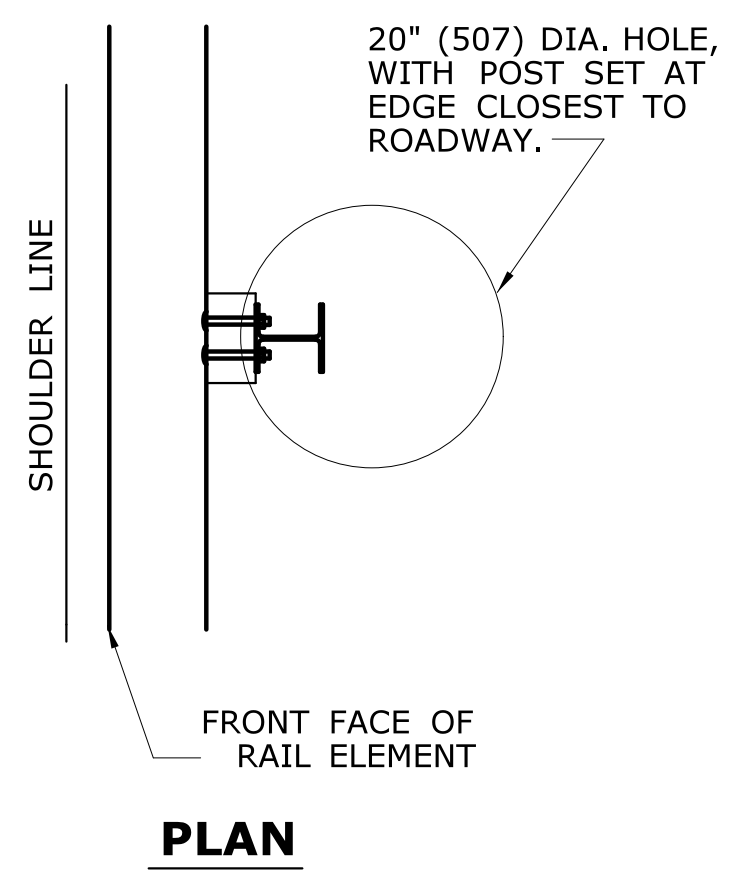
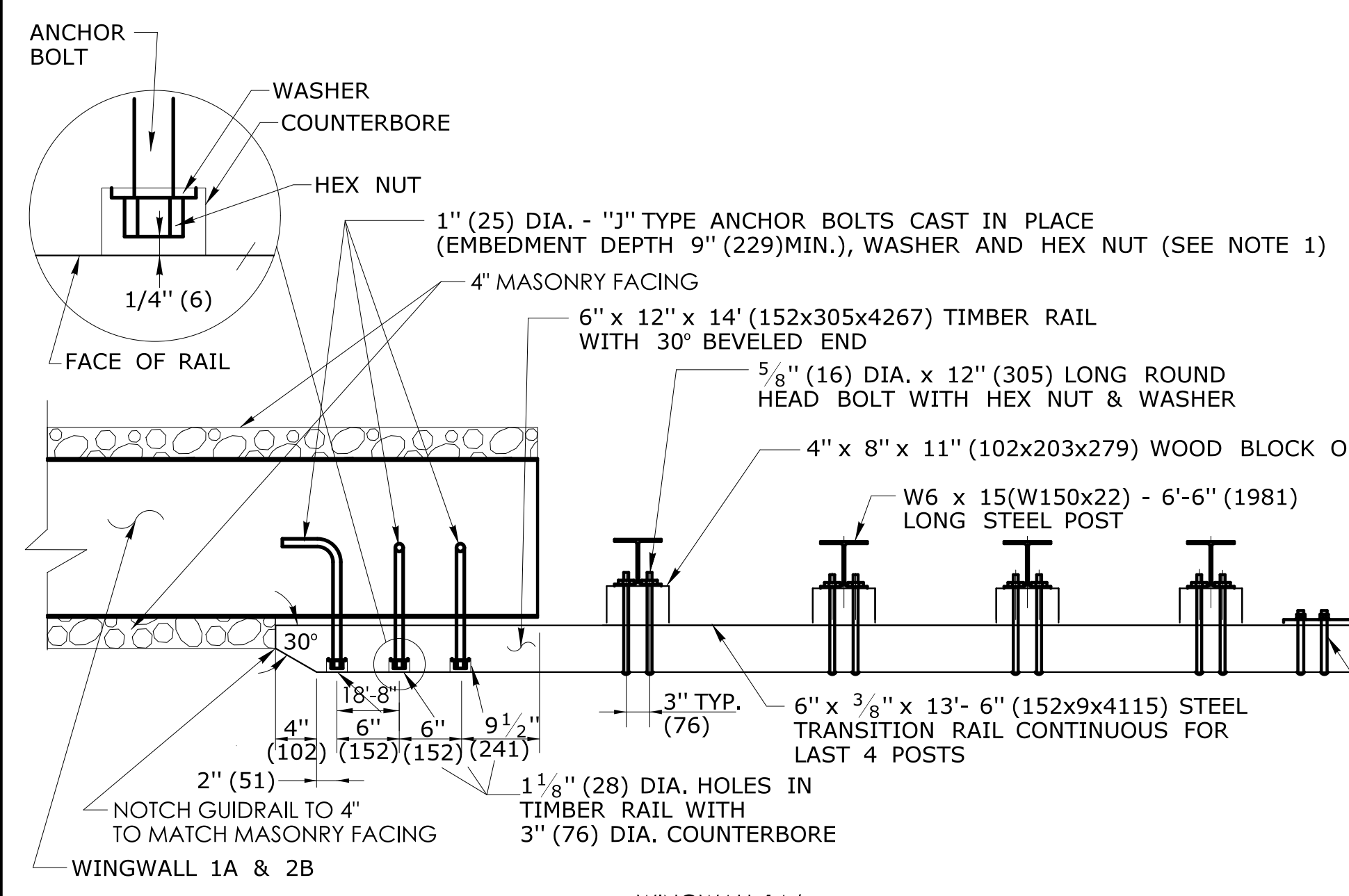


- GENERAL NOTES:**
- THIS DRAWING SHOWS LEADING END ATTACHMENT TO PROPOSED BARRIER/PARAPET. AT EXISTING PARAPETS ATTACH GUIDERRAIL USING D.O.T. APPROVED CHEMICAL ANCHORS.
 - PRIOR TO GUIDERRAIL POST INSTALLATION THE CONTRACTOR SHALL INVESTIGATE POST LOCATIONS FOR THE PRESENCE AND DEPTH OF LEDGE.
 - 20" (507) DIA. EXCAVATED HOLE SHALL BE BACKFILLED WITH SUITABLE MATERIAL, OR GRANULAR FILL COMPACTED IN 6" (150) LIFTS BEFORE DRIVING POST OR POSTS MAY BE SET IN EXCAVATED HOLE AND BACKFILLED WITH CONTROLLED LOW STRENGTH MATERIAL (CLSM).

| Maximum Design Deflection (ft) (mm) | Area Of Concern Plus 2 Posts (see sketch) | SYSTEM 3 | SYSTEM 2 | Standard System | L |
|-------------------------------------|---|---|---|--|--------------------|
| | | W6x15 (W150x22) Posts Spaced at 2'-6" (762) | W6x15 (W150x22) Posts Spaced at 5' (1524) | W6x15 (W150x22) Posts Spaced at 10' (3048) | Min. Length Needed |
| 1' (305) | System 3 | 10' (3048) | 10' (3048) | 50' (16.40m) | 70' (21.34m) |
| 2'-6" (762) | System 2 | - | 10' (3048) | 60' (19.68m) | 70' (21.34m) |
| 4' (1219) | Standard System | - | - | 70' (21.34m) | 70' (21.34m) |

STEEL-BACKED TIMBER GUIDERRAIL
DESIGN DEFLECTION CHART

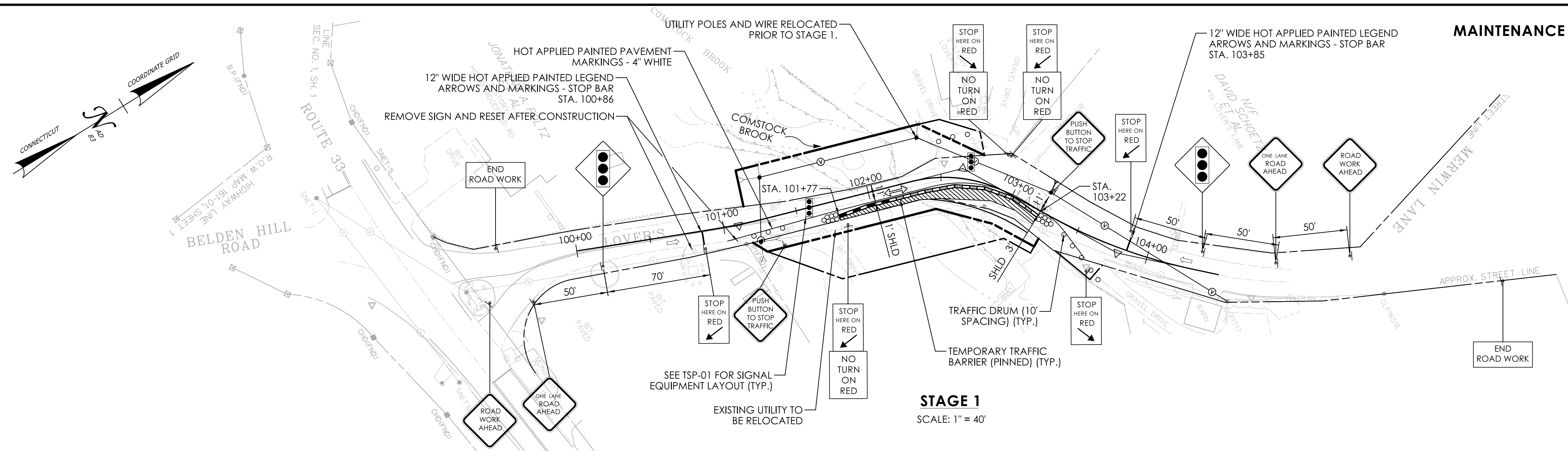
- CASE 1:** IF LEDGE IS LOCATED WITHIN 36" (914) OF THE FINISHED GRADE AT THE POST LOCATION A 20" (507) DIAMETER HOLE SHALL BE DRILLED IN THE LEDGE TO THE MINIMUM EMBEDMENT DEPTH OF 36" (914) AND THE POST SHALL BE CUT AND DRIVEN TO ACHIEVE THIS DEPTH.
- CASE 2:** IF LEDGE IS LOCATED AT A DEPTH GREATER THAN 36" (914) BUT LESS THAN THE FULL EMBEDMENT DEPTH THE POST SHALL BE CUT AND DRIVEN TO ACHIEVE EMBEDMENT TO THE DEPTH OF THE TOP OF LEDGE.



| | | | | |
|---|----------------------|--|--|-------------------------------|
| REVISION DESCRIPTION REV. DATE DESIGNER/DRAFTER: JT CHECKED BY: DW LASTED SAVED BY: J1for FILE NAME: J:\DWG\2021\6\0182\210\0161-0142\Bridg\Contract_Plans\24_Sb_MSH_BR04975_0161_0142_CTDOT Standard.dgn PLOTTED DATE: 10/21/2022 | SIGNATURE/ BLOCK: | STATE OF CONNECTICUT TOWN OF WILTON | PROJECT NUMBER: 0161-0142 PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVERS LANE OVER COMSTOCK BROOK TOWN(S): WILTON DRAWING TITLE: STEEL BACKED GUIDERRAIL BRIDGE END ATTACHMENT | DRAWING NO. S-24 SHEET NO. |
|---|----------------------|--|--|-------------------------------|

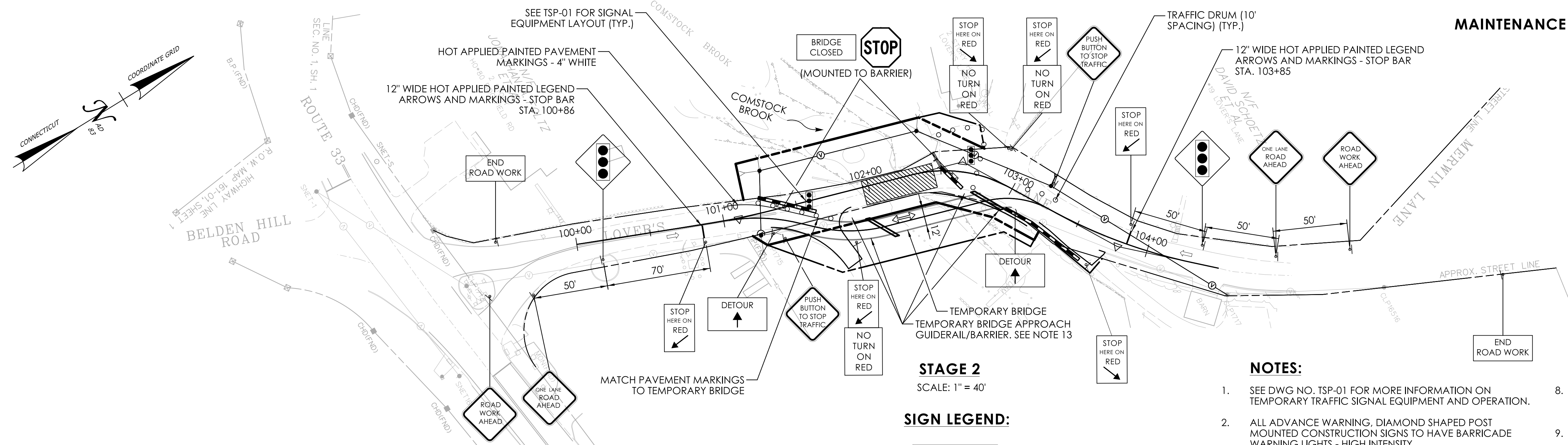
ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

MAINTENANCE AND PROTECTION OF TRAFFIC STAGE 1



STAGE 1
SCALE: 1" = 40'

MAINTENANCE AND PROTECTION OF TRAFFIC STAGE 2



STAGE 2
SCALE: 1" = 40'

NOTES:

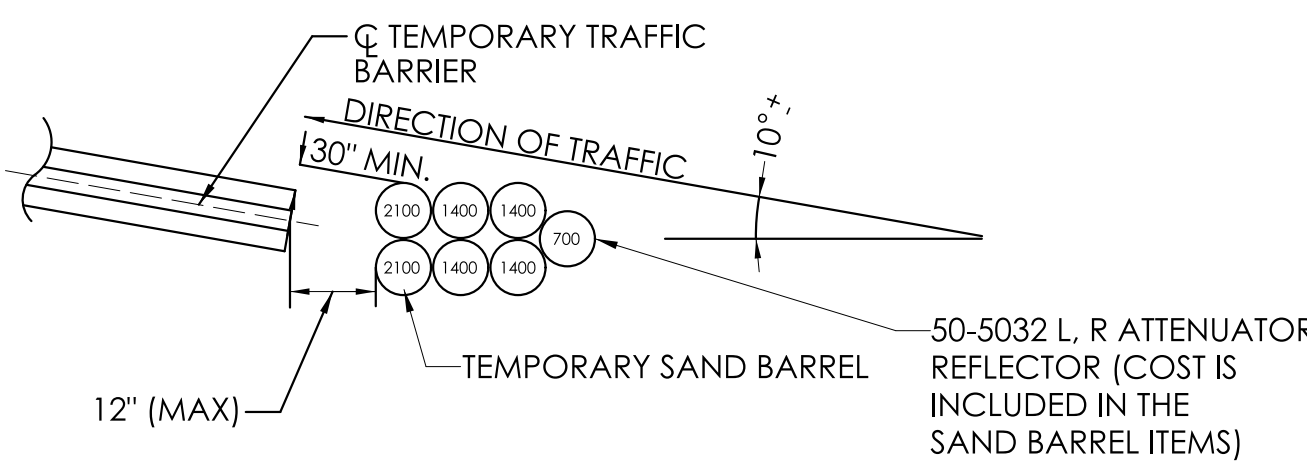
- SEE DWG NO. TSP-01 FOR MORE INFORMATION ON TEMPORARY TRAFFIC SIGNAL EQUIPMENT AND OPERATION.
- ALL ADVANCE WARNING, DIAMOND SHAPED POST MOUNTED CONSTRUCTION SIGNS TO HAVE BARRICADE WARNING LIGHTS - HIGH INTENSITY.
- LOCATIONS OF TEMPORARY SIGNS ARE APPROXIMATE AND SHALL BE ADJUSTED AS DIRECTED BY THE ENGINEER. TEMPORARY SIGNS SHALL BE POST MOUNTED IF POSSIBLE. EXISTING SIGNS IN CONFLICT WITH THE MP&T PLAN SHALL BE COVERED.
- EXISTING SIGNS ARE TO BE RELOCATED AS NEEDED AND AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE PAID UNDER ITEM #0971001A - MAINTENANCE AND PROTECTION OF TRAFFIC.
- ALL SIGNS TO BE PAID FOR UNDER ITEM #1220027 CONSTRUCTION SIGNS.
- DIMENSIONS SHOWN FOR PAVEMENT MARKINGS ARE APPROXIMATE AND SHALL BE USED AS A GUIDE ONLY. THE ACTUAL DIMENSIONS SHALL BE MODIFIED AS DIRECTED BY THE ENGINEER AS NECESSARY TO PROVIDE A SMOOTH TRANSITION IN TRAVEL DIRECTION.
- TEMPORARY PAVEMENT MARKINGS LOCATED BETWEEN THE TEMPORARY STOP BARS SHALL BE REMOVED AFTER EACH STAGE.
- TEMPORARY TRAFFIC BARRIER TO HAVE TYPE DE-7D. DELINEATORS INSTALLED PER TYPICAL DELINEATION DETAILS (STANDARD DWG. NO. TR-1205-01).
- TEMPORARY SAND BARRELS TO HAVE ATTENUATOR REFLECTOR INSTALLED ON FIRST MODULE.
- INSTALL SIGN 31-1906 WITH 80-9916 SUBMOUNTED ON EACH APPROACH OF ROUTE 33 APPROXIMATELY 100 FT FROM LOVERS LANE. INSTALL SIGN 31-1906 WITH 80-9916 SUBMOUNTED ON MERWIN LANE APPROXIMATELY 50 FEET FROM LOVERS LANE.
- INSTALL SIGN 80-1613 WITH 80-9916 SUBMOUNTED ON EACH APPROACH OF ROUTE 33 APPROXIMATELY 200 FT FROM LOVERS LANE. INSTALL SIGN 80-1613 WITH 80-9916 SUBMOUNTED ON MERWIN LANE APPROXIMATELY 100 FEET FROM LOVERS LANE.
- THE TEMPORARY SAND BARRELS SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURERS REQUIREMENTS.
- THE APPROACHES TO THE TEMPORARY BRIDGE WILL REQUIRE GUIDERAIL/BARRIER PROTECTION. THE DESIGN, CONSTRUCTION AND REMOVAL OF THIS APPROACH GUIDERAIL IS INCLUDED IN THE COST FOR THE TEMPORARY BRIDGE. SEE SPECIAL PROVISIONS.

SIGN LEGEND:

| | | | | | | | | |
|---------|--|---------|---------|--|---------|---------|------------------|---------|
| 80-9603 | ROAD WORK AHEAD | 36"X36" | 80-1613 | CONSTRUCTION AHEAD ROAD USE RESTRICTED STATE LIABILITY LIMITED (SEE NOTE #11) | 30"X24" | 80-9612 | END ROAD WORK | 48"X24" |
| 80-9834 | ONE LANE ROAD AHEAD | 36"X36" | 80-9052 | TRAFFIC SIGNAL | 36"X36" | 80-9082 | BRIDGE CLOSED | 48"X30" |
| 80-9933 | PUSH BUTTON TO STOP TRAFFIC | 36"X36" | 31-0552 | STOP | 30"X30" | 80-9710 | DETOUR | 30"X24" |
| 31-1906 | ROAD WORK AHEAD FINES DOUBLED (SEE NOTE #10) | 48"X42" | 80-9916 | LOVERS LN | 48"X10" | 31-0830 | NO TURN ON RED | 30"X36" |
| | | | 31-0801 | STOP HERE ON RED | 24"X36" | 31-0802 | STOP HERE ON RED | 24"X36" |

LEGEND:

- TEMPORARY TRAFFIC BARRIER (PINNED)
- TEMPORARY TRAFFIC BARRIER
- TEMPORARY SAND BARRELS
- CONSTRUCTION SIGN
- TRAFFIC DRUM
- CONSTRUCTION BARRICADE - TYPE 3
- AREA OF CONSTRUCTION
- ALTERNATING ONE-WAY OPERATION
- TEMPORARY TRAFFIC SIGNAL INDICATIONS



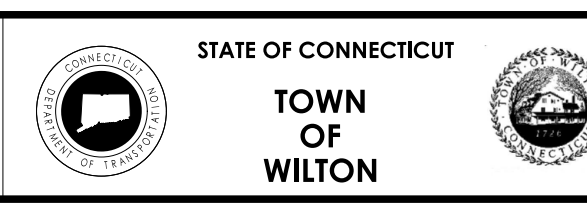
TEMPORARY SAND BARRELS

| | |
|----------------------|------|
| REVISION DESCRIPTION | DATE |
| | |
| | |
| | |
| | |

DESIGNER/DRAFTER: T. LE CHECKED BY: J. BABOWICZ

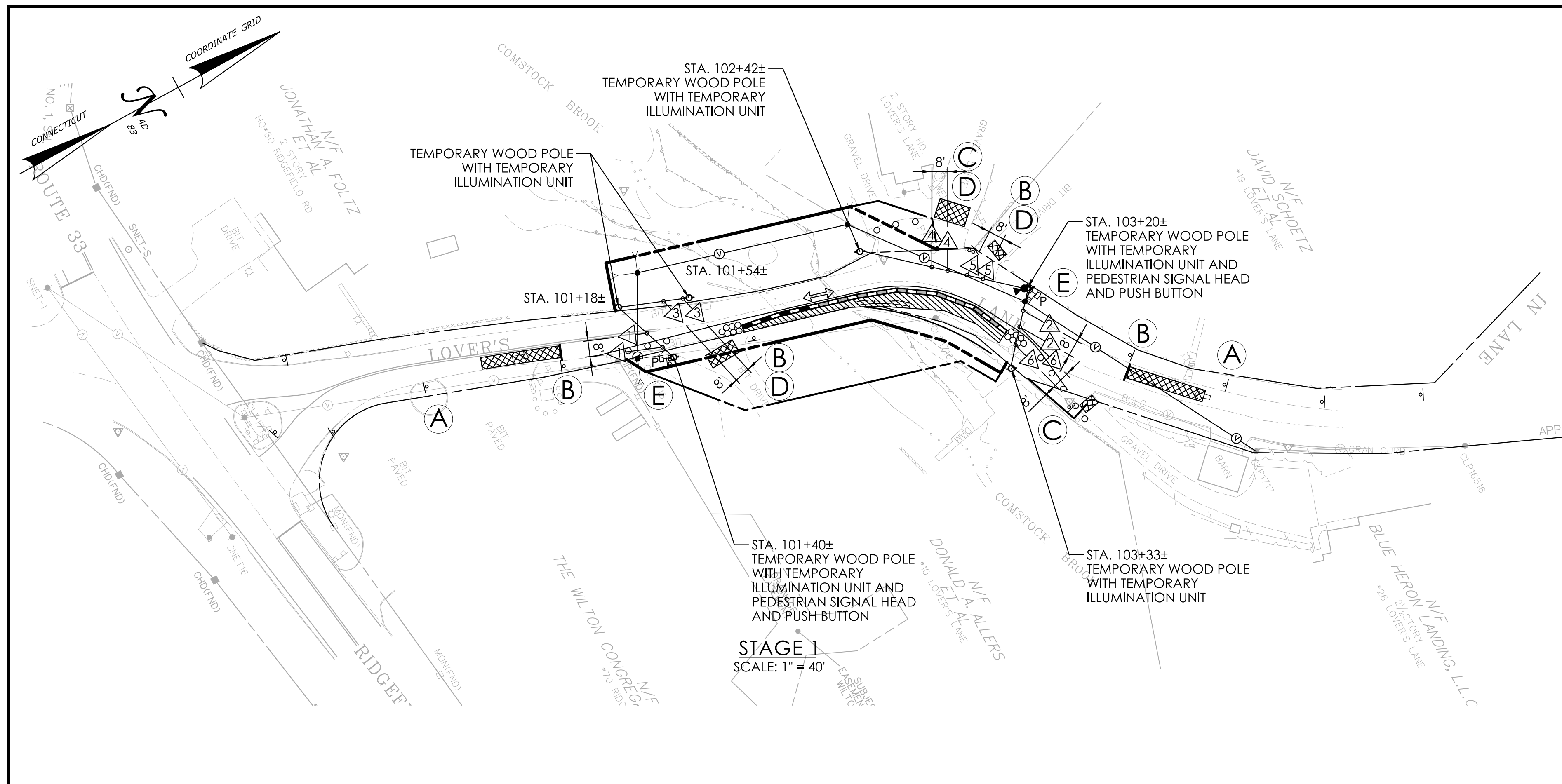
LASTED SAVED BY: TLE FILE NAME: J:\DWG\2016\0182\10161-0142\Traffic\Contract_Plans\SB_MSH_Br04976_0161_0142_MPT-01.dgn
PLOTTED DATE: 10/21/2022

SCALE AS NOTED

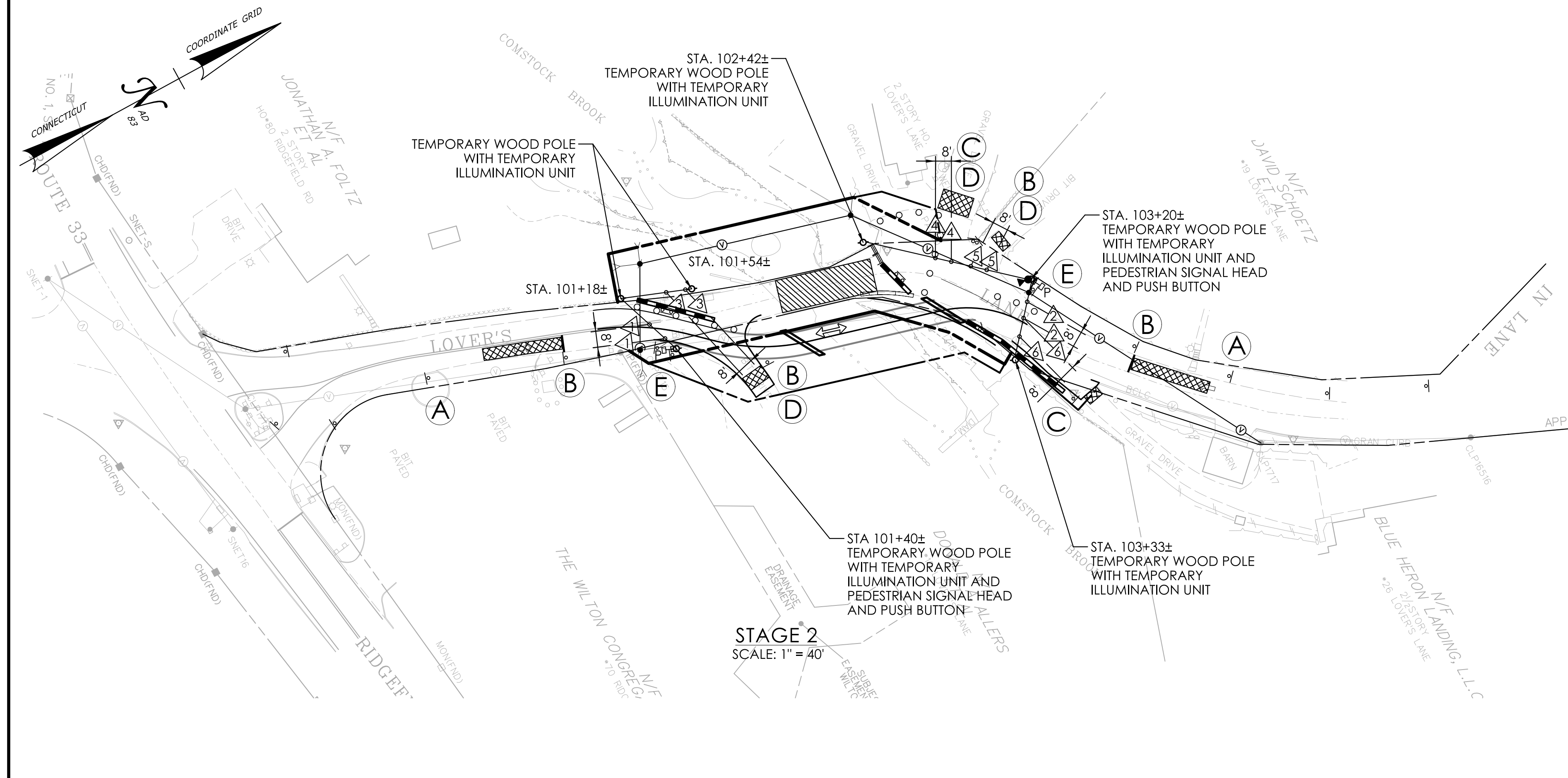


PROJECT NUMBER: 0161-0142
PROJECT DESCRIPTION: REPLACEMENT OF BRIDGE NO. 04975 LOVERS LANE OVER COMSTOCK BROOK
TOWN(S): WILTON
DRAWING TITLE: MAINTENANCE AND PROTECTION OF TRAFFIC

DRAWING NO. MPT-01
SHEET NO.



STAGE 1
SCALE: 1" = 40'

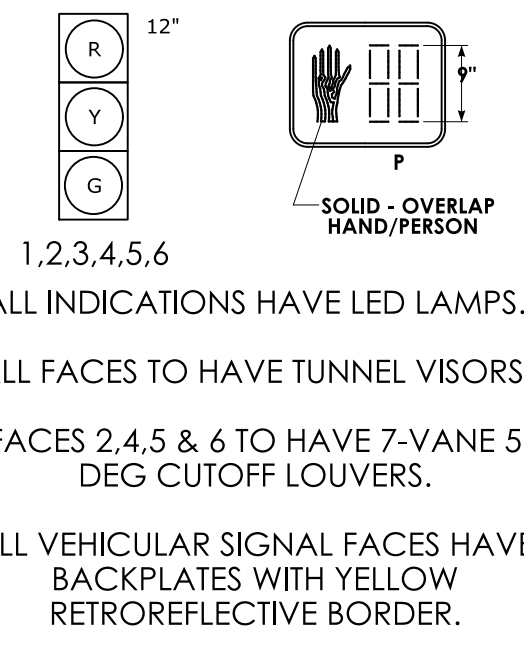


STAGE 2
SCALE: 1" = 40'

MOVEMENT DIAGRAM

| NTOR | PHASE 1 | | | PHASE 2 | | | PHASE 3 | | | PHASE 4 | | | PHASE 5 | | | PHASE 6 | | |
|------------|------------------------|-----|------|----------|-----|------|----------|-----|------|----------|-----|------|----------|-----|------|---------|-----|------|
| FLASH | GRN | CL | CL | GRN | CL | CL | GRN | CL | CL | GRN | CL | CL | GRN | CL | CL | GRN | CL | CL |
| 1 | R | G | Y | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R |
| 2 | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R |
| 3 | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R |
| 4 | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R |
| 5 | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R |
| 6 | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R |
| P | OFF | | | | | | | | | DW | | | | | | | | |
| MIN GRN | 15 | | | 15 | | | 5 | | | 5 | | | 5 | | | 5 | | |
| WALK | | | | | | | | | | | | | | | | | | |
| VEH EXT | 2 | | | 2 | | | 1 | | | 2 | | | 1 | | | 1 | | |
| MAX 1 | 20 | | | 20 | | | 6 | | | 20 | | | 6 | | | 6 | | |
| MAX 2 | 20 | | | 20 | | | 6 | | | 20 | | | 6 | | | 6 | | |
| YELLOW | | 3.0 | | | 3.0 | | | 3.0 | | | 3.0 | | | 3.0 | | | 3.0 | |
| RED | | | 13.0 | | | 13.0 | | | 11.0 | | | 10.0 | | | 10.0 | | | 13.0 |
| ADD INIT | | | | | | | | | | | | | | | | | | |
| MAX INIT | | | | | | | | | | | | | | | | | | |
| TBR | | | | | | | | | | | | | | | | | | |
| TTR | | | | | | | | | | | | | | | | | | |
| MIN GAP | | | | | | | | | | | | | | | | | | |
| MODE | MIN. RECALL THIS PHASE | | | NON-LOCK | | | NON-LOCK | | | NON-LOCK | | | NON-LOCK | | | | | |
| INIT START | | | | | | | | | | | | | | | | | | |

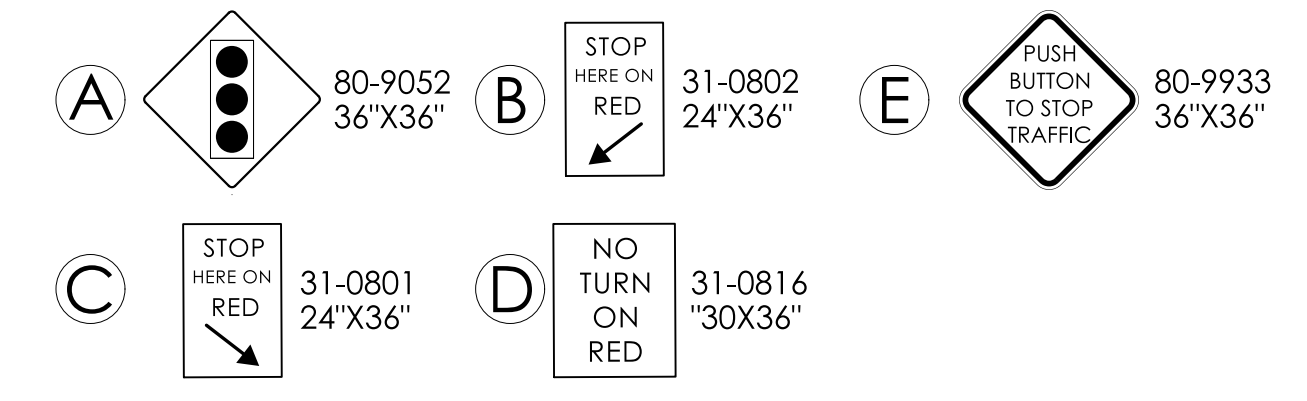
SIGNAL FACES



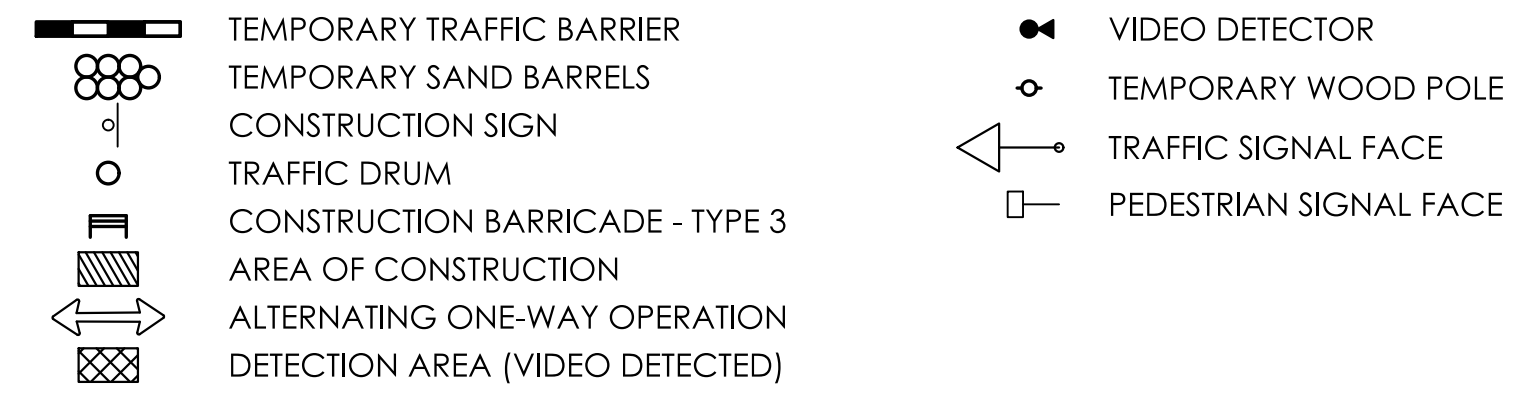
GENERAL NOTES:


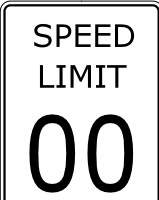

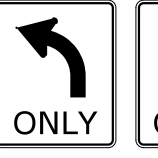



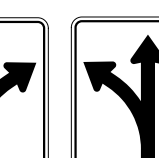




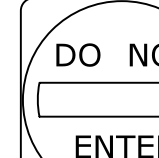
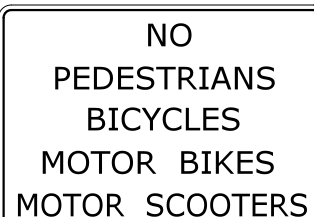
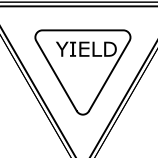
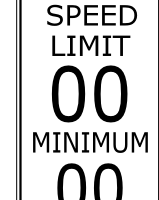

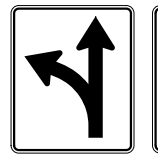
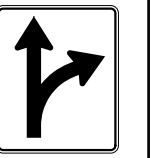
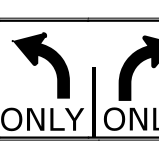
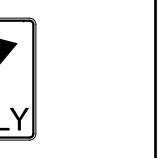

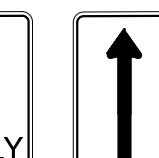

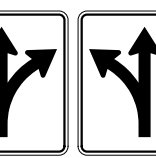




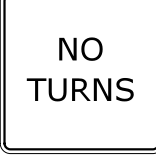
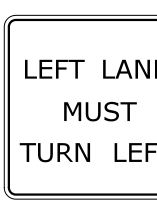


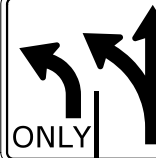
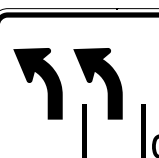

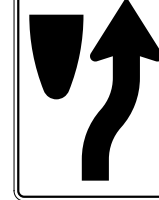


- SIGNS SHALL BE POST MOUNTED.
- THE CONTRACTOR SHALL COVER OR REMOVE CONFLICTING SIGNS AND PAVEMENT MARKINGS.
- SEE SPECIAL PROVISION NO. 1118101A - TEMPORARY SIGNALIZATION FOR ADDITIONAL INFORMATION.
- REFER TO THE MAINTENANCE AND PROTECTION OF TRAFFIC PLANS (MPT-01) FOR ADDITIONAL SIGNS AND PAVEMENT MARKINGS.
- TEMPORARY STOP BARS SHALL BE PAID FOR UNDER ITEM # 1209131 - HOT APPLIED PAINTED LEGEND, ARROWS AND MARKINGS.
- ADJUST VIDEO DETECTION ZONES AS NEEDED WHEN SETTING UP STAGE 2.
- COUNTDOWN ONLY DURING FLASHING PEDESTRIAN CHANGE INTERVAL.
- THE CONTRACTOR SHALL COORDINATE WITH UTILITIES TO ENSURE THAT WIRES FROM TEMPORARY SIGNALIZATION HAVE ADEQUATE CLEARANCE FROM THE PERMANENT UTILITY WIRES.
- VIDEO DETECTION BY USE OF VIDEO360 CAMERA. VIDEO DETECTION LOCATION IS FOR ILLUSTRATION ONLY. EXACT LOCATION SHALL BE DETERMINED BY THE MANUFACTURER OR THEIR DESIGNATED REP.


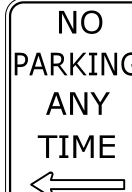


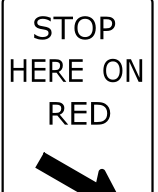


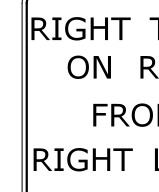
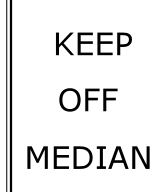

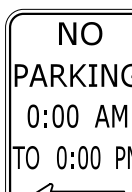


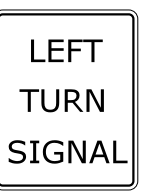
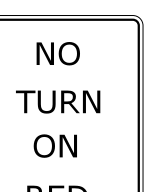
SIGN LEGEND:



LEGEND:



| R1 - SERIES | | | | | R2 - SERIES | | | | | R3 - SERIES | | | | | | | | | | R4 - SERIES | | | | | R5 - SERIES | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|--|---------------|----------------|-------|------------|----------------|---------------|----------------|-------|------------|-------------|------|---------|---|------|--|----|---------|---|------|---|----|---------|---|------|--|----|---------|---|------|--|--|--|--|--|---|---------------|----------------|-------|------------|--|-------|---------|---|------|---|---------------|----------------|-------|------------|----------------|---------------|----------------|-------|------------|-------|-------|---------|---|------|---|-------|---------|---|------|---|---------------|----------------|-------|------------|--|---------------|----------------|-------|------------|--|-------|---------|---|------|--|--|--|--|--|---|---------------|----------------|-------|------------|--|-------|---------|---|------|---|---------------|----------------|-------|------------|---|-------|---------|---|------|--|-------|---------|---|------|---|--|--|--|--|----------------|---------------|----------------|-------|------------|------|-------|---------|---|------|------|-------|---------|---|------|-------|-------|---------|---|------|--|--|--|--|--|
| R1-1  LEGEND - WHITE BACKGROUND - RED <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>1.85</td><td>18</td><td>31-0532</td><td>1</td><td>.080</td></tr> <tr><td>5.19</td><td>30</td><td>31-0552</td><td>1</td><td>.080</td></tr> <tr><td>7.98</td><td>36</td><td>31-0553</td><td>1</td><td>.080</td></tr> <tr><td>13.3</td><td>48</td><td>31-0557</td><td>2</td><td>.100</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 1.85 | 18 | 31-0532 | 1 | .080 | 5.19 | 30 | 31-0552 | 1 | .080 | 7.98 | 36 | 31-0553 | 1 | .080 | 13.3 | 48 | 31-0557 | 2 | .100 | R2-1  <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>5.00</td><td>24X30</td><td>31-5505</td><td>1</td><td>.080</td></tr> <tr><td>7.50</td><td>30X36</td><td>31-5504</td><td>1</td><td>.080</td></tr> <tr><td>12.00</td><td>36X48</td><td>31-5506</td><td>2</td><td>.100</td></tr> <tr><td>20.00</td><td>48X60</td><td>31-5507</td><td>2</td><td>.100</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 5.00 | 24X30 | 31-5505 | 1 | .080 | 7.50 | 30X36 | 31-5504 | 1 | .080 | 12.00 | 36X48 | 31-5506 | 2 | .100 | 20.00 | 48X60 | 31-5507 | 2 | .100 | R3-1  LEGEND - BLACK BACKGROUND - WHITE CIRCLE & DIAGONAL - RED <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>4.00</td><td>24X24</td><td>31-1604</td><td>1</td><td>.080</td></tr> <tr><td>9.00</td><td>36X36</td><td>31-1627</td><td>2</td><td>.080</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 4.00 | 24X24 | 31-1604 | 1 | .080 | 9.00 | 36X36 | 31-1627 | 2 | .080 | R3-5  31-0183  31-0184 | | | | | R3-6  ONLY | | | | | R3-8  31-0282  31-0283 | | | | | R3-8b  ONLY  ONLY | | | | | R4-1  DO NOT PASS | | | | | R4-16  KEEP RIGHT EXCEPT TO PASS | | | | | R5-1  LEGEND - WHITE BACKGROUND - WHITE CIRCLE - RED <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>6.25</td><td>30X30</td><td>31-1119</td><td>1</td><td>.080</td></tr> <tr><td>9.00</td><td>36X36</td><td>31-1120</td><td>2</td><td>.080</td></tr> <tr><td>16.00</td><td>48X48</td><td>31-1121</td><td>2</td><td>.100</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 6.25 | 30X30 | 31-1119 | 1 | .080 | 9.00 | 36X36 | 31-1120 | 2 | .080 | 16.00 | 48X48 | 31-1121 | 2 | .100 | R5-10a(CT)  NO PEDESTRIANS BICYCLES MOTOR SCOOTERS | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1.85 | 18 | 31-0532 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5.19 | 30 | 31-0552 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 7.98 | 36 | 31-0553 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 13.3 | 48 | 31-0557 | 2 | .100 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5.00 | 24X30 | 31-5505 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 7.50 | 30X36 | 31-5504 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 12.00 | 36X48 | 31-5506 | 2 | .100 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 20.00 | 48X60 | 31-5507 | 2 | .100 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4.00 | 24X24 | 31-1604 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 9.00 | 36X36 | 31-1627 | 2 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 6.25 | 30X30 | 31-1119 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 9.00 | 36X36 | 31-1120 | 2 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 16.00 | 48X48 | 31-1121 | 2 | .100 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| R1-2  LEGEND - RED BACKGROUND - WHITE <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>0.97</td><td>18</td><td>31-0520</td><td>1</td><td>.080</td></tr> <tr><td>3.90</td><td>36</td><td>31-0523</td><td>1</td><td>.080</td></tr> <tr><td>6.77</td><td>48</td><td>31-0522</td><td>2</td><td>.100</td></tr> <tr><td>10.83</td><td>60</td><td>31-0528</td><td>2</td><td>.100</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 0.97 | 18 | 31-0520 | 1 | .080 | 3.90 | 36 | 31-0523 | 1 | .080 | 6.77 | 48 | 31-0522 | 2 | .100 | 10.83 | 60 | 31-0528 | 2 | .100 | R2-4a  <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>32</td><td>48X96</td><td>31-5510</td><td>2</td><td>.125</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 32 | 48X96 | 31-5510 | 2 | .125 | R3-2  LEGEND - BLACK BACKGROUND - WHITE CIRCLE & DIAGONAL - RED <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>4.00</td><td>24X24</td><td>31-1603</td><td>1</td><td>.080</td></tr> <tr><td>9.00</td><td>36X36</td><td>31-1629</td><td>2</td><td>.080</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 4.00 | 24X24 | 31-1603 | 1 | .080 | 9.00 | 36X36 | 31-1629 | 2 | .080 | R3-6  31-0157  31-0158 | | | | | R3-8  ONLY  ONLY | | | | | R3-8  31-0295  31-0219 | | | | | R3-8b  ONLY  ONLY | | | | | R4-3  SLOWER TRAFFIC KEEP RIGHT | | | | | R5-1a  LEGEND - WHITE BACKGROUND - RED <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>6.00</td><td>36X24</td><td>31-1122</td><td>2</td><td>.080</td></tr> <tr><td>8.75</td><td>42X30</td><td>31-1123</td><td>2</td><td>.100</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 6.00 | 36X24 | 31-1122 | 2 | .080 | 8.75 | 42X30 | 31-1123 | 2 | .100 | R5-11  AUTHORIZED VEHICLES ONLY | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 0.97 | 18 | 31-0520 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 3.90 | 36 | 31-0523 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 6.77 | 48 | 31-0522 | 2 | .100 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 10.83 | 60 | 31-0528 | 2 | .100 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 32 | 48X96 | 31-5510 | 2 | .125 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4.00 | 24X24 | 31-1603 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 9.00 | 36X36 | 31-1629 | 2 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 6.00 | 36X24 | 31-1122 | 2 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 8.75 | 42X30 | 31-1123 | 2 | .100 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| R1-3P  LEGEND - WHITE BACKGROUND - RED <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>0.75</td><td>18X6</td><td>31-0508</td><td></td><td>.080</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 0.75 | 18X6 | 31-0508 | | .080 | R3-3  NO TURNS | | | | | R3-7  LEFT LANE MUST TURN LEFT | | | | | R3-8  ONLY  ONLY | | | | | R3-8  ONLY | | | | | R3-8b  ONLY  ONLY | | | | | R4-7  <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>5.00</td><td>24X30</td><td>31-1526</td><td>1</td><td>.080</td></tr> <tr><td>12.00</td><td>36X48</td><td>31-1536</td><td>2</td><td>.100</td></tr> <tr><td>20.00</td><td>48X60</td><td>31-1546</td><td>2</td><td>.100</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 5.00 | 24X30 | 31-1526 | 1 | .080 | 12.00 | 36X48 | 31-1536 | 2 | .100 | 20.00 | 48X60 | 31-1546 | 2 | .100 | R5-10a(CT)  LEGEND - BLACK TOP SECTION BACKGROUND - YELLOW BOTTOM SECTION BACKGROUND - WHITE <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>32.50</td><td>60X78</td><td>31-1719</td><td>2</td><td>.125</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 32.50 | 60X78 | 31-1719 | 2 | .125 | R5-10c  NO PEDESTRIANS <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>2.00</td><td>24X12</td><td>31-1774</td><td>1</td><td>.080</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 2.00 | 24X12 | 31-1774 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 0.75 | 18X6 | 31-0508 | | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5.00 | 24X30 | 31-1526 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 12.00 | 36X48 | 31-1536 | 2 | .100 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 20.00 | 48X60 | 31-1546 | 2 | .100 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 32.50 | 60X78 | 31-1719 | 2 | .125 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2.00 | 24X12 | 31-1774 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

| R6 - SERIES | | | | | R7 - SERIES | | | | | R8 - SERIES | | | | | R9 - SERIES | | | | | R10 - SERIES | | | | | R11 - SERIES | | | | | RETROREFLECTIVE STRIP | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|--|---------------|----------------|-------|------------|----------------|---------------|----------------|-------|------------|-------------|-------|---------|---|------|-------------|-------|---------|---|------|---|--|--|--|--|----------------|---------------|----------------|-------|------------|-----------------------|-------|---------|---|------|--|--|--|--|--|----------------|---------------|----------------|-------|------------|-------|-------|---------|---|------|--|--|--|--|--|----------------|---------------|----------------|-------|------------|------|-------|---------|---|------|---|-------|---------|---|------|----------------|---------------|----------------|-------|------------|---|-------|---------|---|------|--|---------------|----------------|-------|------------|----------------|---------------|----------------|-------|------------|---|-------|---------|---|------|----------------|---------------|----------------|-------|------------|---|-------|---------|---|------|---|--|--|--|--|----------------|---------------|----------------|-------|------------|------|-------|---------|---|------|---|------|---------|---|------|----------------|---------------|----------------|-------|------------|------|-------|---------|---|------|-------|-------|---------|---|------|---|--|--|--|--|----------------|---------------|----------------|-------|------------|------|------|---------|---|------|------|------|---------|---|------|
| R6-1  <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>3.00</td><td>36X12</td><td>31-1188</td><td>1</td><td>.080</td></tr> <tr><td>6.75</td><td>54X18</td><td>31-1189</td><td>2</td><td>.100</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 3.00 | 36X12 | 31-1188 | 1 | .080 | 6.75 | 54X18 | 31-1189 | 2 | .100 | R7-1  (R) → (D) ↔ (L) ← LEGEND - RED BACKGROUND - WHITE <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>1.50</td><td>12X18</td><td>31-0630</td><td>1</td><td>.080</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 1.50 | 12X18 | 31-0630 | 1 | .080 | R8-7  EMERGENCY STOPPING ONLY <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>12.00</td><td>48X36</td><td>31-0670</td><td>2</td><td>.100</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 12.00 | 48X36 | 31-0670 | 2 | .100 | R9-3  LEGEND - BLACK BACKGROUND - WHITE CIRCLE & DIAGONAL - RED <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>2.25</td><td>18X18</td><td>31-1705</td><td>1</td><td>.080</td></tr> <tr><td>4.00</td><td>24x24</td><td>31-1769</td><td>1</td><td>.080</td></tr> <tr><td>6.25</td><td>30x30</td><td>31-1770</td><td>1</td><td>.080</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 2.25 | 18X18 | 31-1705 | 1 | .080 | 4.00 | 24x24 | 31-1769 | 1 | .080 | 6.25 | 30x30 | 31-1770 | 1 | .080 | R10-6  31-0801  31-0802 <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>6.00</td><td>24X36</td><td></td><td>1</td><td>.080</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 6.00 | 24X36 | | 1 | .080 | R10-11b  NO TURN ON RED <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>9.00</td><td>36X36</td><td>31-0816</td><td>2</td><td>.080</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 9.00 | 36X36 | 31-0816 | 2 | .080 | R10-11c(CT)  RIGHT TURN ON RED FROM RIGHT LANE ONLY <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>7.50</td><td>30X36</td><td>31-0847</td><td>1</td><td>.080</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 7.50 | 30X36 | 31-0847 | 1 | .080 | R11-1  KEEP OFF MEDIAN <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>5.00</td><td>24X30</td><td>31-1915</td><td>1</td><td>.080</td></tr> <tr><td>20.00</td><td>48X60</td><td>31-1918</td><td>2</td><td>.100</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 5.00 | 24X30 | 31-1915 | 1 | .080 | 20.00 | 48X60 | 31-1918 | 2 | .100 | LEGEND - N/A BACKGROUND - RED <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>1.00</td><td>4X48</td><td>31-5003</td><td>1</td><td>.080</td></tr> <tr><td>2.00</td><td>4X72</td><td>31-5004</td><td>1</td><td>.080</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 1.00 | 4X48 | 31-5003 | 1 | .080 | 2.00 | 4X72 | 31-5004 | 1 | .080 |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 3.00 | 36X12 | 31-1188 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 6.75 | 54X18 | 31-1189 | 2 | .100 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1.50 | 12X18 | 31-0630 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 12.00 | 48X36 | 31-0670 | 2 | .100 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2.25 | 18X18 | 31-1705 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4.00 | 24x24 | 31-1769 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 6.25 | 30x30 | 31-1770 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 6.00 | 24X36 | | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 9.00 | 36X36 | 31-0816 | 2 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 7.50 | 30X36 | 31-0847 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5.00 | 24X30 | 31-1915 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 20.00 | 48X60 | 31-1918 | 2 | .100 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1.00 | 4X48 | 31-5003 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2.00 | 4X72 | 31-5004 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| R6-1  <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>3.00</td><td>36X12</td><td>31-1177</td><td>1</td><td>.080</td></tr> <tr><td>6.75</td><td>54X18</td><td>31-1178</td><td>2</td><td>.100</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 3.00 | 36X12 | 31-1177 | 1 | .080 | 6.75 | 54X18 | 31-1178 | 2 | .100 | R7-2a  VARIABLE TIMES (R) → (D) ↔ (L) ← LEGEND - RED BACKGROUND - WHITE <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>1.50</td><td>12X18</td><td>31-0603</td><td>1</td><td>.080</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 1.50 | 12X18 | 31-0603 | 1 | .080 | R9-3a  NO PEDESTRIAN CROSSING <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>1.50</td><td>12X18</td><td>31-1702</td><td>1</td><td>.080</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 1.50 | 12X18 | 31-1702 | 1 | .080 | R10-7  DO NOT BLOCK INTERSECTION <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>5.00</td><td>24X30</td><td>31-0806</td><td>1</td><td>.080</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 5.00 | 24X30 | 31-0806 | 1 | .080 | R10-10L  LEFT TURN SIGNAL <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>7.50</td><td>30X36</td><td>31-0852</td><td>1</td><td>.080</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 7.50 | 30X36 | 31-0852 | 1 | .080 | R10-11a  NO TURN ON RED <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>7.50</td><td>30X36</td><td>31-0830</td><td>1</td><td>.080</td></tr> <tr><td>12.00</td><td>36X48</td><td>31-0819</td><td>2</td><td>.100</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 7.50 | 30X36 | 31-0830 | 1 | .080 | 12.00 | 36X48 | 31-0819 | 2 | .100 | NOTES: 1. THE LEGEND "O.S.T.A." SHALL APPEAR ON ALL R - SERIES SIGNS EXCEPT WHEN SUFFIXED WITH THE LETTER "Z". 2. FOR SPECIFIC SIGN DESIGN CONTACT CONN. D.O.T., DIVISION OF TRAFFIC ENGINEERING. FOR BOLT HOLE PATTERN REFER TO FHWA PUBLICATION "STANDARD HIGHWAY SIGNS". SIGNS OF DIFFERENT DIMENSIONS TO BE ERCTED ON THE SAME POSTS, OR SPAN/MAST ARM MOUNTED, MAY REQUIRE SPECIAL BOLT HOLE PATTERNS. 3. POSTS - SEE STANDARD SHEET TR-1208_02 - "METAL SIGN POSTS AND SIGN MOUNTING DETAILS." 4. POSTS SHALL BE 4 LBS./FT. 5. SIGNS SHALL BE FABRICATED OF ONE CONTINUOUS PIECE OF SHEET ALUMINUM. SPICING OF SHEET ALUMINUM WILL NOT BE ACCEPTED. 6. FOR OVERHEAD MOUNTED SIGNS, SEE STANDARD SHEET TR-1114_01 - "BONDING AND UTILITY POLE ATTACHMENT DETAILS. SIGN HANGER, "Y" CLAMP DETAIL." COLORS: BACKGROUND - WHITE - EXCEPT AS NOTED. LEGEND - BLACK - EXCEPT AS NOTED. ALL SIGNS TO USE TYPE IX RETROREFLECTIVE SHEETING. | | | | | RETROREFLECTIVE STRIP LEGEND - N/A BACKGROUND - RED <table border="1"> <thead> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> </thead> <tbody> <tr><td>1.00</td><td>4X48</td><td>31-5003</td><td>1</td><td>.080</td></tr> <tr><td>2.00</td><td>4X72</td><td>31-5004</td><td>1</td><td>.080</td></tr> </tbody> </table> | | | | | AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | 1.00 | 4X48 | 31-5003 | 1 | .080 | 2.00 | 4X72 | 31-5004 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 3.00 | 36X12 | 31-1177 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 6.75 | 54X18 | 31-1178 | 2 | .100 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1.50 | 12X18 | 31-0603 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1.50 | 12X18 | 31-1702 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5.00 | 24X30 | 31-0806 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 7.50 | 30X36 | 31-0852 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 7.50 | 30X36 | 31-0830 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 12.00 | 36X48 | 31-0819 | 2 | .100 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AREA (SQ. FT.) | SIZE (INCHES) | CONN. D.O.T. # | POSTS | ALUM. THK. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1.00 | 4X48 | 31-5003 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2.00 | 4X72 | 31-5004 | 1 | .080 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |